



City of Lone Tree Planning Commission Agenda Tuesday, April 12, 2016

Meeting Location:
Meeting Procedure:

City Council Meeting Room, Lone Tree Civic Center, 8527 Lone Tree Parkway

The Lone Tree Planning Commission and staff will meet in a public Study Session at 6:00 p.m. in the lower level of the Civic Center. The Regular Meeting will be convened at 6:30 p.m. in the City Council meeting room. Contact Jennifer Drybread, jennifer.drybread@cityoflonetree.com if special arrangements are needed to attend (at least 24 hours in advance). Comments from the public are welcome during the Public Comment portion of the meeting (brief comments on items not appearing on the regular meeting agenda). Those persons requesting to comment on an agenda item will be called upon by the Chair. If you have any questions please contact Jennifer Drybread, Senior Planner, at jennifer.drybread@cityoflonetree.com, or 303-708-1818.

5:30 p.m. Study Session Agenda

1. Administrative Matters
-

6:30 p.m. Regular Meeting Agenda

1. Opening of Meeting / Roll Call
2. Conflict of Interest Inquiry
3. Public Comment (For Items NOT appearing on the agenda)
4. Minutes of the March 22, 2016 Planning Commission meeting
5. Site Improvement Plans for the Regional Transportation District's Southeast Rail Extension Stations:
 - a. Sky Ridge Station (SP16-09R)
 - b. Lone Tree City Center Station (SP16-10R)
 - c. RidgeGate Station (SP16-11R)
6. Adjournment

MINUTES OF THE

Lone Tree Planning Commission Meeting
March 22, 2016

Lone Tree Civic Center

1. Attendance

In attendance were:

- Dave Kirchner, Chair
- Andrew Dodgen, Vice Chair
- Rhonda Carlson, Planning Commissioner
- Daryl Heskin, Planning Commissioner
- Richard Rodriguez, Planning Commissioner
- Kevin Spencer, Planning Commissioner
- Herb Steele, Planning Commissioner

In attendance from staff were:

- Kelly First, Community Development Director
- John Cotten, Public Works Director
- Jennifer Drybread, Senior Planner
- Hans Friedel, Planner III

2. Opening of Meeting / Roll Call

Chair Kirchner called the meeting to order at 6:30 p.m.

3. Conflict of Interest Inquiry

Commissioners Rodriguez and Spencer recused themselves due to a potential conflict of interest over the Tract GG agenda item. They were not present at the start of the meeting or during the discussion of this agenda item.

4. Public Comment (For Items NOT appearing on the agenda)

There was no public comment for items not on the agenda.

5. Minutes of the February 23, 2016 Planning Commission meeting

Commissioner Steele moved to approve the minutes, Commissioner Dodgen seconded, and the minutes were approved by the Commissioners present.

6. RidgeGate Section 22, Filing 1, Preliminary Plan (Also known as Tract GG or The Retreat at RidgeGate) Project SB16-12R

Ms. Drybread introduced the item, preliminary plan approval for 50 single family detached homes and approval of an amendment to the RidgeGate Residential West Village Sub-Area Plan. She described that the preliminary plan has been reduced from 70 lots, as shown on an earlier submittal to the Planning Commission. She provided an overview of the preliminary plan, including access via the future extension of Cabela Drive, and relocation of the trail. She also outlined the history of the preliminary plan, including a previous submittal which was withdrawn during a City Council Meeting due to multiple concerns, and the public outreach, referral, and sub-area plan review process.

She described that planning staff and Public Works continues to support a full public access connection between the proposed development and the Montecito neighborhood via Alicante Road; however, in response to concerns from residents, the applicant proposes an emergency-only connection. The resubmittal contained fewer and lower retaining walls. The architecture, which has received RidgeGate Design Review Committee (DRC) approval was also presented.

Staff finds that the Preliminary Plan is in conformance with the Subdivision Code, the Zoning Code, the Comprehensive Plan, the RidgeGate PDD 4th Amendment, and the RidgeGate West Village Residential Sub-Area Plan. Staff finds that the proposed amendments to the RidgeGate West Village Sub-Area Plan regarding Planning Area 11 are in keeping with the overall intent of the Plan and the RidgeGate Planned Development.

Staff recommended the Planning Commission recommend approval to the City Council of the Preliminary Plan, including the Sub-Area Plan amendment, subject to the following:

1. The Final Plat shall depict the connection between Tract GG and Montecito at Alicante Road as a full public access.
2. The Final Plat application shall include a detailed landscape plan, including detailed plans for the parks, retaining walls, entryways, and pump house.
3. The applicant shall provide wildfire mitigation measures as called for in the proposed Sub-Area Plan chapter on Planning Area 11 in the CC&Rs to be recorded with the Final Plat.
4. The applicant shall post a large map in the sales office and provide a map to purchasers and prospective purchasers of lots in Tract GG that shows the extension of Cabela Drive to the bluffs, with a note that states that there are a maximum of 346 residential units zoned for development on the mesa tops. The applicant will also post signs with the same information and a map along the extension of Cabela Drive, with such signs to be maintained by the Rampart Range Metro District.
5. The developer shall provide information to residents about living with wildlife when they buy their homes, available through the Colorado Parks and Wildlife offices.
6. Construction inspection reports, as-built records and a final written and sealed certification shall be provided (by a licensed professional structural engineer

and/or professional Geotechnical Engineer) demonstrating that the retaining walls as constructed are in conformance with the approved structural engineer design provided. This Certification shall be provided before approvals for issuance of associated building permits.

7. Final approval by the Public Works Department.

Ms. Drybread then introduced Mr. Darryl Jones with Coventry Development to speak from the perspective of the landowner. He clarified that they did not expect future development on the bluffs for some time due to the lack of utility infrastructure, as Parker Water and Sanitation District is the provider here for that service. He also clarified that the trail would be relocated by the Rampart Range Metropolitan District and that the land for the pump house would be dedicated by the District. Mr. Jones stated that Coventry continues to support the project, and it meets their long-range planning objectives.

Ms. Lisa Evans, Managing Director of Century Communities, accompanied by her design team, presented the project. She stated that they believe they have addressed all concerns from the previous submittal. The community would be called the Retreat at RidgeGate. Homes would range in size from 2,800 – 4,400 square feet and would be priced between \$800,000 and \$1,200,000. The density would be 1.2 dwelling units per acre. She provided an overlay comparing the lower density of the Retreat at RidgeGate with the existing Montecito community. She also showed a graphic overlaying the revised preliminary plan with the original submittal, visually depicting the reduction in the number of lots from 70 to 50. There were two ranch and three two-story home plans.

The resubmittal includes gaps between five clusters (A-E) of homes to preserve views of the drainage channel and views of open space. Ms. Evans showed photo simulations to illustrate reduced massing and density of homes – highlighting the increased contiguity of open space. Also, she highlighted how one would now have a nearly unobstructed view of the bluffs from Alicante Road; whereas on the previous submittal, houses were in this viewshed. The homes would be stair stepped and integrated into the topography.

She presented a park plan that showed entry landscaping at the primary and secondary entrances – including public art – and two smaller pocket parks within the community. All lots would have access to the regional trail connector.

Key issues during the previous Planning Commission hearing on the former application were the emergency access-only connection between Montecito and the Retreat at Alicante Road, and locating the pump station in a commercial area across (east) Cabela Drive from the neighborhoods. She showed a graphic of the pump house.

Ms. Evans showed a graphic depicting the location of informative signage showing where future growth would occur. She then submitted a copy of her presentation for the record and reiterated that they believe the development met the zoning requirements and Comprehensive Plan.

Commissioner Dodgen sought clarification of the number of units that could eventually go on the mesa tops above the bluffs. If Southridge Preserve were included, there could be up to 346 future residential units on the bluffs. He wanted to ensure that Century Communities would inform future residents of the development potential on top of the mesa.

He also sought clarification that each home would receive DRC approval. Ms. Evans responded that they would submit multiple homes at a time to DRC – but that this was correct.

Also, Commissioner Dodgen inquired about the fire access road from the original submittal. It was determined to no longer be necessary per the Fire District's analysis since they only have 38 homes on the main street in the development. The 10-foot wide access between the two cul-de-sacs could consist of crusher fines and could accommodate vehicles up to 15,000 lbs. Ms. Drybread stated that this could serve as fire access in the summer, early fall and late spring, when there would be no snow on the path. Ms. Evans stated that this connection is intended to serve as access for the Southgate Water and Sanitation District to their water main underlying the path.

Commissioner Dodgen inquired about who would maintain the drainage channel – this would be the Rampart Range Metro District (RRMD).

Commissioner Dodgen inquired about the impacts of future development on the mesas on drainage and flow rate through the channel. Mr. Cotten responded that detention facilities would be required on top of the mesas to reduce the flow of water through the channel.

Commissioner Dodgen further inquired whether the pump station would contribute to the entry-way architecture of the subdivision – on the same level as Montecito. Ms. Evans responded that the pump station was small and would be set back off the road. He wanted to make sure that the pump station had an appealing look. Ms. Evans responded that it would be designed within Southgate's regulations, and Century would make it as visually appealing as possible. She also added that the main thing that you will see as you enter the subdivision is the future Marriott TownePlace Suites hotel. Commissioner Dodgen asked if Southgate would park vehicles there – yes, during maintenance of the pump station, but Southgate would not house vehicles there.

Commissioner Dodgen inquired about the coordination of the relocation of the trail with the construction of the roadway. His concern was damage to the trail and a gap in connectivity during the time of construction. Mr. Jones responded that the realignment of the trail would be coordinated with the road construction and that there would not be a gap in trail connectivity during construction.

Commissioner Dodgen inquired of the applicant's traffic engineer, Chris McGranahan, about the reduced trip generation of the resubmitted plan. Mr. McGranahan stated that the average single family home produced approximately ten vehicle trips per day, so the total would be about 500 trips now– as opposed to 700 trips with the original 70 lot submittal. He stated that this wouldn't change much of the impact on Montecito if the Alicante Road connection were to remain open – as the number of homes from Tract GG potentially taking this connection was only changing from 40 to 30 per day.

Commissioner Dodgen inquired about who was responsible for monitoring fire danger levels and communicating with SMFRA. Further, Commissioner Dodgen asked the applicant if it would be mandatory in the HOA documents that residents could only choose plants from the Firewise list provided by SMFRA.. Ms. Evans responded that notice of

this would be in the sale documents, including a fire-wise plant list. She stated that would be a part of the homeowner documentation.

Commissioner Dodgen inquired about the potential vacation of the portion of Alicante Road in Montecito and who that would go to. Mr. Cotten stated that State Statute requires it revert to the properties on either side, which would be the adjoining lots. However, it is his hope that the Montecito HOA would step up and accept ownership. As proposed by the applicant, there would be an agreement in place between the HOAs whereby the Retreat will provide snow removal and Montecito would take care of long-term maintenance costs of that segment of Alicante Road.

Commissioner Dodgen inquired about whether lot premiums and upgrades would increase the price of homes from the \$800,000 to \$1,200,000 range. Ms. Evans responded that the homes were semi-custom, and that it could. Commissioner Dodgen inquired what the minimum lot premium would be. Ms. Evans stated that they had not set this.

Liesel Cooper, Executive Vice President for Century Communities, responded that the \$800,000 to \$1,200,000 would be the expected finish price for the homes with lot premiums and upgrades. Ms. Evans stated that 50% to 60% of the buying public chooses the same model as the model homes.

Commissioner Dodgen inquired about the varied streetscape and the breakdown of ranch to two-story homes. Ms. Evans stated that they would facilitate the desire of the public; however, did not want to designate certain lots as ranches as this could lead to a monotonous streetscape. He further inquired about the height differential between ranches and two-story homes. Paul Brady, the project architect, responded that the smaller ranch model would not include a raised volume above the house like in Montecito; however, the ranch would have a clerestory that could be as high as 16 feet.

Commissioner Carlson addressed the concept of a monotonous streetscape. She felt that limiting homes within a cluster to no more than 30% seems limiting and that she was hoping for more of a commitment to allow for low-profile homes as provided in the sub-area plan. She suggested an exception to allow more than 30% ranch models within a cluster, provided the elevations vary.

Ms. Evans responded that if all the homes in a cluster were a ranch, this would violate the streetscape diversity requirements. Commissioner Carlson wanted there to be more support for low-profile massing. Ms. Evans responded that they would look at this. Ms. Evans stated that the homes would not rest on a flat plane, but would be recessed into the topography to reduce mass.

Commissioner Carlson recommended that the applicant use drought tolerant plantings as provided in the sub-area plan, and replace Kentucky/Texas Hybrid Sod (not native to Colorado high desert) with Tall Fescue (or offer Tall Fescue as an option), as it uses 50% less water, is disease resistant, is widely available, and features the same green color. Ms. Evans appreciated this and indicated that the landscape plan was forthcoming.

Commissioner Carlson inquired whether there would be a bridge for road crossing over the Cottonwood Creek (wetland area), between lots 26 & 27, to support rather than

diminish wildlife movement and preserve the creek. The applicant and Mr. Cotten responded that the road crossing will be elevated and graded, allowing drainage underneath. Mr. Cotten responded that there would be side slopes from the road, which would go down into the valley and ascend out of it. He said they had looked at using retaining walls that would have reduced impact on vegetation, but created more of a barrier for wildlife. Ms. Evans stated that the side slopes off the crossing would allow for access to the drainage for maintenance and would be native and revegetated. Chair Kirchner inquired if there would be a culvert beneath the road. Mr. Cotten responded there would, and that water would flow under the road in a flood, not over.

Commissioner Carlson inquired about building setbacks per the sub-area plan. She asked what the minimum side setback would be from the building to the closest retaining wall. In a prior meeting, the walls were as close as 3 feet. Ms. Evans responded that the distances between homes would be 15, 20, and in some cases 25 feet. The minimum distance between a home and a side retaining wall would be 7.5 feet. Commissioner Carlson asked about the distance between Lot 50 and the future road. The distance from the lot to the right-of-way will be 25' to 30' and from the home to the right-of-way will be about 50 to 60 feet. Fencing and landscaping by the homeowners could provide some separation.

Commissioner Carlson further asked, since lots 48-50 are close to (the future) Cabela Drive, will there be a fence separating the road from the community? The applicant responded that home owners will be responsible for installing a fence in their back yard. It would be an open rail fence as called for in the sub-area plan. Ms. Evans stated that they would post a sign by the road there clearly stating there would be a road extension.

Commissioner Carlson added that City Council raised several concerns in the previous public meeting, including providing a transition to the bluffs, given this is an environmentally sensitive area. She asked the applicant for their interpretation of a "transition to the bluffs" and how have they satisfied this concern?

The applicant responded that they were not cutting into the bluffs, but working with the natural topography and protecting natural resources. They have satisfied this concern by removing the homes where they were previously cutting into the bluffs, and removed the retaining walls that were cumulatively as high as 40 feet.

Commissioner Carlson asked if the applicant will use the same retaining wall material they used in Montecito. Ms. Evans responded that they will use the same material used at Bluffmont Heights. Commissioner Carlson asked if the applicant can reduce the height of the individual walls, similar to Bluffmont Heights, where the total 25-foot high retention wall was built with 5-6-foot terraced walls (instead of using 3 – 8-foot tiers)? The applicant responded that such might be possible, but the area where the accumulated wall height is 23 feet would not be perceived as tall, as the walls would be built into the channel, and one stretch of the wall at the bottom is very short in length.

Commissioner Carlson supports connected streets, per staff recommendation and in compliance with the City's Comprehensive Plan.

Commissioner Heskin inquired regarding the colors of the homes. Ms. Evans responded that choices of colors were not tied to specific model homes, but that prospective home

buyers would have a choice of colors. He stated that some of the colors were “vanilla” and asked whether there were other color schemes available. Ms. Evans responded that there were a variety of color schemes available.

Commissioner Heskin said he wrestled with the randomness of the market choosing where two-story and one-story homes would go. His concern was specifically regarding two-story homes being predominantly located near the high-points of the subdivision. Ms. Evans stated that the tallest side of the house was never towards the bluffs; walkouts would be towards the channel. The proximity to the road would determine the main elevation of the house, so they step down away from the bluffs.

Commissioner Heskin appreciated the reduction in lots from 70 to 50, and felt this opened up views between the homes and better transitioned to the bluffs. He also commended the architect on the high-quality materials and architecture of the homes. He did express a concern over EIFS. Paul Brady, the project architect, stated that the majority of stucco would be cementitious with EIFS-like materials used in very limited applications under protrusions, window sills, etc. Commissioner Heskin cautioned to be sure that material was properly waterproofed, and said he appreciated the use of stucco.

Commissioner Heskin inquired as to the slope of the walkouts to the retaining walls. Commissioner Heskin was concerned that retaining walls that were close to the back of lots would need fencing for safety so people did not fall over the walls. Ms. Evans responded that this would be in the covenants. Commissioner Heskin wanted fencing to be constructed over the retaining walls concurrent with development so there would be no gaps in the fencing for safety. Ms. Evans stated that the fencing would go in likely with the development of each cluster.

Commissioner Steele stated that there were many good changes since last time. He stated that the different remediation measures were positive. He felt that the clustering of houses in the current configuration would foster community interaction.

Commissioner Steele sought clarification of whether the pump house was recessed into the hillside. Ms. Evans responded that this was the case, and that there would be 360-degree fencing around the pump house for security. He inquired if there could be solid fencing as opposed to open-rail, to achieve better screening. Lisa Albers, project engineer, responded that a taller, solid fence would require concertina wire. Commissioner Steele inquired if the pump house fencing would be consistent with the allowed fencing for the homes – both will be black metal and the only difference would be spikes on top of the pump house fencing. The pump house would require lighting. Commissioner Steele did not want the building to be illuminated at night and impacting the residences. Ms. Albers stated that the lighting would be downcast, in conformance with City standards.

Commissioner Steele inquired as to how the lots would be priced, and the expected relationship between higher-priced lots and higher-priced, larger homes. Ms. Cooper stated that sometimes people would sometimes choose a smaller home after going for a really high lot premium. Therefore, higher-priced lots did not necessarily dictate that this would correspond with the most expensive house.

Commissioner Steele expressed concern, having served on several HOA boards, that as complexity was added to the maintenance responsibilities, clarification would be needed

for the future HOAs boards. HOA maintenance responsibilities would include the pocket parks, common landscaping, retaining walls, the two private roads, and snow removal of the Alicante Road connection. He said that maintaining financial reserves would be really important versus just having an annual budget.

Commissioner Steele expressed concern over a small HOA maintaining the private streets into perpetuity, given that only 10 of the 50 lots are served by private streets. Ms. Evans provided a multipronged response. First, private maintenance of these roads would be disclosed. Second, there would be a plat note added, per request of staff, that these would be private roads into perpetuity. Ten homes would be on private streets that did not meet minimal city width standards. Commissioner Steele expressed concern that the majority of residents in the HOA would complain about maintaining the private streets. He expressed concern about the ongoing maintenance of the sidewalks on these private roads as well. Commissioner Steele inquired as to why these roads were so narrow. Ms. Evans responded that this was partially due to the desire to avoid requiring more retaining walls. If the private roads were wider to meet public standards, retaining walls would need to be added. She said it was also a trade-off that the HOA would have private roads, but fewer walls to maintain.

Commissioner Steele stated that there were three options: (1) that these would all be private roads and walls, (2) that they would be all public streets and walls, and (3) that there would be mixture of both HOA and city responsibilities. Commissioner Steele stated that his position was to keep things simple, that these would all be public streets with sidewalks. Ms. Cooper replied that part of the reason private streets were proposed was to minimize the impact of the bluffs. Ms. Cooper stated that this would all be disclosed to prospective home buyers. Commissioner Steele expressed concern about the availability and willingness of snow-removal contractors to contract for such a small job. Ms. Evans responded that the same contractor removing the snow, would likely be the same contractor maintaining the common landscaping at the entries and in the parks, and did not feel that would be a problem.

Commissioner Steele stated that he was conflicted about what to do with the Alicante Road connection. He asked of Mr. Cotten, if instead of the two HOAs maintaining the two halves of the connection, if the City could maintain the connection but discourage through traffic of cars by installing speed bumps or other change in street material. The Montecito streets are narrow and there are on-street parking, child safety, and other concerns. Mr. Cotten responded that for this to be a public street it would have to be available for any member of the public to drive down. Mr. Cotten stated that, after investigation, there are two alternatives (1) that it stays a public street without surface modification or (2) that it be private and maintained by the HOAs.

Mr. Jones stated that Council Member Millet is against private roads, and expressed this in a recent application in RidgeGate. She has concern that the HOA will want the City to maintain the roads in the future.

There was a question regarding maintenance responsibility for Tract M. The entity responsible will be the HOA, although the preliminary plan incorrectly stated it would be the RRMD. This will be corrected.

Chair Kirchner appreciated the work by the applicant to address public, City Council, and Planning Commission comments. He stated that the plan was more in line with what they were looking for.

He said that private roads are of concern to the City Council as subsequent buyers may not be aware of them or what it would take to bring them up to City standards. Ms. Albers stated that bringing these up to standard width would result in an additional 5 foot-8 foot retaining wall, with a potential cumulative height of 21 – 24 feet. Commissioner Kirchner thinks that wall trade-off will be a decision for City Council. He recommends all public roads to avoid problems in the future.

In terms of the Alicante Road connection, for reasons given by City and Public Works staff, he tends to go along with staff in suggesting it be open to the public, as 85% of the traffic on between these developments would be Montecito residents, and it was designed as a connected road.

Chair Kirchner also stated that the mitigation efforts in the sub-area plan calling for ranches was good, and that the new wall heights were well thought through.

Chair Kirchner opened the meeting for public comment.

Jeff Nodland, of the Board of Directors for the Montecito HOA, with David Williams, commented on the Alicante Road connection. His position continued to be that it be emergency access only. He emphasized that no residents are requesting it be a public road. He felt it may be inaccurate that traffic patterns will alter [if the connection is made a public access], as Montecito residents are used to driving around [to Crossington Drive]. The reasonable solution at hand was what Century was proposing.

Greg Fong, 10660 Montecito, stated that his house was right at the intersection of Alicante Road, with a view of the bluffs. He was told this would be open space. He had three requests, (1) remove lots 1-5 as they were directly in his view corridor, (2) to continue looking at the massing of the homes, and (3) that Alicante Road be closed to through traffic. His concern was that retail traffic will not want to wait for the light, and will instead use Alicante Road.

Commissioner Dodgen inquired of the Montecito HOA representatives if they have discussed the added cost of maintaining their portion of Alicante Drive and generally what the level of involvement on these issues has been with residents versus just the Board. He asked whether they would have to get a 2/3rds vote of the HOA to take over their portion of Alicante Drive.

Jeff Nodland said that approximately 20% of their residents have attended the HOA meeting where the connection was discussed. Residents that expressed the most concern, were residents next to Tract GG. Many homeowners have contacted the board and that they feel confident they can obtain the necessary votes, provided the costs are not significant.

Chair Kirchner closed the period for public comment.

Commissioner Dodgen offered a follow up question of the HOA representative regarding whether he felt residents wanted the connection to be emergency access only. Mr.

Nodland indicated that the connection was the top concern he has heard, and that some members have even discussed the possibility of making all the roads private in Montecito. They have not had a formal meeting determining full community support to make their portion of the Alicante connection private.

Commissioner Dodgen inquired of Mr. Cotten if speed bumps would be an option – to discourage through traffic on Alicante Road. Mr. Cotten responded that this was not out of the question, but something that was discouraged. He stated that people do not like to live next to speed bumps.

Commissioner Carlson was concerned with the 30% restriction on ranch homes within a cluster. A straw poll was taken and none of the other Commissioners expressed concern over the streetscape diversity standards. Again, Commissioner Carlson's concern was that perhaps the market would want more ranches. Ms. Evans suggested that perhaps they could exclude ranches from this provision.

Commissioner Dodgen moved to recommend approval of the application for preliminary plan and sub-area plan amendment, with staff conditions. Commissioner Heskin seconded. Chair Kirchner opened discussion on whether the Alicante Road should be public. Commissioner Dodgen was conflicted. Commissioner Carlson supported the connection being public. Commissioner Steele stated that although the Comprehensive Plan encouraged connected communities, he also respects the wishes of local communities, however he supported full public access. Commissioner Heskin felt very strongly that the provisions of the Lone Tree Comprehensive Plan be followed. He felt it should be a public street. Chair Kirchner stated that it should be a public street.

Commissioner Steele inquired as to making an amendment that all the streets in the Retreat be made public. There was discussion on this issue. Commissioner Steele stated that the applicant should provide full details to the City Council on both options (walls or private streets). Commissioner Carlson felt that if changing them to public roads would result in more retaining walls, she would advocate they be private roads. Chair Kirchner stated that this should be left to Council. Commissioner Carlson added that native grasses be considered. Commissioner Heskin reiterated that the developer build fence on top of the retaining walls. Commissioner Dodgen stated that the developer be sure to educate residents about the wildfire risk and the plant materials they can use. He also said that the sales staff should be trained to tell prospective buyers about the future development on the bluffs. They accepted staff conditions with a separate recommendation that the decision of public-private roads be left to the City Council. The motion passed unanimously.

7. RidgeGate Section 15, Filing 5, Second Amendment, Lot 1-C, Lincoln District East SIP 12th Amendment, a part of Phase II at Lincoln Commons – Starbucks, Project SP15-94R

Mr. Friedel introduced the application. Staff found that the application is in conformance with the Comprehensive Plan, SIP requirements of the Lone Tree Zoning Code, and in conformance with the Lincoln Commons Commercial/Mixed-Use Sub-Area Plan.

Staff recommended the Planning Commission approve the SIP amendment:

1. Subject to final approval by the City Public Works Department.

Mr. Haaf introduced the application and provided an overview of the site plan. The separate elements are consistent with Lincoln District East. The prominent patio east of the building creates a pedestrian-friendly transition to the coffee shop. The drive through lane wraps around the building.

There is a nonexclusive parking agreement – shared parking – throughout the Lincoln District East master planned development. There are 25 spaces required for Starbucks applying the shared parking agreement formula in the City's Code. There are 17 spaces provided, leaving 8 to be met by the common parking area.

The variety of materials render the store fronts comfortable to pedestrians – combined with streetscape materials, benches. The project will have a low retaining wall to screen the drive through. There is a variety of landscaping to generally match what is at Lincoln District East (LDE).

The building will meet LEED standards. There is no planned alcohol service.

Commissioner Rodriguez inquired for comparative purposes what the queue is at the Starbucks location on Maximus Drive. The applicant responded that they have not done a study examining the queuing at the Maximus Starbucks; however, he felt that this one has adequate queuing.

Commissioner Spencer expressed concern regarding vehicle stacking. He expressed concern regarding the Commons Street intersection with the private drive into the center, noting that this project will bring more traffic through that intersection. He inquired about a four-way stop. Mr. Cotten responded that there are not typically four-way stops where a public street intersects with a private drive. He also said that four-way stops require traffic warrants, and that this intersection does not meet this warrant at that this point.

Commissioner Spencer inquired about the availability of spaces outside the Starbucks lot. There will be seven surplus spaces for future need. He complimented the architecture and said it fits in nicely with the area.

Commissioner Steele stated that it was an attractive project. His concern was regarding the available parking. He was concerned that the traffic study was done on a Tuesday morning, and that Snooze was extremely busy on Saturday mornings. He thought stacking was an issue. It was noted that the screening wall would be 32 inches high and would block car headlights. He complimented the project landscaping and suggested that this location serve wine.

Commissioner Heskin inquired about the colors of the building. The applicant responded that the idea was to follow the requirements for this area. Mr. Jones responded that there was some flexibility, although the colors are largely driven

by the Lincoln Commons guidelines. Commissioner Heskin stated that it would be nice to have some variety in colors here, and thought there was an opportunity to introduce more color to the area with this project. The applicant responded that they were given some flexibility by DRC in this area, although they feel there is variation with the alternating brick and tan color patterns. The Starbucks is a little different than the other buildings at Lincoln Commons; however, it is mostly earth tones. Lisa Sunderland, with SCM solutions, stated that the DRC reviewed the building and that their opinions were incorporated into the building. Commissioner Heskin felt that the sameness would at some point become monotonous.

Commissioner Carlson inquired about the level of LEED certification Starbucks was pursuing. The applicant responded that it would be LEED-New Construction (NC). She further inquired if Starbucks considered using solar panels at their stores? The applicant responded that some stores have solar panels. The decision is up to Starbucks Corporate.

Commissioner Carlson inquired about parking, whether with the exception of handicapped parking, the applicant would consider signing the parking spots that front the entrance on east side as short-term 15 minute spots? The applicant responded that Starbucks Corporate was unlikely to do this.

Commissioner Carlson inquired whether the applicant would consider taller plantings to screen the north and south sides of the patio from the drive-through. The applicant responded that Feather Reed grasses were planned for the north and south sides of the patio.

Commissioner Carlson commended the applicant on the canopy at the electronic order board and drive-through window, as it will provide protection from the elements. She asked about time frame for construction. The applicant responded that it would take 110-120 days.

Commissioner Dodgen inquired about whether Lone Tree dental was referred on this project since it would have an impact on their parking. The applicant stated that the consultant who did the parking study inquired about parking issues between Snooze and the dental office. The dentist stated that parking tends to flow between them and is generally less of an impact in the afternoon

Commissioner Dodgen inquired about truncated domes mentioned in the referral letter from Public Works staff. Mr. Cotten responded that they were tactile features required at ADA ramps.

Commissioner Dodgen inquired about vehicle stacking. Ms. Sunderland stated that Starbucks typically don't provide stacking for more than 12 cars.

Commissioner Dodgen inquired about the area of the parking study. Mr. Friedel responded it was the area east of Town Ridge Drive. There was a nonexclusive

parking agreement in the center, and that there were adequate spaces in the common lot to accommodate the required Starbucks parking.

Chair Kirchner said he had shared the previous comment that it will be busy during weekend mornings. He inquired about hours for trash pickup and deliveries, relative to potential conflicts with parking. The applicant stated these were done during off-business hours. He said he appreciated the application.

Chair Kirchner opened the meeting for public comment, and there being none, closed it.

Commissioner Dodgen moved to approve SIP amendment, and Commissioner Steele seconded, and the motion passed unanimously.

Adjournment

There being no further business, Chair Kirchner asked for a motion to adjourn. Commissioner Dodgen made the motion to adjourn and Commissioner Steele seconded. The motion was approved unanimously. The meeting was adjourned at 10:11 P.M.

These minutes have been reviewed and confirmed by

_____ (name), on _____(date)



CITY OF LONE TREE

STAFF REPORT

TO: City of Lone Tree Planning Commission
FROM: Kelly First, Community Development Director
DATE: April 6, 2016
FOR: April 12, 2016 Planning Commission Meeting
SUBJECT: RidgeGate Section 15 Filing 17
Site Improvement Plan SP16-09R
Sky Ridge Station Station

Owner

RidgeGate Investments, Inc
Keith Simon
10270 Commonwealth Street
Lone Tree, CO 80124
720-279-2581

Representative

Regional Transportation District
Nathan Herman
1600 Blake Street
Denver, CO 80202
303-628-9000

Planning Commission Meeting Date: April 12, 2016
City Council Meeting Date: April 19, 2016

A. REQUEST:

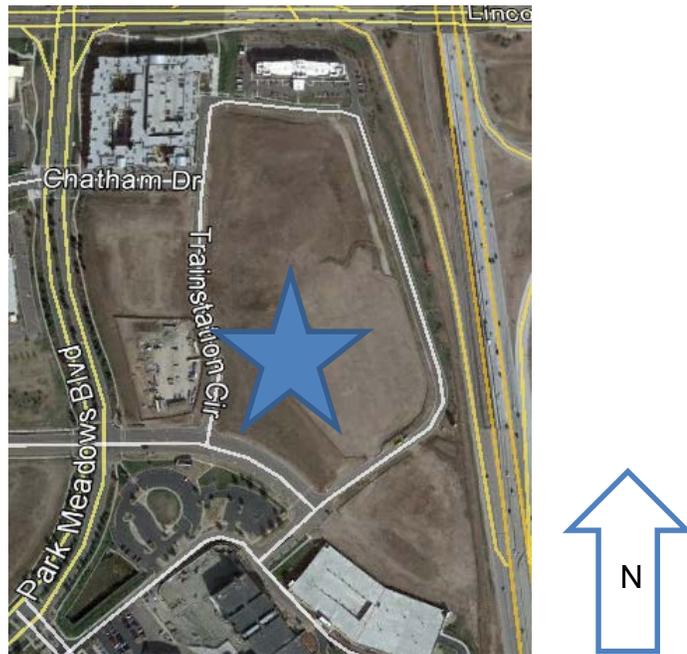
Approval of a Site Improvement Plan (SIP) for the Sky Ridge Light Rail Station.

In addition to the platforms, this station includes a landscaped transition plaza, a Kiss-n-Ride, and bicycle storage.

LOCATION:

The site is located west of I-25 and north of Sky Ridge Avenue, between Trainstation Circle East and Trainstation Circle West.

Vicinity Map



B. SITE CHARACTERISTICS:

The site has no distinctive topographical or vegetative features. As part of the Southeast Extension project, RTD prepared an Environmental Assessment, which resulted in a Finding of No Significant Impact.

C. SERVICE PROVIDERS:

Water:	Southgate Water District
Sanitation:	Southgate Sanitation District
Police:	Lone Tree Police
Fire:	South Metro Fire Rescue Authority
Metro District:	Rampart Range Metropolitan District

D. BACKGROUND:

The 2.3 mile-long southeast extension project, from Lincoln Avenue to RidgeGate Parkway, is part of the RTD's 2004 voter-approved FasTracks Program. The Southeast Line includes 19.1 miles extending from I-25/Broadway to Lincoln Avenue, with an additional connection along I-225 from I-25 to Parker Road. Construction on the extension is expected to begin this spring with service beginning in 2019. When completed, residents will be able to travel from the RidgeGate station on the new R Line through Aurora to Peoria Station and connect to the commuter train to Denver International Airport. They will be able to travel from RidgeGate to Union Station on the

E Line and from RidgeGate to 18th and California in downtown Denver on the F Line. The attached Fact Sheet from RTD provides additional information.

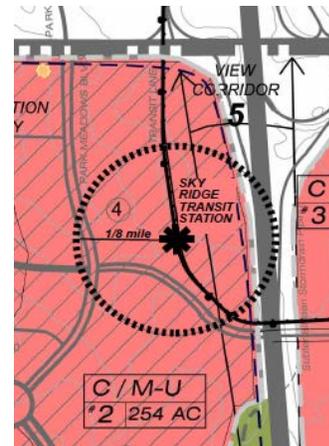
This SIP is one of three station SIPs that comprise the RTD Southeast Rail Extension project. Additional stations are Lone Tree City Center (SP16-10R) and RidgeGate (SP16-11R).

In January of 2015, City staff and members of the RTD design team presented an overview of the project to the Planning Commission and City Council. The purpose of the work sessions was to seek feedback about key design elements and address them prior to the project going out for bid, thereby reducing the need for potentially costly changes later on. Comments regarding aesthetic elements like canopies, walls, fencing and landscaping were provided and have been incorporated into SIP. In March, 2016, RTD again provided an overview of the stations to the Planning Commission and City Council, in anticipation of the SIPs coming forward for City Council action in April.

E. DESCRIPTION:

Zoning. The site is zoned Planned Development (PD) under the RidgeGate Planned Development 4th Amendment. The proposed use is in conformance with zoning, and is located within Planning Area C/M-U #2.

The location and design concept for the station are also consistent with the Sky Ridge Station TOD Sub-Area Plan.



Site Design. The site is designed to provide convenient and safe access and circulation, and contains design features that create an inviting and safe public realm. The light rail track extends down along the eastern side of the station area. Stairs and ramps lined with landscaped planters will guide pedestrians from street level up to the platform, which is approximately nine feet above street level. Benches are proposed along landscaped walls.

A semi-circular gathering space is provided between the stairs on the platform level of the transition plaza. This area will have a stone feature wall lined with a Colorado rose sandstone veneer and buff sandstone cap. This gathering plaza may be used year round for special events and community activities.

Site Plan



Access. Riders can access the station as pedestrians and bicyclists from the surrounding area, or be dropped off at the station. Bicycle storage is provided in close proximity to the platform. Large employment centers like Sky Ridge Medical Center, Charles Schwab, and offices to the north are all within walking distance to the station. Various multi-family projects are also nearby and mixed-use development around the station is planned in the future. Crosswalks and signage are provided to guide access to and around the station.

Vehicle pull-offs for passengers are provided on either side of Trainstation Circle West.

RTD does not plan on bus stops at this location.

Parking. Sky Ridge Station is a Kiss-n-Ride station only, meaning there is no vehicular parking provided.

Design Elements. The station platform includes overhead canopy structures that are white, barrel shaped, with a standing seam metal roof. Benches, bike racks, trash receptacles, and other site features are detailed in the SIP.

Stone walls at the plaza will have a metal ornamental fencing with custom art panel, where art could be added in the future. Other fencing at the site includes wrought iron fencing around the traction power substation at the north end of the site, and RTD standard chain link fencing along the track, which is outside the pedestrian zone.

Landscaping. The site will be landscaped as generally shown on the colored site plan and landscape plan. Details are still in progress relative to specific landscape species, sizes and quantities date. It is recommended that the final landscape plan be subject to staff approval prior to final approval of the SIP.

Lighting. The location of street, parking and pedestrian lighting and associated specifications are included on the SIP. Lighting is compliant with City requirements.

F. REFERRALS:

The project was sent on referral to all key agencies and subscribers to the City's referral list. All responses of substance are attached.

G. STAFF FINDINGS AND RECOMMENDATION:

Staff finds that the application is in conformance with the SIP requirements of the Lone Tree Zoning Code, the Subdivision Code, the Comprehensive Plan, the RidgeGate PDD, 4th Amendment, and the Sky Ridge Station TOD Sub-Area Plan.

Staff recommends the Planning Commission recommend approval of the SIP to City Council, subject to the following conditions:

1. Final approval of the Site Improvement Plan is subject to City of Lone Tree Public Works approval.
2. Prior to final SIP approval, the landscape plan shall be further detailed per City Zoning requirements to specify proposed plant species, quantities, and sizes.

H. ATTACHMENTS:

1. Letter of Authorization
2. Development Application
3. Narrative & Statement of Design Intent
4. Referral Comments
5. RTD Fact Sheet
6. SIP
7. Renderings

END

Letter of Authorization

Regarding Development Applications for Land Use Entitlements

City of Lone Tree
9220 Kimmer Drive #100
Lone Tree, CO 80124

RE: Property Address: Ridgegate Section 15

Assessor's Parcel Number (SPN): _____

To Whom It May Concern:

I/We, the owner(s) of the above described real property, authorize Nathan Herman
of Regional Transportation District to act as an agent on my/our behalf for the purpose of
creating, filing and/or managing an application for Sky Ridge Station Site Improvement Plan
_____ (type of development or permit application).

The undersigned hereby certifies to being the fee owner(s) or legally authorized representative of the fee
owner(s) of the real property described above.

Keith Simon, Ridgegate Investments, Inc.
(Print Name of Owner)

Keith Simon, Vice President
(Signature of Owner or Authorized Representative)

State of Colorado
County of Douglas

The foregoing instrument was acknowledged before me this 17th day of March, 2016
by Keith D. Simon.

Elizabeth Matthews
(Notary's official signature)

5-12-2016
(Commission expiration date)

NOTARY SEAL

**ELIZABETH MATTHEWS
NOTARY PUBLIC
STATE OF COLORADO
NOTARY ID 20004014222
MY COMMISSION EXPIRES 05/12/2016**



CITY OF LONE TREE

Site Improvement Plan Project Narrative & Statement of Design Intent Template

Planning Division

9220 Kimmer Drive, Lone Tree, Colorado 80124

303.708.1818 | www.cityoflonetree.com

Project Name RTD – Sky Ridge Station

Project # SP16-09R

Project Location North of Sky Ridge Ave. between Trainstation Circle East and West

Date March 17, 2016

Project Narrative

[ARTICLE XXVII - Site Improvement Plan \(SIP\) Project Narrative](#). The SIP process is intended to provide for development that enhances the quality of life in the City by promoting high-quality design and a strong economy, and by fostering a sustainable and healthy community. The SIP process is required to ensure the development will be in conformance with the [Comprehensive Plan](#), the [Design Guidelines](#), applicable chapters of this [Code](#) and applicable Planned Developments and Sub-Area Plans.

Using this form or a separate page(s), the applicant shall provide a written narrative describing their project. Use the following outline (Sec. 16-27-60) as a guide when formulating your narrative – please disregard sections that do not apply to your project:

1. General information.

- a. Provide the subdivision name, filing number, planning area number when located in a Planned Development, lot and block number or street address and section, township and range if not in a subdivision, and name of project.

RidgeGate Section 15 Filing 17
RidgeGate PDD 4th Amendment, Planning Area C/M-U #2
Sky Ridge Station

- b. Indicate zoning of the site and the zoning and current uses of adjacent land.

RidgeGate Planned Development District, 4th Amendment

The site is undeveloped. The Sky Ridge Medical Center is located immediately south of the station. All other land adjacent to the station is undeveloped. The Schwab office campus and a medium density residential complex are located further to the north/northwest of the station.

2. Development impacts. Describe overall impacts of the proposed development on adjacent lands and methods for mitigating those impacts.

The proposed Sky Ridge station is part of RTD's Southeast Rail Extension Project. RTD prepared an Environmental Assessment (EA) for the project which resulted in a Finding of No Significant Impact (October 2014). The EA addressed impacts to the adjacent lands and mitigation measures. For the Sky Ridge Station, no impacts to adjacent lands were identified in the EA.

3. Compliance with Intent and Approval Standards. Describe how the development complies with the Intent (Section 16-27-10) and Approval Standards (Subsection 16-27-90(a)) of this Article.

The Sky Ridge station is in compliance with the City's Comprehensive Plan, the Design Guidelines, and the RidgeGate PDD, 4th Amendment. The station design is following the standards set forth by the RidgeGate development to ensure it will be integrated both functionally and aesthetically with adjacent development.

4. Development phasing. Describe the proposed development schedule and phases of development for all proposed construction.

Construction of the Southeast Rail Extension is expected to start in 2016 and be completed in 2019. No phasing of the project is planned.

5. Other project data.

- a. Total number of employees on maximum shift when known (for parking purposes).

There will be no employees on site except for periodic maintenance personnel.

- b. Square footage of building.

No buildings are planned at the station.

- c. Lot area.

5.568 acres

- d. Anticipated opening date.

2019

6. **Sustainability.** Highlight ways in which the project furthers the City's environmental goals regarding sustainability. This may include a general description of the project location relative to other uses, public transit and trails; ease of travel to key destinations on foot or bicycle; water conservation and water quality measures; site layout; green building practices; or operational aspects of the use such as waste reduction, recycling or commuter trip reduction programs.

The RidgeGate Station is being designed to safely and efficiently meet the needs of all users, regardless of age and ability. High capacity transit service will provide a viable option to the private automobile, resulting in a more balanced transportation system for Lone Tree residents.

7. **Variances if applicable.** For those SIPs for which a variance from the standards in this Chapter, the Design Guidelines or Sub-Area Plans is requested, the narrative shall also explain the need for the variance. (Public notice may be required, see Section 16-26-60).

No variances are required.

Statement of Design Intent

Please describe how the project meets the intent of the [City of Lone Tree Design Guidelines](#), including the city's Core Design Principles (p. 11). If the project is located within a Planned Development that is governed by additional design standards or guidelines, please address how the project satisfies the intent of those standards and guidelines as well.

Please use the outline below as a guide in formulating your response. You may also use this opportunity describe particular strengths, unique features, sustainable practices, or innovations that distinguish the design of the project, as well as any particular opportunities or challenges that should be considered. This Statement of Design Intent is intended to encourage thoughtful consideration of design guidelines and to give project reviewers and decision makers a more thorough understanding of the project.

1. **Overall Design Concept.** Briefly describe the use and overall concept for the project as a whole.

The Sky Ridge Station is a proposed transit station located along RTD's proposed Southeast Rail Extension. The Southeast Rail extension extends light rail transit service 2.3 miles south of the existing Lincoln Station. The Sky Ridge Station will be a kiss-n-ride station near the Sky Ridge Medical Center. The concept of the station includes a vehicle drop off area, pedestrian plaza, and station light rail station platforms equipped with overhead canopies, seating, trash receptacles, and ticket vending machines. The vehicle drop off area will be located along Train Station Circle West. Pedestrian access is provided from Train Station Circle West and Sky Ridge Avenue to a pedestrian plaza via stairs and ramps.

2. **Context and Site.** Describe how the project relates functionally and visually to the context of the surrounding area. Consider issues of form and character, the natural environment, vehicular and pedestrian access and circulation, etc.

The Sky Ridge Station is conveniently located at the northeast corner of Train Station Circle and Sky Ridge Avenue in close proximity to the Sky Ridge Medical Center, Charles Schwab Campus and RidgeGate Apartments. The surrounding area is connected with a network of sidewalk adjacent to the existing street network that provides easy pedestrian access to the proposed station. Direct pedestrian access to the Sky Ridge Station is provided from the existing sidewalk network from Train Station Circle West and Sky Ridge Avenue to a pedestrian plaza via stair and ramps. The pedestrian plaza connects directly to the station

platform for loading and unloading of trains. Vehicles may access the proposed kiss-n-ride via Train Station Circle. Bicycle storage is provided in close proximity to the platform. No bus stops are proposed at the station.

3. **Public Realm.** Describe how the project contributes to an inviting, safe and functional public realm. Consider public spaces, street/sidewalk – level experience, lighting, landscaping, and signage.

The focus of the Sky Ridge Station is pedestrians. Benches line proposed landscape walls. Stairs and ramps lead pedestrians from street level up to the level of the light rail platform which is located approximately nine feet above street level. Landscape planters line the ramps and stairs. Lighting and cameras are provided at street level, along the stairs and ramps, in the pedestrian plaza, and on the light rail platform to create a safe and inviting environment.

A gathering space is provided between the stairs on the platform level of the transition plaza. This area is defined by a stone feature wall and will be landscaped, lined with an ornamental fence, and enhanced with pedestrian amenities. It is intended to be used year round for special events and other community activities.

Signage is proposed throughout the station to provide way finding as well as RTD system information. In addition, a large monument sign is proposed at the corner of Sky Ridge Avenue and Train Station Circle.

4. **Architectural Design.** Describe how the architectural design contributes to the unique qualities of the area and how design concepts result in a unified, functional and high-quality design. Consider building form and composition, façade composition and articulation, and materials, colors, and lighting.

All proposed landscape walls are lined with a Colorado rose sandstone veneer and have a buff sandstone cap. The large feature wall will be Colorado strip stone with vertical Colorado rose sandstone veneer monuments. The base of the wall will have landscape planting and LED accent lighting. The station platform overhead canopy structures are white, barrel shaped, with a standing seam roof.

Applicant Contact Information

Name: Nathan Herman
Business: Regional Transportation District
Address: 1600 Blake Street, Denver, CO 8020
Phone: 303.628.9000
Email: Nathan.Herman@RTD-Denver.com

Preparer Contact Information

Name: Lee Kellar
Business: WSP | Parsons Brinckerhoff
Address: 555 17th Street, Suite 500, Denver, CO 80202
Phone: 303.728.3047
Email: kellarlw@pbworld.com

Owner Contact Information

Name: Keith Simon
Business: RidgeGate Investments, Inc.
Address: 10270 Commonwealth Street, Suite B, Lone Tree, CO 80124
Phone: 720.279.2581
Email: KSimon@coventrydevelopment.com

Kelly First

From: Linda Langewisch <llangewisch@msihoa.com>
Sent: Wednesday, March 30, 2016 12:46 PM
To: Kelly First
Subject: FW: City of Lone Tree referral request

Good Afternoon:

The RidgeGate West Village HOA has no comment to the referral re: light rail stations. They seem to be well thought out and accessible.

Thank you,

Linda Langewisch, CMCA
Community Manager
MSI, LLC
6892 So. Yosemite Court Suite 2.101
Centennial, Co 80112
720.974.4273
Fax 303.751.7396
LLangewisch@msihoa.com

From: Kelly First [<mailto:Kelly.First@cityoflonetree.com>]
Sent: Monday, March 28, 2016 4:40 PM
Subject: City of Lone Tree referral request

Good Afternoon,

You are invited to review and comment on a referral request from the City of Lone Tree. There are three separate, but related, Site Improvement Plans (SIPs) for each of the three future light rail stations in Lone Tree as part of RTD's Southeast Rail Extension Project. Please forward any comments to kelly.first@cityoflonetree.com by **April 12th, 2016 via email or in writing to the address below.**

[Sky Ridge Station](#)
[Lone Tree City Center Station](#)
[RidgeGate TOD Station](#)

If you have already received this information through the City's website subscription list, please disregard this email. I apologize for any duplicates.
Thank you for your time!

Kelly A. First, AICP, LEED® AP
Community Development Director

City of Lone Tree
9220 Kimmer Drive, Suite 100
Lone Tree, CO 80124
Main ph: 303-708-1818
Direct line: 720-509-1274



SOUTH METRO FIRE RESCUE

LIFE SAFETY BUREAU

9195 E Mineral Ave, Centennial, CO 80112

PHONE: 720.989.2230 www.southmetro.org FAX: 720.989.2130

Jennifer Drybread
Senior Planner
City of Lone Tree
Community Development
9220 Kimmer Dr.
Lone Tree, CO 80124
303-708-1818

File #/Name: SP16-09R
Project Type: SkyRidge Station
Referral Received: March 25, 2016
Comments Due: April 12, 2016

SMFR Record #: REFSI16-00030
Review Date: April 1, 2016
Plan Reviewer: Jeff Sceili
720-989-2244
Jeff.sceili@southmetro.org

South Metro Fire Rescue has reviewed the above referenced referral submittal for compliance with the International Fire Code and other applicable standards. The SIP is approved as submitted.

2015 Fact Sheet



SOUTHEAST RAIL EXTENSION AT A GLANCE

- The Southeast Rail Extension is part of RTD's 2004 voter-approved FasTracks plan to expand transit across the Denver metro region.
- The extension will expand the Southeast Rail Line 2.3 miles from Lincoln Station to RidgeGate Parkway in Lone Tree.
- The expansion encompasses three new stations: a Kiss-n-Ride at Sky Ridge Avenue near Sky Ridge Medical Center; one at the future Lone Tree City Center; and a 2,000-slot Park-n-Ride at RidgeGate Parkway.
- The project has completed the Environmental Assessment (EA) needed to qualify for the Federal Transit Administration's (FTA) New Starts grant funding.
- In July 2014, the RTD Board authorized a \$207 million extension of the existing Southeast Light Rail Line farther south into Lone Tree. This action keeps the extension in the pipeline to receive a potential federal grant from the FTA.
- Stakeholders in the southeast have committed to contributing \$35 million to \$40 million in cash, right-of-way and other items to get the light rail extension project completed.

PROJECT OVERVIEW

- 2008: RTD FasTracks conducted an Environmental Evaluation (EE) to determine ideal alignment, station locations, potential environmental impacts and a companion mitigation plan.
- 2010: Final EE and 30 percent of basic engineering completed.
- 2011: Project team began an EA based on the EE to pursue federal funding.
- 2012: Team begins New Starts application process. RTD engineering begins work on advanced basic engineering and coordination of the project scope with stakeholders.
- 2013: Project accepted into the FTA's New Starts grant development phase and RTD FasTracks will submit additional information for further review over the next two to three years.
- 2014: The FTA signed the Finding of No Significant Impact, which concludes the EA undertaken to fulfill the requirements of the National Environmental Policy Act, a requirement for eligibility for federal funding.

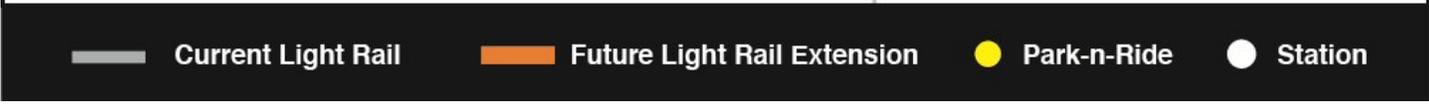
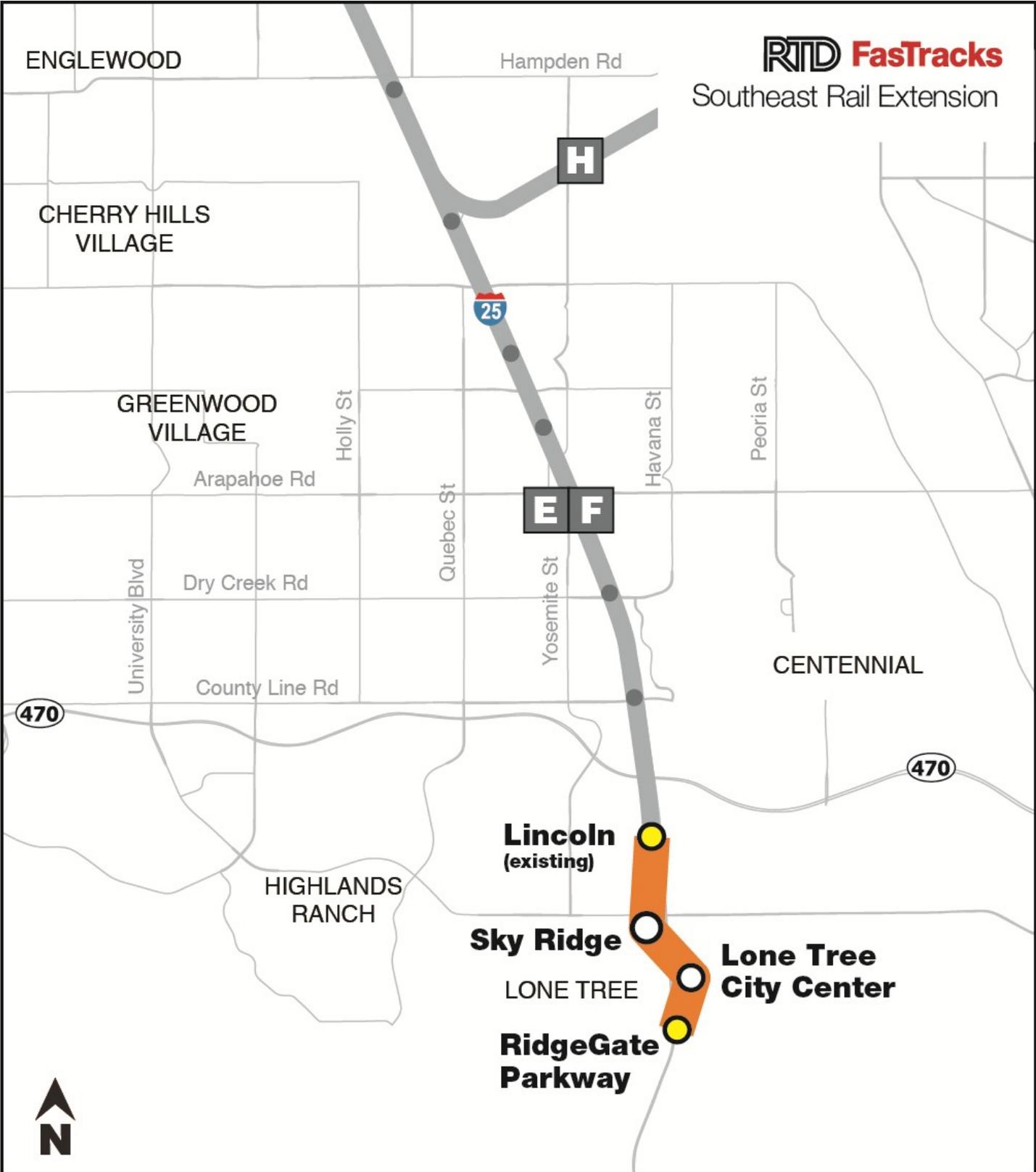
SOUTHEAST RAIL EXTENSION FAST FACTS

- Length: 2.3 miles
- Vehicle: Light rail
- Stations: 3
- Parking: 1,300 new spaces
- Service Frequency: 6 min (peak) / 7.5 min (off-peak)

For more information or to request a presentation, call 303.299.2831



RTD FaTracks
Southeast Rail Extension

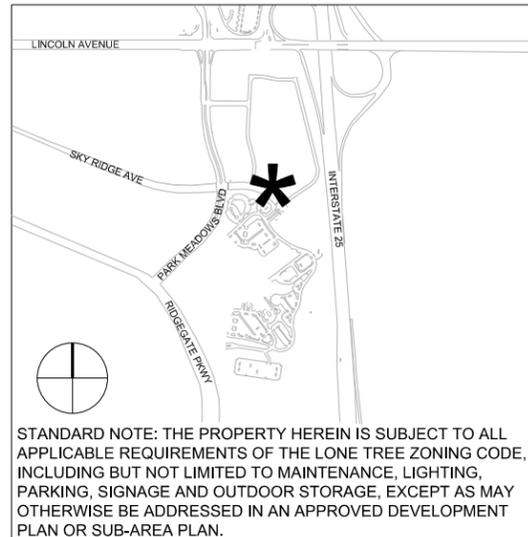


One region. One mission.

SIP SHEET INDEX		
NUMBER	Sheet Name	Sheet Issue Date
1 of 12	SITE PLAN	03/18/16
2 of 12	GRADING PLAN	03/18/16
3 of 12	LANDSCAPE PLAN	03/18/16
4 of 12	SITE FURNISHINGS	03/18/16
5 of 12	SITE FURNISHINGS	03/18/16
6 of 12	PHOTOMETRICS PLAN	03/18/16
7 of 12	LIGHTING FIXTURES	03/18/16
8 of 12	EAST PLATFORM ELEVATION	03/18/16
9 of 12	WEST PLATFORM ELEVATION	03/18/16
10 of 12	WALL ELEVATIONS	03/18/16
11 of 12	GRAND STAIR DETAILS	03/18/16
12 of 12	SECONDARY STAIR DETAILS	03/18/16

RIDGEGATE SECTION 15 FILING 17

RIDGEGATE PDD, 4" AMENDMENT, PLANNING AREA C/M-U #2
 2.438 ACRES
 SP16-09R
 (SKY RIDGE STATION)



STANDARD NOTE: THE PROPERTY HEREIN IS SUBJECT TO ALL APPLICABLE REQUIREMENTS OF THE LONE TREE ZONING CODE, INCLUDING BUT NOT LIMITED TO MAINTENANCE, LIGHTING, PARKING, SIGNAGE AND OUTDOOR STORAGE, EXCEPT AS MAY OTHERWISE BE ADDRESSED IN AN APPROVED DEVELOPMENT PLAN OR SUB-AREA PLAN.

Vicinity Map

<p>OWNER RIDGEGATE INVESTMENTS, INC. 10270 COMMONWEALTH STREET, SUITE B LONE TREE, CO 80124 P: 720.279.2581 CONTACT: KEITH SIMON</p>	<p>LANDSCAPE DESIGN BRITINA DESIGN GROUP 1615 CALIFORNIA STREET, SUITE 411 DENVER, CO 80202 P: 303.456.2887 CONTACT: CHRIS COOPER</p>
<p>APPLICANT REGIONAL TRANSPORTATION DISTRICT SOUTHEAST RAIL EXTENSION 1560 BROADWAY DENVER, CO 80202 P: 303.299.6965 CONTACT: ANDY MUTZ</p>	<p>STRUCTURAL ENGINEERING SAN ENGINEERING LLC 801 WEST MINERAL AVENUE, SUITE 200 LITTLETON, CO 80120 P: 303.953.9014 CONTACT: STEVE HAKES</p>
<p>LEAD DESIGN FIRM WSP PARSONS BRINCKERHOFF 555 17TH STREET, SUITE 500 DENVER, CO 80202 P: 303.832.9091 CONTACT: KAREN CREAMER</p>	<p>LIGHTING AND ELECTRICAL DESIGN PK ELECTRICAL, INC. 5105 DTC PARKWAY, SUITE 420 GREENWOOD VILLAGE, COLORADO 80111 P: 720.481.3290 CONTACT: ALAN WISKUS</p>
<p>ARCHITECT IRON HORSE ARCHITECTS 475 17TH STREET, SUITE 720 DENVER, CO 80202 P: 720.855.7572 CONTACT: TARA BURKE</p>	<p>GEOTECHNICAL ENGINEERING YEH AND ASSOCIATES, INC. 2000 CLAY STREET, SUITE 200 DENVER, CO 80211 P: 303.781.9590 CONTACT: MICHAEL KIEFER</p>

THIS SIP HAS BEEN REVIEWED AND FOUND TO BE COMPLETE AND IN ACCORD WITH CITY REGULATIONS, AS APPROVED BY THE CITY ON (DATE).

BY: _____

NAME: _____
 TITLE: COMMUNITY DEVELOPMENT DIRECTOR

DATE: _____

BY: _____

NAME: _____
 TITLE: CITY ENGINEER

DATE: _____

BY: _____

NAME: _____
 TITLE: MAYOR

DATE: _____

THE OWNER(S) OF THE LANDS DESCRIBED HEREIN, HEREBY AGREE(S) (1) TO DEVELOP AND MAINTAIN THE PROPERTY DESCRIBED HEREON IN ACCORDANCE WITH THIS APPROVED SITE IMPROVEMENT PLAN AND IN COMPLIANCE WITH CHAPTER 16 OF THE LONE TREE MUNICIPAL CODE AND THAT (2) THE HEIRS, SUCCESSORS AND ASSIGNS OF THE OWNER(S) SHALL ALSO BE BOUND. THE SIGNATURES OF THE OWNERS(S)/(S) REPRESENTATIVE(S) BELOW INDICATE THAT ANY REQUIRED AUTHORIZATIONS TO ENTER THIS AGREEMENT, INCLUDING ANY CORPORATE AUTHORIZATIONS, HAVE BEEN OBTAINED.

(NAME OF OWNER) _____

(SIGNATURE OF OWNER) _____

(PRINTED NAME AND TITLE) _____

STATE OF _____)

_____) ss.

COUNTY OF _____)

SUBSCRIBED AND SWORN TO BEFORE ME THIS ____ DAY OF

_____, 20____, BY _____.

WITNESS MY HAND AND OFFICIAL SEAL.
 MY COMMISSION EXPIRES:

(NOTARY PUBLIC)
 APPROVAL BY THE CITY OF LONE TREE DOES NOT SIGNIFY THAT THE REQUIREMENTS OF THE AMERICANS WITH DISABILITIES ACT (ADA) HAVE BEEN SATISFIED. THE APPLICANT IS RESPONSIBLE TO ENSURE THAT SAID ADA REQUIREMENTS HAVE BEEN MET.

RTD FasTracks
Southeast Rail Extension
 1560 BROADWAY, SUITE 700
 DENVER, COLORADO 80202
 (303) 299-6990

Balfour Beatty
 555 17th STREET, SUITE 400
 DENVER, CO 80202

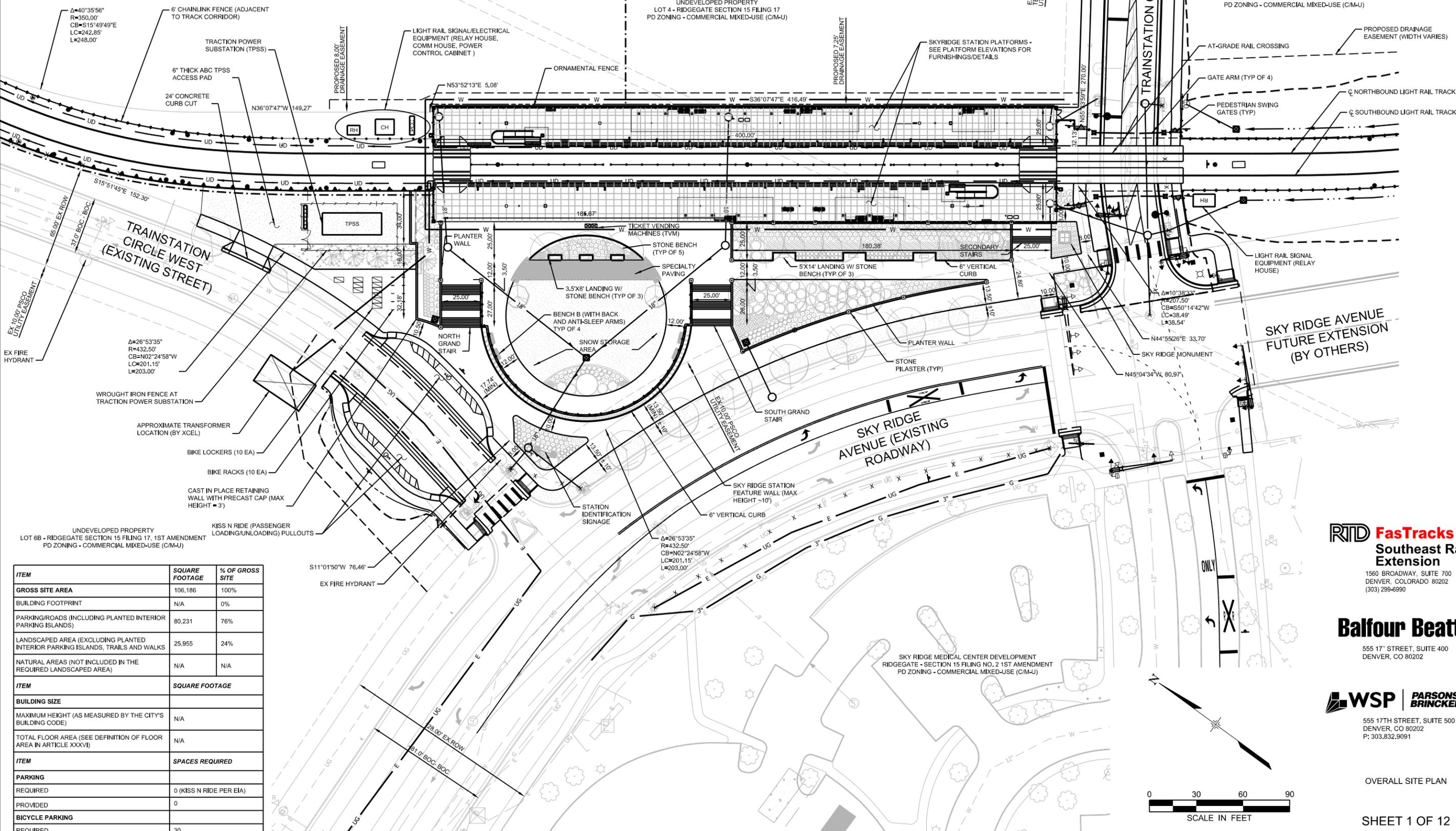
WSP | PARSONS BRINCKERHOFF
 555 17TH STREET, SUITE 500
 DENVER, CO 80202
 P: 303.832.9091

March 18, 2016

COVER

RIDGEGATE SECTION 15 FILING 17

RIDGEGATE PDD, 4th AMENDMENT, PLANNING AREA C/M-U #2
 2.438 ACRES
 SP16-09R
 (SKY RIDGE STATION)



ITEM	SQUARE FOOTAGE	% OF GROSS SITE
GROSS SITE AREA	106,186	100%
BUILDING FOOTPRINT	N/A	0%
PARKING/ROADS (INCLUDING PLANTED INTERIOR PARKING ISLANDS)	80,231	76%
LANDSCAPED AREA (EXCLUDING PLANTED INTERIOR PARKING ISLANDS, TRAILS AND WALKS)	25,955	24%
NATURAL AREAS (NOT INCLUDED IN THE REQUIRED LANDSCAPED AREA)	N/A	N/A

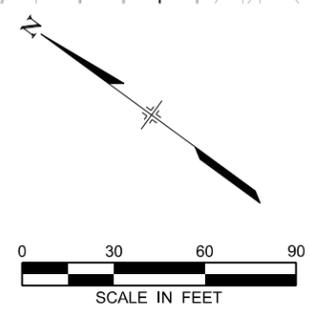
ITEM	SQUARE FOOTAGE
BUILDING SIZE	
MAXIMUM HEIGHT (AS MEASURED BY THE CITY'S BUILDING CODE)	N/A
TOTAL FLOOR AREA (SEE DEFINITION OF FLOOR AREA IN ARTICLE XXXVI)	N/A

ITEM	SPACES REQUIRED
PARKING	
REQUIRED	0 (KISS N RIDE PER EIA)
PROVIDED	0
BICYCLE PARKING	
REQUIRED	30
PROVIDED	30

RTD FasTracks
Southeast Rail Extension
 1560 BROADWAY, SUITE 700
 DENVER, COLORADO 80202
 (303) 299-6990

Balfour Beatty
 555 17th STREET, SUITE 400
 DENVER, CO 80202

WSP | PARSONS BRINCKERHOFF
 555 17th STREET, SUITE 500
 DENVER, CO 80202
 P: 303.832.9091

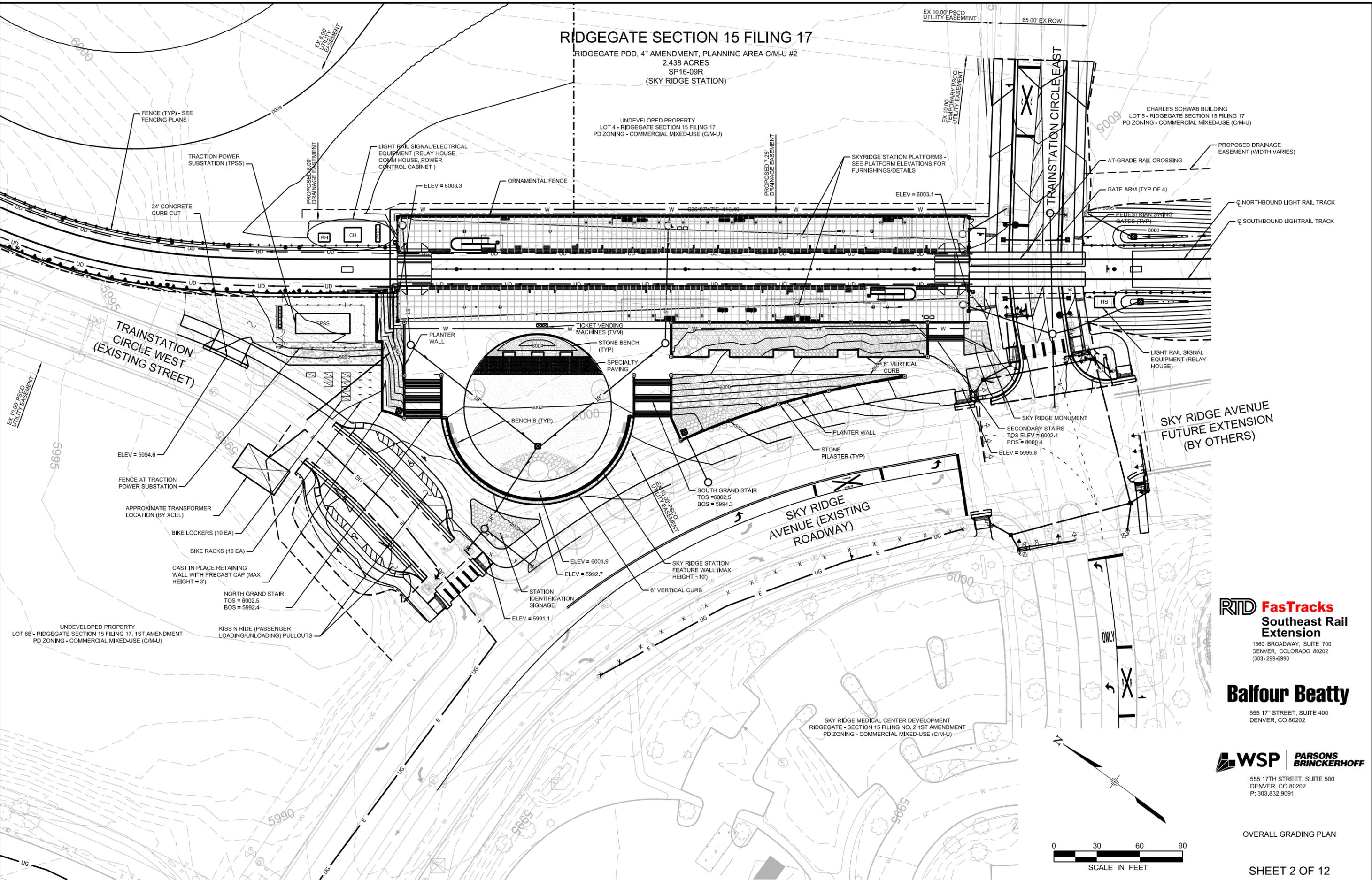


OVERALL SITE PLAN

SHEET 1 OF 12

RIDGEGATE SECTION 15 FILING 17

RIDGEGATE PDD, 4th AMENDMENT, PLANNING AREA C/M-U #2
 2.438 ACRES
 SP16-09R
 (SKY RIDGE STATION)

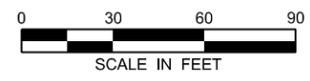


SKY RIDGE AVENUE
 FUTURE EXTENSION
 (BY OTHERS)

RTD FasTracks
Southeast Rail Extension
 1560 BROADWAY, SUITE 700
 DENVER, COLORADO 80202
 (303) 299-6990

Balfour Beatty
 555 17th STREET, SUITE 400
 DENVER, CO 80202

WSP | PARSONS BRINCKERHOFF
 555 17th STREET, SUITE 500
 DENVER, CO 80202
 P: 303.832.9091



OVERALL GRADING PLAN

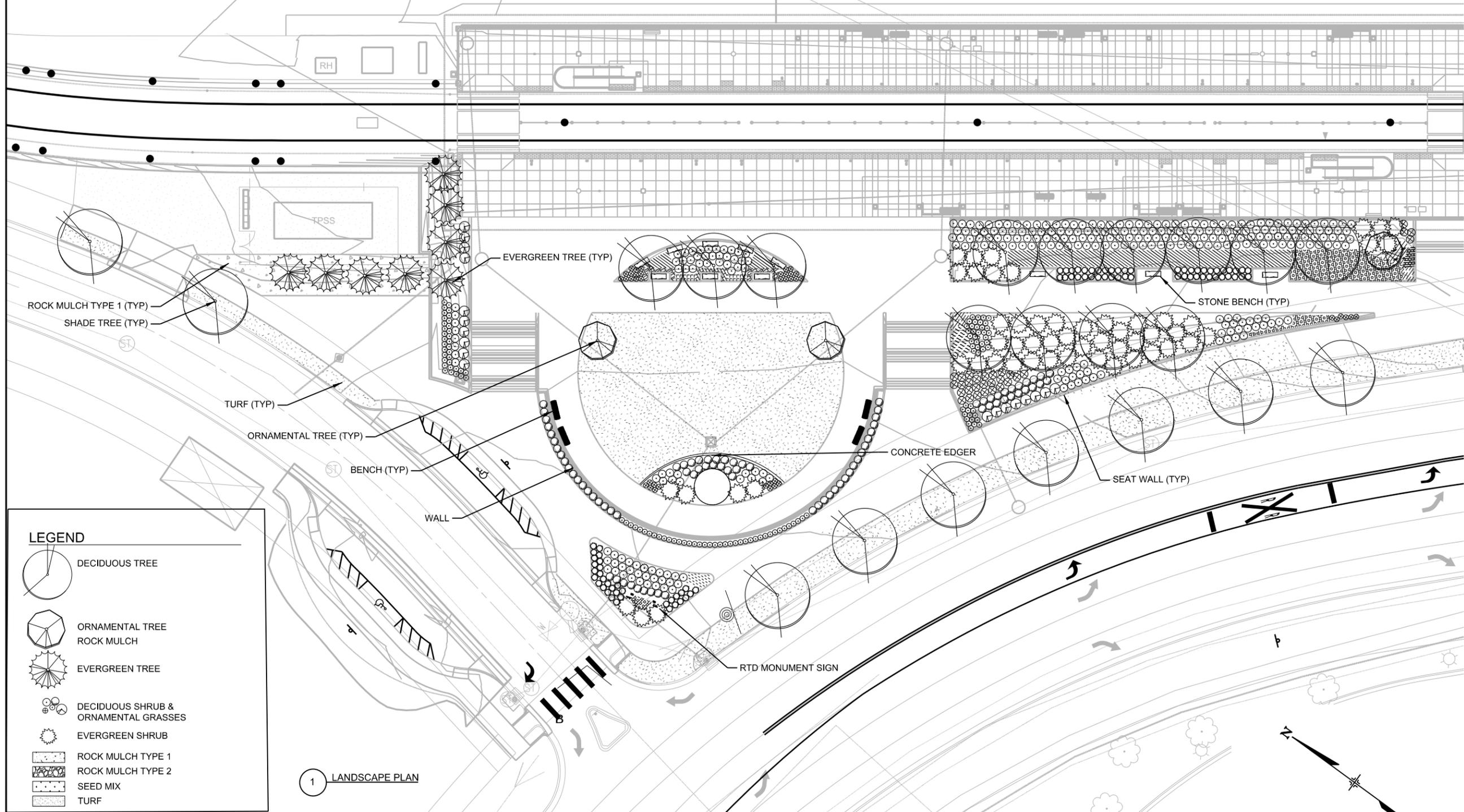
SHEET 2 OF 12

RIDGEGATE SECTION 15 FILING 17

RIDGEGATE PDD, 4" AMENDMENT, PLANNING AREA C/M-U #2
 2.438 ACRES
 SP16-09R
 (SKY RIDGE STATION)



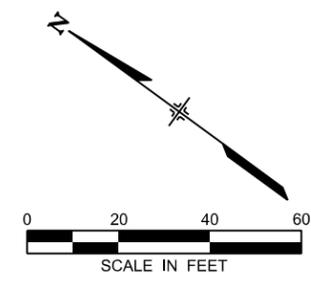
PRELIMINARY
 NOT FOR
 CONSTRUCTION



LEGEND

- DECIDUOUS TREE
- ORNAMENTAL TREE
- EVERGREEN TREE
- DECIDUOUS SHRUB & ORNAMENTAL GRASSES
- EVERGREEN SHRUB
- ROCK MULCH TYPE 1
- ROCK MULCH TYPE 2
- SEED MIX
- TURF

1 LANDSCAPE PLAN



RTD FasTracks
 Southeast Rail Extension
 1560 BROADWAY, SUITE 700
 DENVER, COLORADO 80202
 (303) 299-6990

Balfour Beatty
 555 17TH STREET, SUITE 400
 DENVER, CO 80202

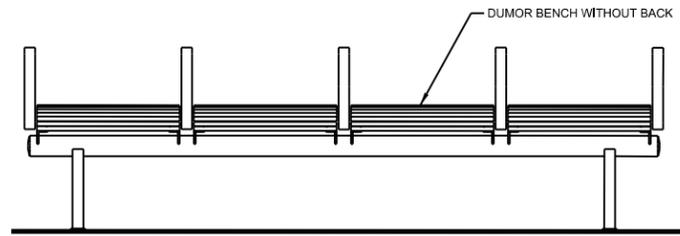
WSP | PARSONS BRINCKERHOFF
 555 17TH STREET, SUITE 500
 DENVER, CO 80202
 P: 303.832.9091

LANDSCAPE PLAN

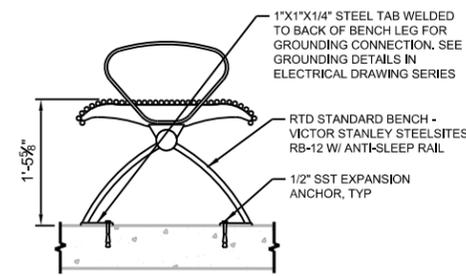
SHEET 3 OF 12

RIDGEGATE SECTION 15 FILING 17

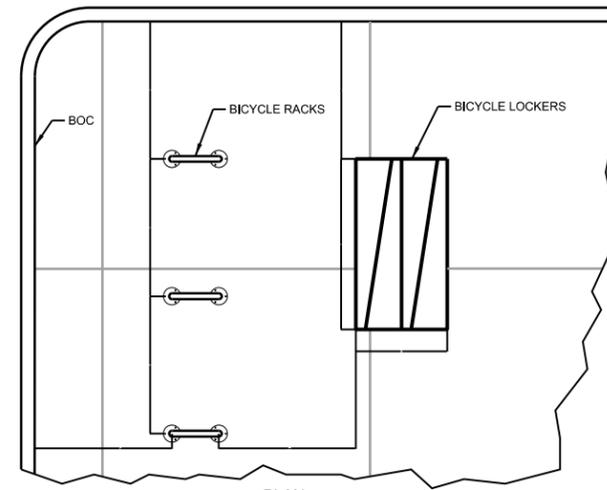
RIDGEGATE PDD, 4" AMENDMENT, PLANNING AREA C/M-U #2
 2.438 ACRES
 SP16-09R
 (SKY RIDGE STATION)



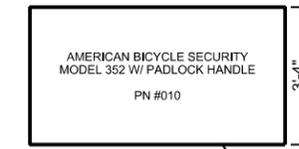
BENCH A FRONT ELEVATION



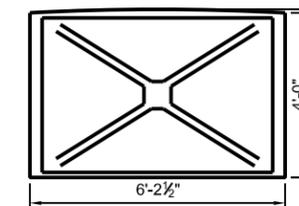
BENCH A SIDE ELEVATION



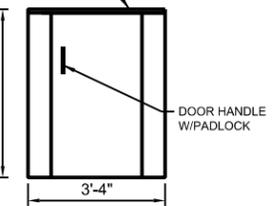
BICYCLE RACK & LOCKER LAYOUT (TYP)



TOP VIEW

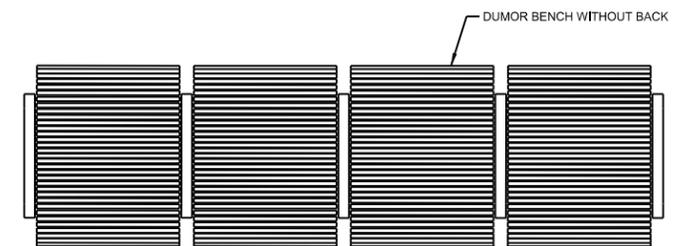


SIDE VIEW

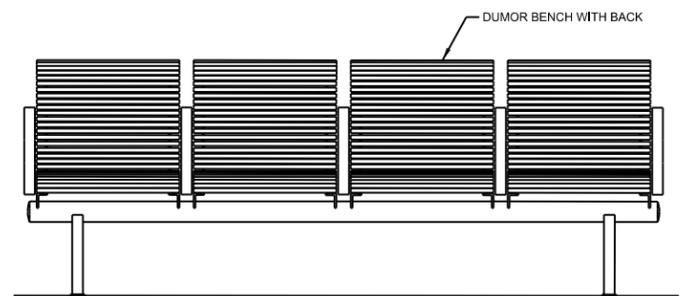


FRONT VIEW

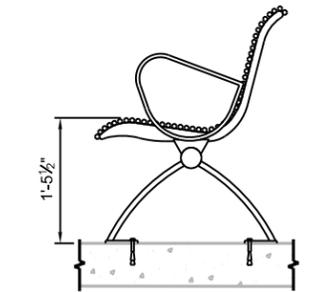
BICYCLE LOCKERS



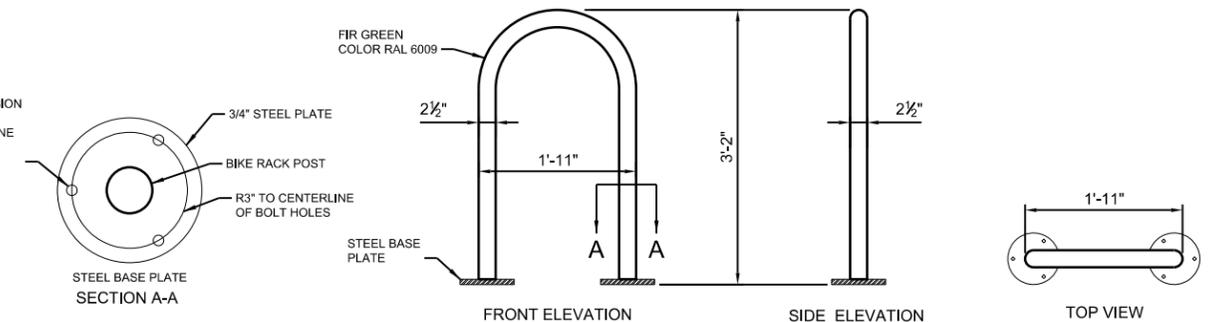
BENCH A PLAN



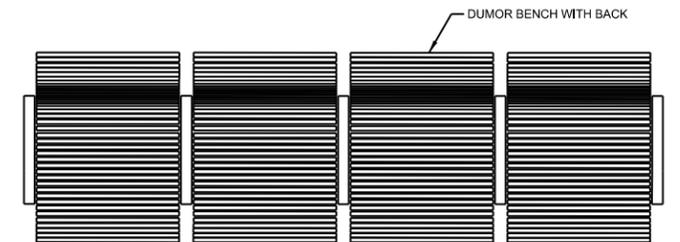
BENCH B FRONT ELEVATION



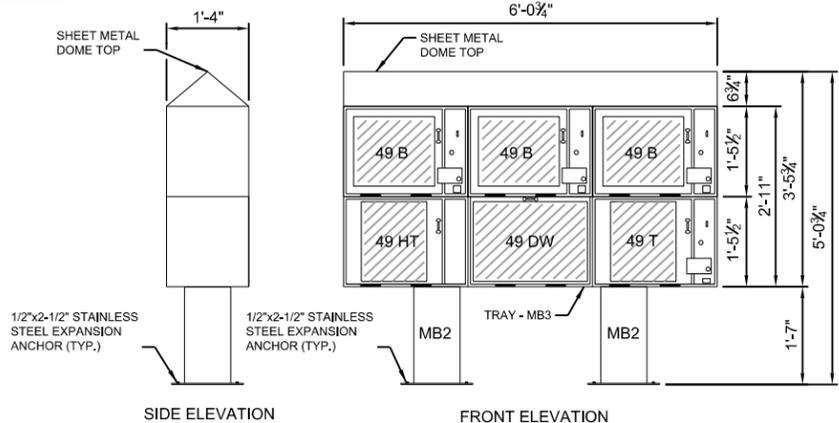
BENCH B SIDE ELEVATION



DETAIL 2 - BICYCLE RACK



BENCH B PLAN



NEWSPAPER "RAK" SYSTEMS

- NOTES:**
1. STANDARD COLOR SHALL BE RAL 6009 FIR GREEN
 2. ALL FABRICATED COMPONENTS ARE STEEL SHOT-BLASTED, ETCHED, PHOSPHATIZED AND ELECTROSTATICALLY POWDER-COATED WITH TGIC POLYESTER POWDER COATING.
 3. ALL SPECIFICATIONS ARE SUBJECT TO CHANGE. PLEASE CONTACT FACTORY FOR DETAILS.
 4. ALL DIMENSIONS ARE IN INCHES.
 5. AVAILABLE WITH LOCK OR LATCH
 6. "RAK" SYSTEM MODEL NOS. AND DESCRIPTIONS AVAILABLE FROM:
 RAK SYSTEMS, INC.
 5500 PLANTATION RD.
 THEODORE, AL 36582 U.S.A.
 TOLL FREE: (800) 467-1725
 LOCAL: (251) 653-4080
 FAX: (251) 653-1014

RTD FasTracks
Southeast Rail Extension
 1560 BROADWAY, SUITE 700
 DENVER, COLORADO 80202
 (303) 299-6990

Balfour Beatty
 555 17TH STREET, SUITE 400
 DENVER, CO 80202

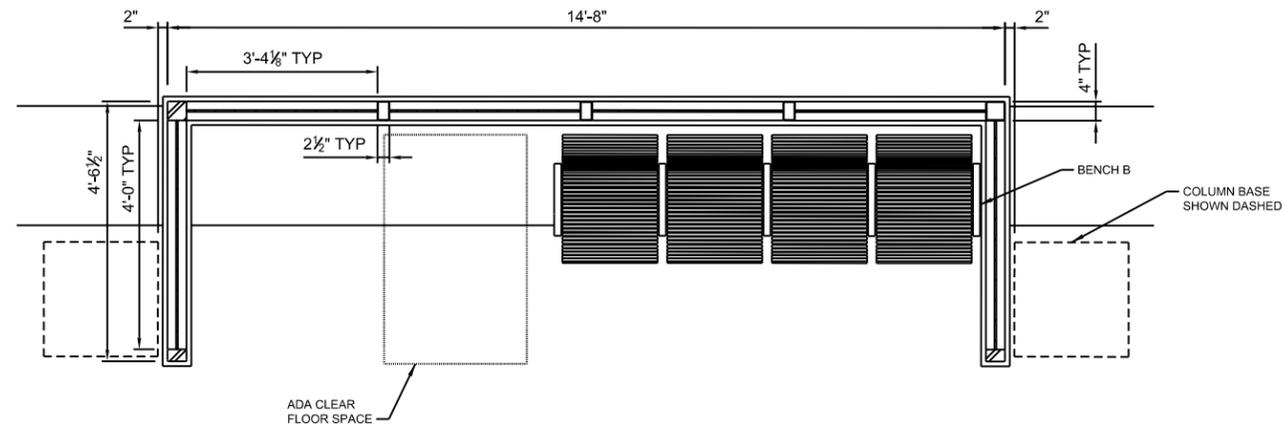
WSP | PARSONS BRINCKERHOFF
 555 17TH STREET, SUITE 500
 DENVER, CO 80202
 P: 303.832.9091

SITE FURNISHINGS

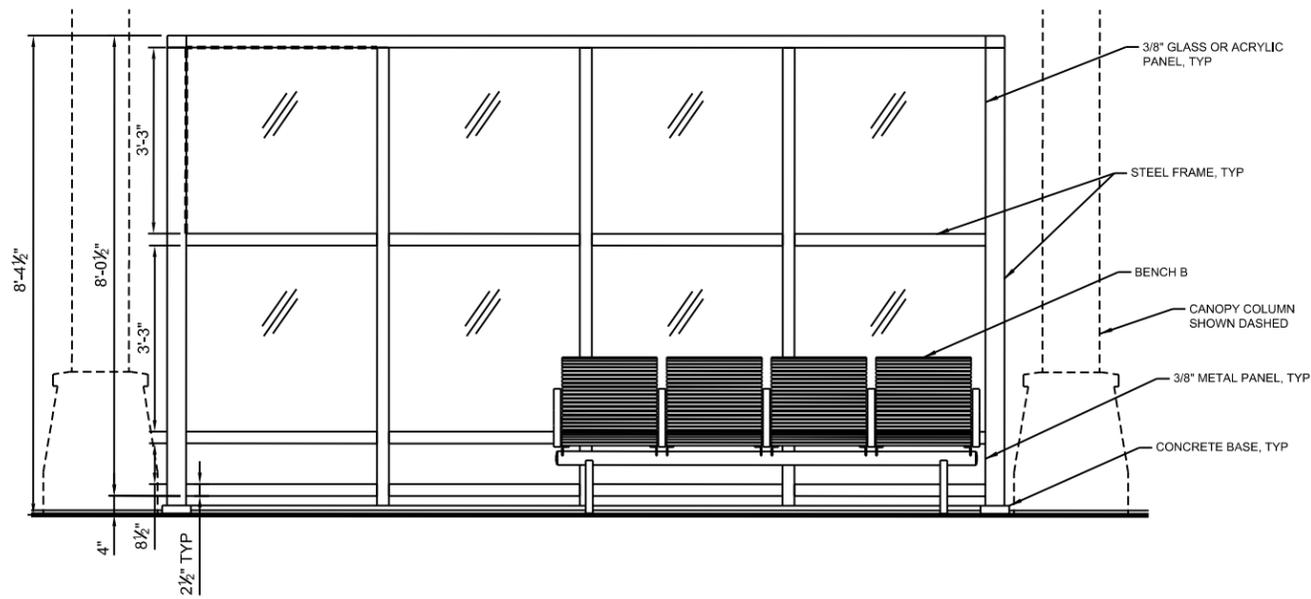
SHEET 4 OF 12

RIDGEGATE SECTION 15 FILING 17

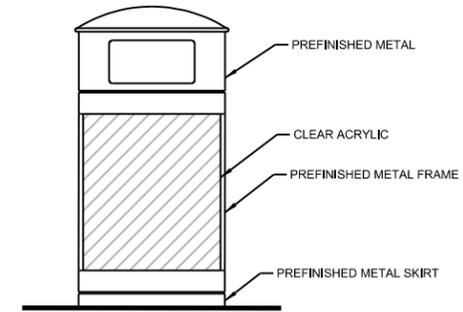
RIDGEGATE PDD, 4th AMENDMENT, PLANNING AREA C/M-U #2
 2.438 ACRES
 SP16-09R
 (SKY RIDGE STATION)



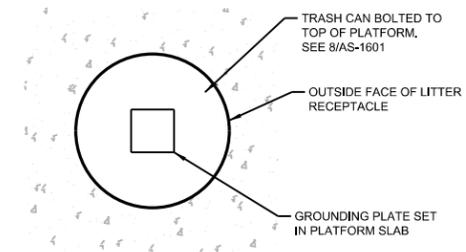
WINDSCREEN TYPE 1 PLAN



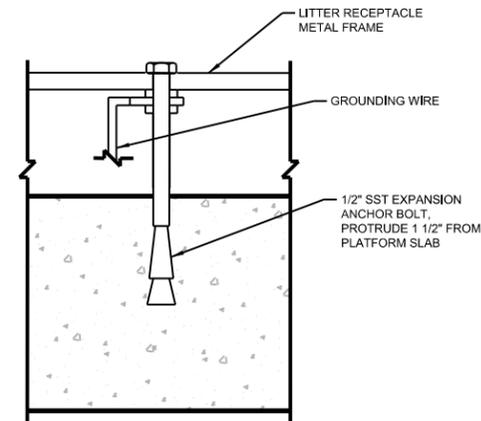
WINDSCREEN TYPE 1 FRONT ELEVATION



LITTER RECEPTACLE ELEVATION



LITTER RECEPTACLE PLAN



LITTER RECEPTACLE GROUNDING CONNECTION

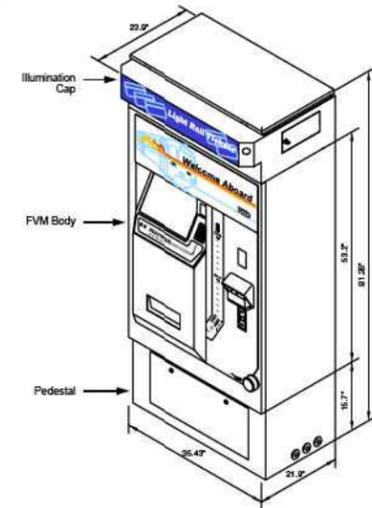


Figure 11-2: FVM Dimensions

FVM	FVM Unpacked	FVM Packed
Width	35.4"	Approximately 44"
Height	63.2"	Approximately 63"
Depth	21.0"	Approximately 31"
Weight	Approximately 551 lbs.	Approximately 591 lbs.

Table 11-1: FVM Dimensions and Weight

RTD FasTracks
Southeast Rail Extension
 1560 BROADWAY, SUITE 700
 DENVER, COLORADO 80202
 (303) 299-6990

Balfour Beatty
 555 17th STREET, SUITE 400
 DENVER, CO 80202

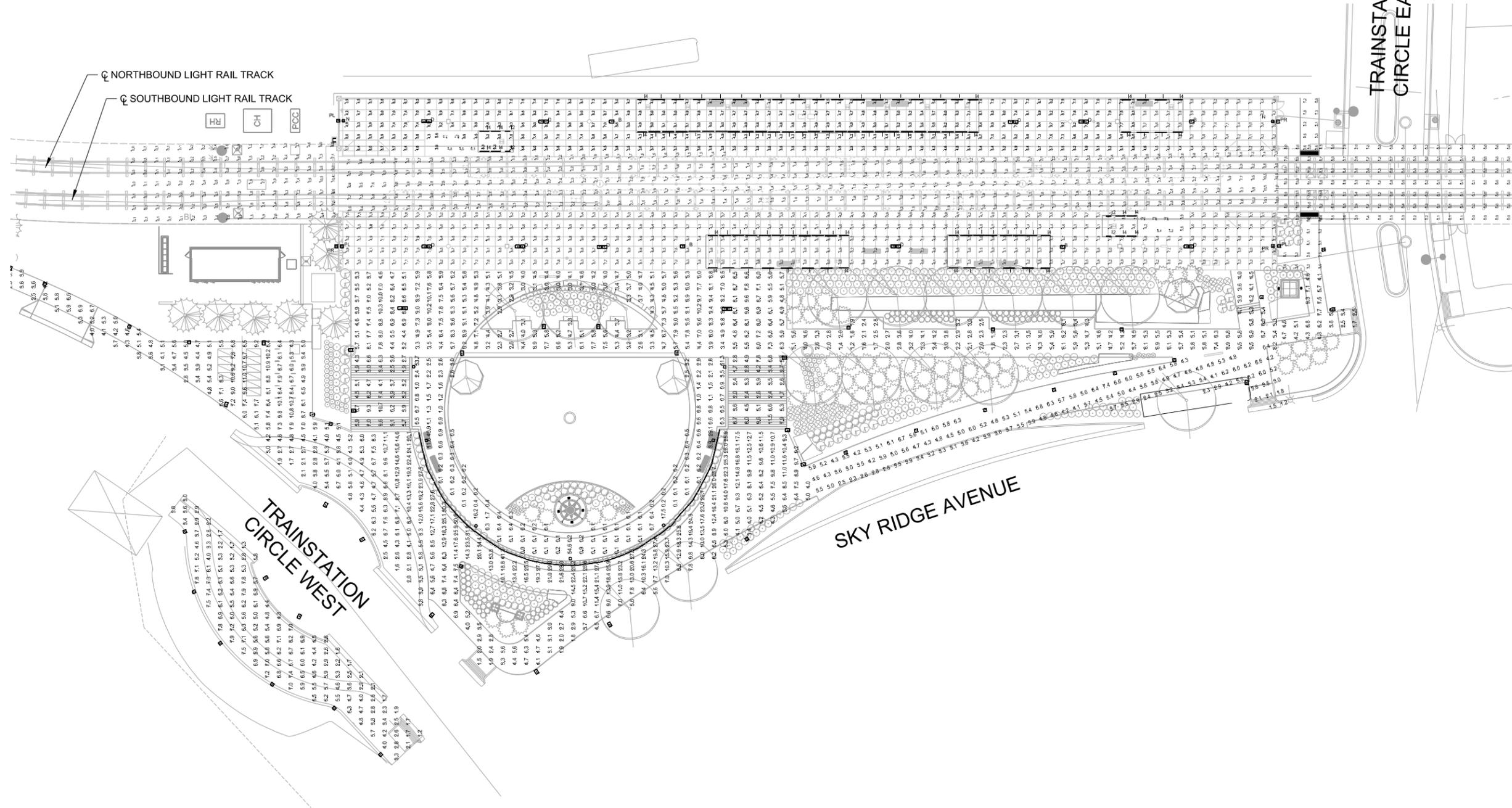
WSP | PARSONS BRINCKERHOFF
 555 17th STREET, SUITE 500
 DENVER, CO 80202
 P: 303.832.9091

SITE FURNISHINGS

SHEET 5 OF 12

RIDGEGATE SECTION 15 FILING 17

RIDGEGATE PDD, 4" AMENDMENT, PLANNING AREA C/M-U #2
2,438 ACRES
SP16-09R
(SKY RIDGE STATION)



RTD FasTracks
Southeast Rail Extension
1560 BROADWAY, SUITE 700
DENVER, COLORADO 80202
(303) 299-6990

Balfour Beatty
555 17th STREET, SUITE 400
DENVER, CO 80202

WSP | PARSONS BRINCKERHOFF
555 17th STREET, SUITE 500
DENVER, CO 80202
P: 303.832.9091

LIGHTING PHOTOMETRICS

SHEET 6 OF 12

RIDGEGATE SECTION 15 FILING 17

RIDGEGATE PDD, 4" AMENDMENT, PLANNING AREA C/M-U #2
2.438 ACRES
SP16-09R
(SKY RIDGE STATION)

WARP9™ LED with PicoEmitter™
Small & Large Luminaire

FEATURES

- LED PicoEmitter™ technology with up to 25% boost in lumen output
- Patented design, incorporating visual stealth technology
- Sealed optical chamber, IP-66 rated

Features exclusive w/HUBB technology

- Wireless system with a 0 - 10V dimming interface with a dimming range of 10 - 100%



WHP9LE-LED
WHP9SE-LED

W/HUBB

ORDERING INFORMATION (Example)

15A	WPS1E35	60L5K120	SG	SF/A-25	PRA12-5125SA/SG
-----	---------	----------	----	---------	-----------------

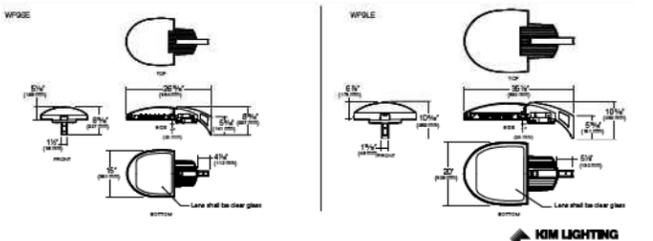
MOUNTING	ENK	WPS1E35	FEATURE FINISH	FEATURE OPTIONS	POLE
15A	1 Arm Side ML	0.52	BL Black	NFO Neighbor Friendly Optic	See Arms & Pole Selection Guide at www.kimlighting.com for pole EPA & ordering.
25B	2 Arm Side ML	1.04	DB Dark Bronze	SF Single Face for 120, 277, 347V	
25L	2 Arm Side ML	0.82	SG Stealth Gray	DF Double Face for 200, 240, 480V	
35Y	3 Arm Side ML	1.3	PS Platinum Silver	A-25 Photocell Replace	
35Y	3 Arm Side ML	1.3	WH White	A-30 120V Photocell Button	
45C	4 Arm Side ML	1.5	CC Custom Color*	A-31 200V Photocell Button	
45C	4 Arm Side ML	1.5	WH White	A-32 240V Photocell Button	
45C	4 Arm Side ML	1.5	PS Platinum Silver	A-33 277V Photocell Button	
45C	4 Arm Side ML	1.5	SG Stealth Gray	A-35 347V Photocell Button	
45C	4 Arm Side ML	1.5	WH White	A-34 480V Photocell Button	
45C	4 Arm Side ML	1.5	CC Custom Color*	L5 Polycarbonate Lens†	
45C	4 Arm Side ML	1.5	WH White	YL Tempe-Resistant Lens†	
45C	4 Arm Side ML	1.5	PS Platinum Silver	WIH-IM In-Fixture wireless control module	
45C	4 Arm Side ML	1.5	SG Stealth Gray	SCL Occupancy Sensor up to 16 ft.	
45C	4 Arm Side ML	1.5	WH White	SCH Occupancy Sensor up to 30 ft.	
45C	4 Arm Side ML	1.5	SG Stealth Gray	SWSF-15A 1 fixture side mt.	
45C	4 Arm Side ML	1.5	WH White	SWSF-25B 2 fixtures side mt. 180°	
45C	4 Arm Side ML	1.5	PS Platinum Silver	SWSF-25L 2 fixtures side mt. 90°	
45C	4 Arm Side ML	1.5	SG Stealth Gray	SWSF-35T 3 fixtures side mt. 120°	
45C	4 Arm Side ML	1.5	WH White	SWSF-45C 4 fixtures side mt. 90°	
45C	4 Arm Side ML	1.5	CC Custom Color*	SWSF-45C 4 fixtures side mt. 90°	
45C	4 Arm Side ML	1.5	WH White	SWSF-45C 4 fixtures side mt. 90°	

ELECTRICAL MODULES

Source	Color Temperature	Wattage
60L 60TDC, 65W for WPS2E	2K 580nm Amber	120 120V
120L 120LED, 120W for WPS2E	3K 3000K	208 208V
120L 120LED, 120W for WPS2E	4K 4200K	240 240V
120L 120LED, 120W for WPS2E	5K 5100K	277 277V
		347 347V
		480 480V

FINISH
Super TGIC powder coat paint over a titanated zirconium conversion coating.

FINISH
TGIC powder coat paint over a titanated zirconium conversion coating.



KIM LIGHTING
Low Level Floodlight; Concealed Lens, Symmetric Downward Light Throw, Wall Mounted
revision 3/14/08 • llf50.pdf

LLF-50
Type: F
Job: SERE SKYRIDGE STATION
Page: 2 of 3



Fixture
Catalog number includes housing and junction box, optical system, and door frame with standard tempered glass lens.

Electrical Module
HPS = High Pressure Sodium
PMH = Pulse Start Metal Halide
PL = Compact Fluorescent

Lamp Lamp Line
Watts Type Volts
100 HPS 120

Finish
Color: Black² Dark Bronze Light Gray Stealth Gray³ Platinum Silver White Custom Color⁴
Cat. No.: BL DB LG SG PS WH CC
²Specify black for minimum fixture brightness.
³Custom colors subject to additional charges, minimum quantities and extended lead times. Consult representative. Custom color descriptor: COLOR TBD

Cat. No.: LLF-50
Concealed Lens, Symmetric Downward Light Throw, Wall Mounted.
Arrows indicate main thrust of light distribution, in elevation.

Cat. Nos. for Electrical Modules available:

High Pressure Sodium	Pulse Start Metal Halide	Compact Fluorescent
<input type="checkbox"/> 50HPS120	<input type="checkbox"/> 70PMH120	<input type="checkbox"/> 42PL ¹
<input type="checkbox"/> 50HPS208	<input type="checkbox"/> 100PMH120	
<input type="checkbox"/> 50HPS240	<input type="checkbox"/> 100PMH208	
<input type="checkbox"/> 50HPS277	<input type="checkbox"/> 100PMH240	
	<input type="checkbox"/> 100PMH277	
	<input type="checkbox"/> 100PMH347	

Lamp Lamp Line
Watts Type Volts
50 PMH 277

Socket
Medium Base Medium Base GX24q-4 Base

ANSI Ballast Type
M-98 M-90

¹42PL operates one 26, 32, or 42 watt lamp at 120 thru 277 volts.

KIM LIGHTING
Compact Floodlights
revision 11/13/14 • kl_cfl_spec.pdf

CFL
Type:
Job:
Page: 2 of 6



Fixture
Cat. No. designates CFL fixture and beam pattern.

Single fixture EPA:
0.3 (45° tilt)
0.5 (Face on)

Beam Pattern:
Wide Flood
Narrow Spot

Electrical Module
PMH = Pulse Start Metal Halide
INC = Incandescent
HAL = Halogen
PL = Compact Fluorescent
LED = Light Emitting Diodes

Lamp Lamp Line
Watts Type Volts
50 PMH 277

Socket
GX23-2 2-pin Base GX24q-3 4-pin Base N/A

Cat. Nos. for Electrical Modules available:

Pulse Start Metal Halide	Incandescent	Halogen	Compact Fluorescent	LED
<input type="checkbox"/> 50PMH120	<input type="checkbox"/> 60INC120	<input type="checkbox"/> 150HAL120	<input type="checkbox"/> 13PL120 ¹	<input type="checkbox"/> 27L2KUV ^{2,3} (32.4W)
<input type="checkbox"/> 50PMH277			<input type="checkbox"/> 13PL277 ¹	<input type="checkbox"/> 27L3KUV ^{2,3} (32.4W)
				<input type="checkbox"/> 27L4KUV ^{2,3} (32.4W)
				<input type="checkbox"/> 27L5KUV ^{2,3} (32.4W)

Lamp
E-17, Clear E-17, Clear T-10, Coated T-4, Clear

Socket
Medium Base Medium Base Medium Base Mini-can Base

ANSI Ballast
M-110 M-98

Lamp
Twin Tube, Coated Triple Tube, Coated LED

Socket
GX23-2 2-pin Base GX24q-3 4-pin Base N/A

¹For CFL only.
²27L = 27 LEDs, 2K = 580 nm Amber, 3K = 3000 Kelvin, 4K = 4200 Kelvin, 5K = 5100 Kelvin.
³UV = Universal voltage 120 through 277V electronic driver
⁴For 32.4W, Max Amps. are: 27 for 120V, 16 for 208V, 14 for 240V, 12 for 277V.
⁵42PL operates 26, 32, and 42 watt lamps at 120 thru 277 volts (50-60 Hz).

Finish
TGIC powder coat paint over a titanated zirconium conversion coating.

Color: Black Dark Bronze Light Gray Stealth Gray³ Platinum Silver White Custom Color⁴
Cat. No.: BL DB LG SG PS WH CC
²Specify black for minimum fixture brightness.
³Custom colors subject to additional charges, minimum quantities and extended lead times. Consult representative. Custom color descriptor: COLOR TBD

RTD FasTracks
Southeast Rail Extension
1560 BROADWAY, SUITE 700
DENVER, COLORADO 80202
(303) 299-6990

Balfour Beatty
555 17th STREET, SUITE 400
DENVER, CO 80202

WSP | PARSONS BRINCKERHOFF
555 17TH STREET, SUITE 500
DENVER, CO 80202
P: 303.832.9091

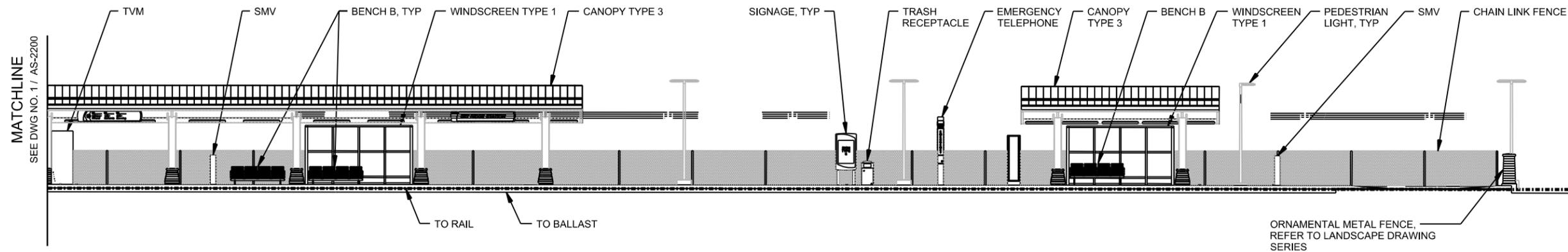
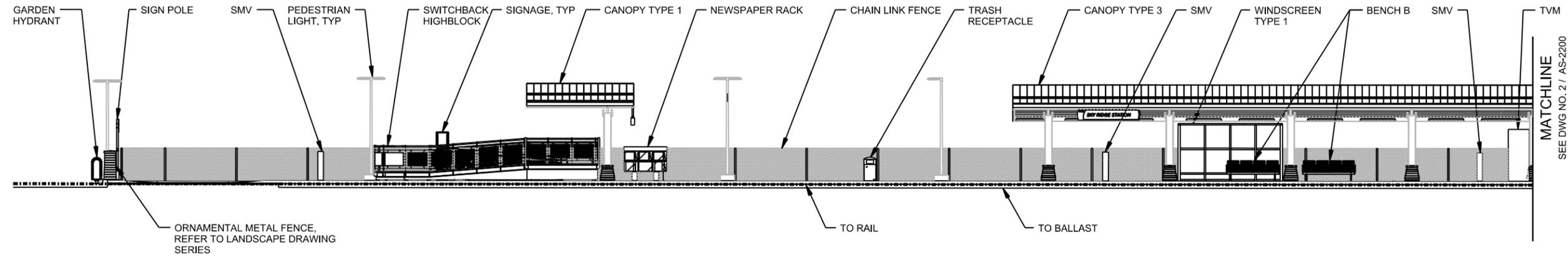
LIGHTING FIXTURES

SHEET 7 OF 12

Southeast Rail Extension - Sky Ridge Station

RIDGEGATE SECTION 15 FILING 17

RIDGEGATE PDD, 4" AMENDMENT, PLANNING AREA C/M-U #2
 2.438 ACRES
 SP16-09R
 (SKY RIDGE STATION)



RTD FasTracks
Southeast Rail Extension
 1560 BROADWAY, SUITE 700
 DENVER, COLORADO 80202
 (303) 299-6990

Balfour Beatty
 555 17TH STREET, SUITE 400
 DENVER, CO 80202

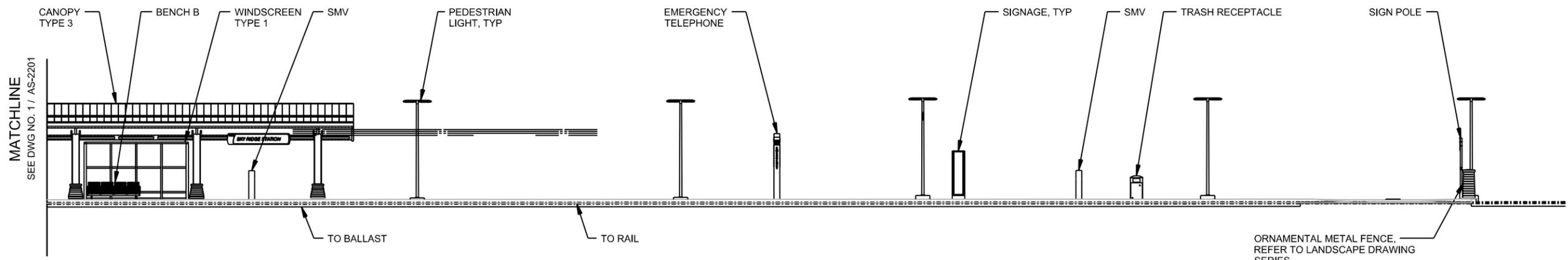
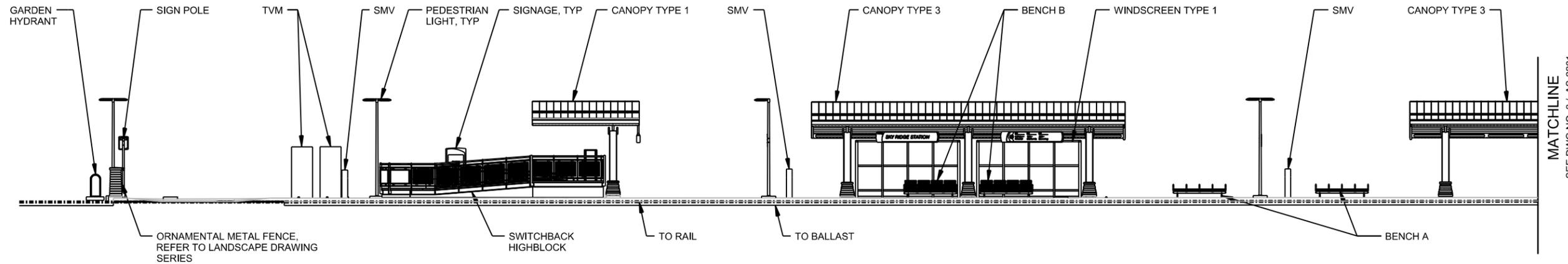
WSP | PARSONS BRINCKERHOFF
 555 17TH STREET, SUITE 500
 DENVER, CO 80202
 P: 303.832.9091

EAST PLATFORM ELEVATION

SHEET 8 OF 12

RIDGEGATE SECTION 15 FILING 17

RIDGEGATE PDD, 4" AMENDMENT, PLANNING AREA C/M-U #2
 2,438 ACRES
 SP16-09R
 (SKY RIDGE STATION)



MATCHLINE
SEE DWG NO. 2 / AS-2201

MATCHLINE
SEE DWG NO. 1 / AS-2201

RTD FasTracks
Southeast Rail Extension
 1560 BROADWAY, SUITE 700
 DENVER, COLORADO 80202
 (303) 299-6990

Balfour Beatty
 555 17TH STREET, SUITE 400
 DENVER, CO 80202

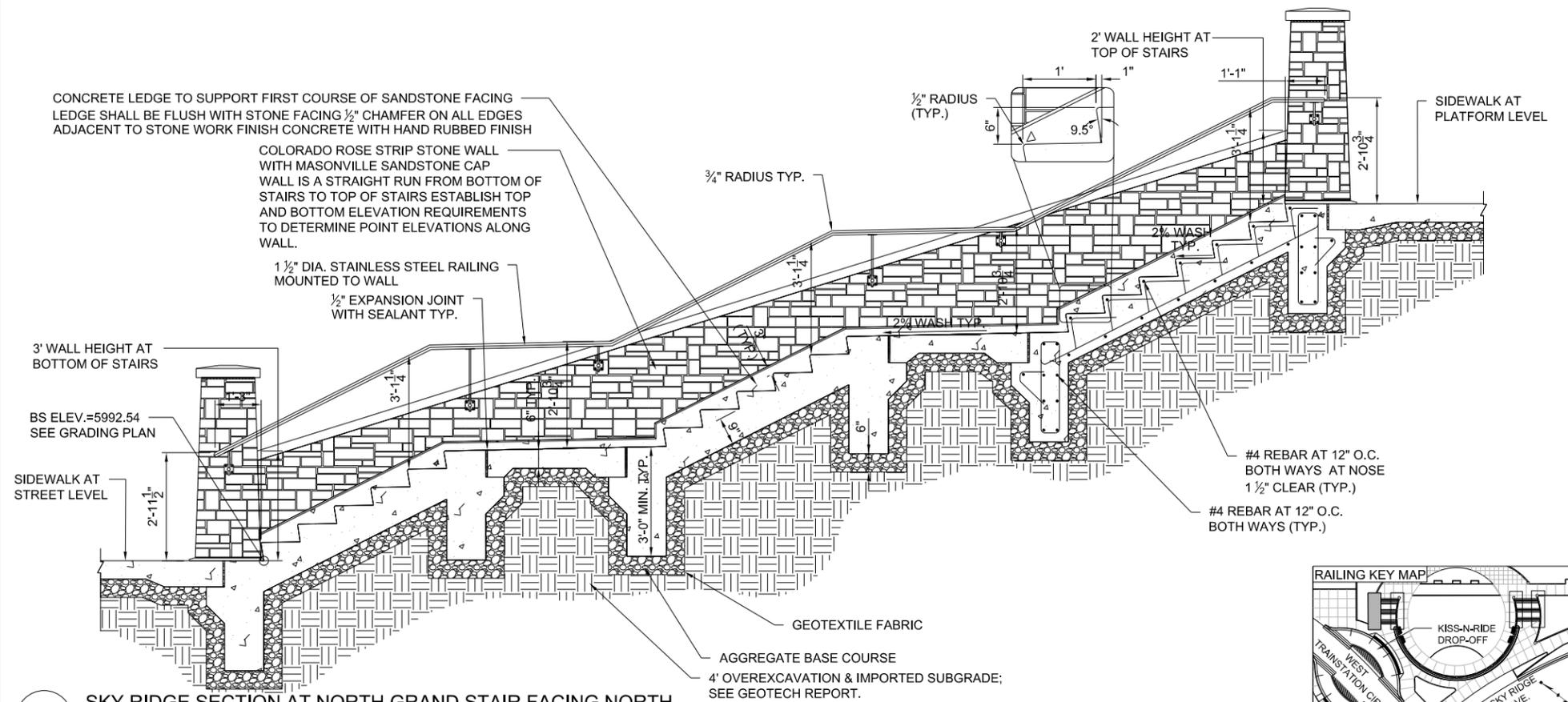
WSP | PARSONS BRINCKERHOFF
 555 17TH STREET, SUITE 500
 DENVER, CO 80202
 P: 303.832.9091

WEST PLATFORM ELEVATION

SHEET 9 OF 12

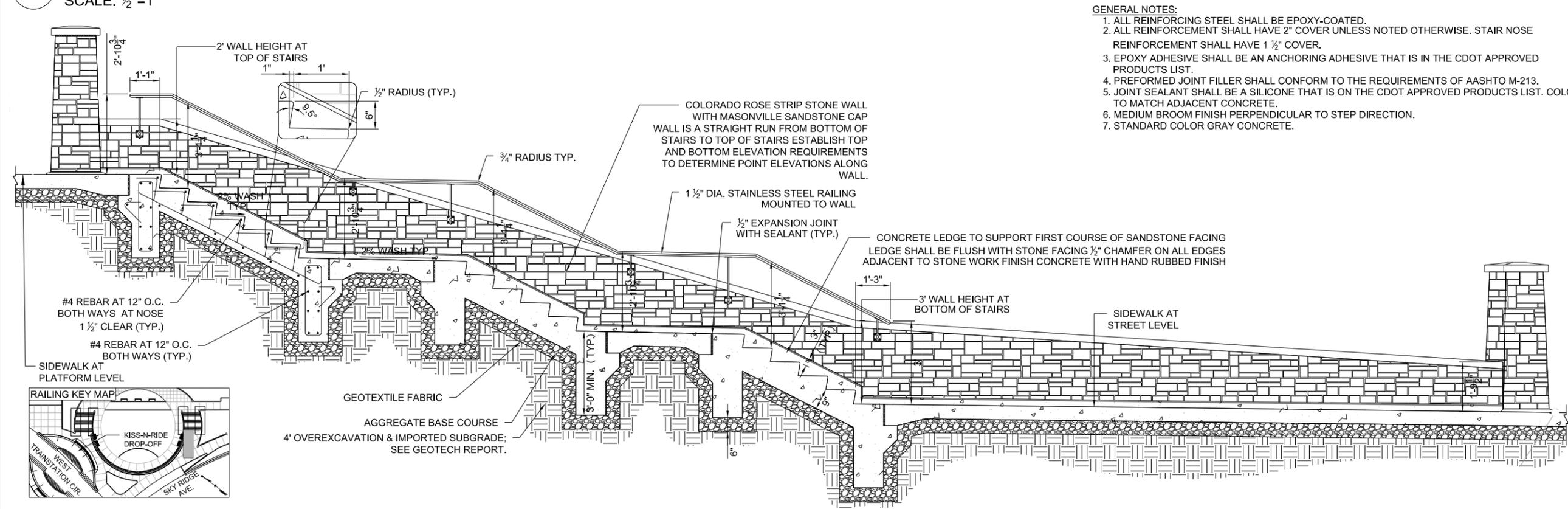
RIDGEGATE SECTION 15 FILING 17

RIDGEGATE PDD, 4" AMENDMENT, PLANNING AREA C/M-U #2
 2.438 ACRES
 SP16-09R
 (SKY RIDGE STATION)



- GENERAL NOTES:**
1. ALL REINFORCING STEEL SHALL BE EPOXY-COATED.
 2. ALL REINFORCEMENT SHALL HAVE 2" COVER UNLESS NOTED OTHERWISE. STAIR NOSE REINFORCEMENT SHALL HAVE 1 1/2" COVER.
 3. EPOXY ADHESIVE SHALL BE AN ANCHORING ADHESIVE THAT IS IN THE CDOT APPROVED PRODUCTS LIST.
 4. PREFORMED JOINT FILLER SHALL CONFORM TO THE REQUIREMENTS OF AASHTO M-213.
 5. JOINT SEALANT SHALL BE A SILICONE THAT IS ON THE CDOT APPROVED PRODUCTS LIST. COLOR TO MATCH ADJACENT CONCRETE.
 6. MEDIUM BROOM FINISH PERPENDICULAR TO STEP DIRECTION.
 7. STANDARD COLOR GRAY CONCRETE.

1 SKY RIDGE SECTION AT NORTH GRAND STAIR FACING NORTH
 SCALE: 1/2"=1'



- GENERAL NOTES:**
1. ALL REINFORCING STEEL SHALL BE EPOXY-COATED.
 2. ALL REINFORCEMENT SHALL HAVE 2" COVER UNLESS NOTED OTHERWISE. STAIR NOSE REINFORCEMENT SHALL HAVE 1 1/2" COVER.
 3. EPOXY ADHESIVE SHALL BE AN ANCHORING ADHESIVE THAT IS IN THE CDOT APPROVED PRODUCTS LIST.
 4. PREFORMED JOINT FILLER SHALL CONFORM TO THE REQUIREMENTS OF AASHTO M-213.
 5. JOINT SEALANT SHALL BE A SILICONE THAT IS ON THE CDOT APPROVED PRODUCTS LIST. COLOR TO MATCH ADJACENT CONCRETE.
 6. MEDIUM BROOM FINISH PERPENDICULAR TO STEP DIRECTION.
 7. STANDARD COLOR GRAY CONCRETE.

2 SKY RIDGE SECTION AT SOUTH GRAND STAIR FACING SOUTH
 SCALE: 1/2"=1'

RTD FasTracks
Southeast Rail Extension
 1560 BROADWAY, SUITE 700
 DENVER, COLORADO 80202
 (303) 299-6990

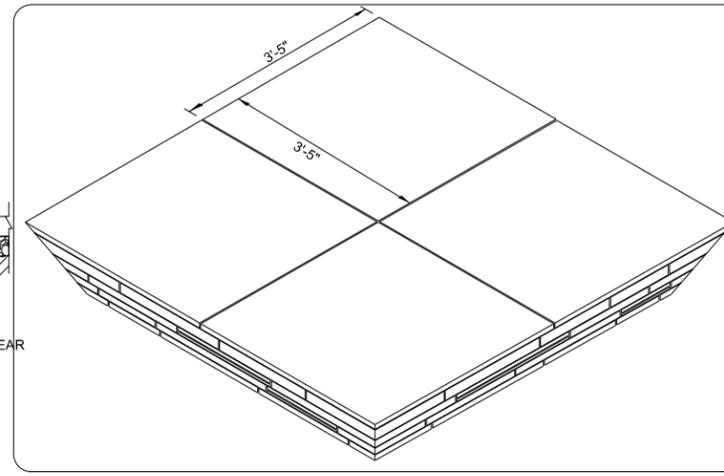
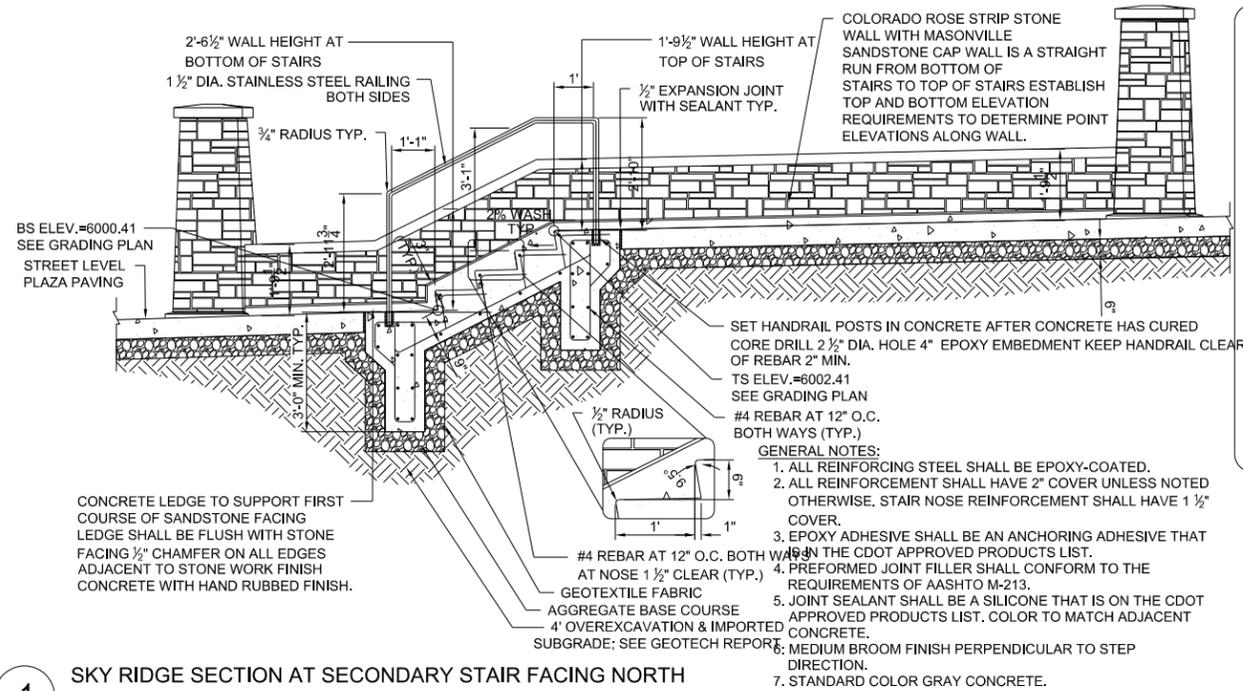
Balfour Beatty
 555 17TH STREET, SUITE 400
 DENVER, CO 80202

WSP | PARSONS BRINCKERHOFF
 555 17TH STREET, SUITE 500
 DENVER, CO 80202
 P: 303.832.9091

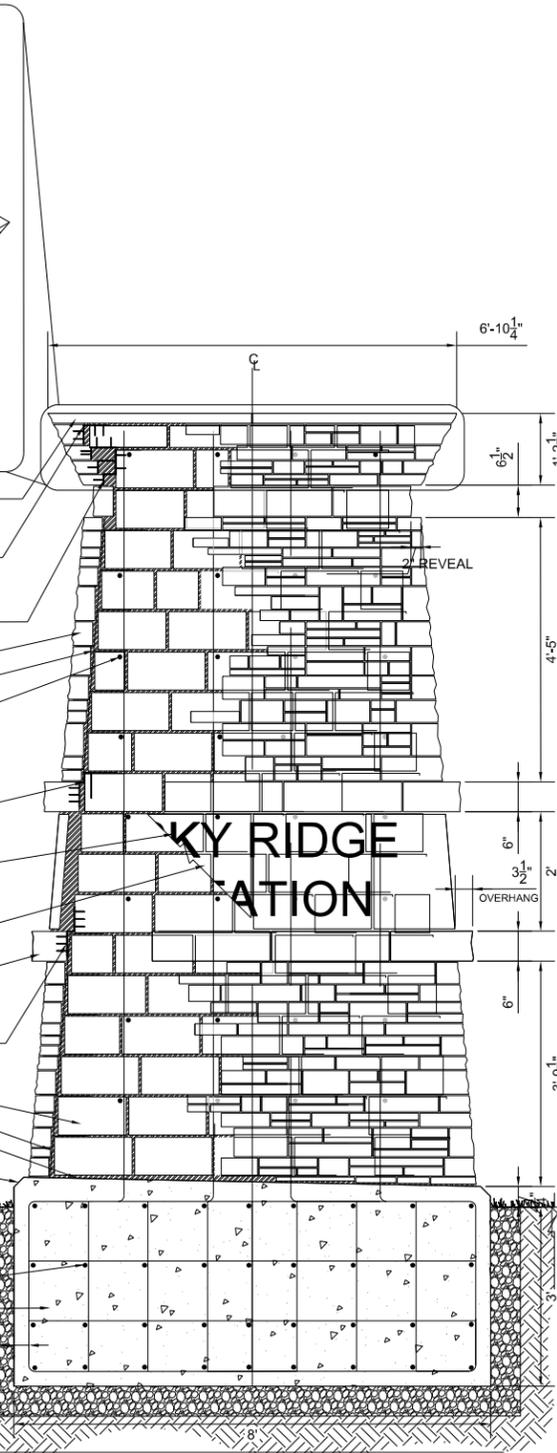
GRAND STAIR DETAILS

RIDGEGATE SECTION 15 FILING 17

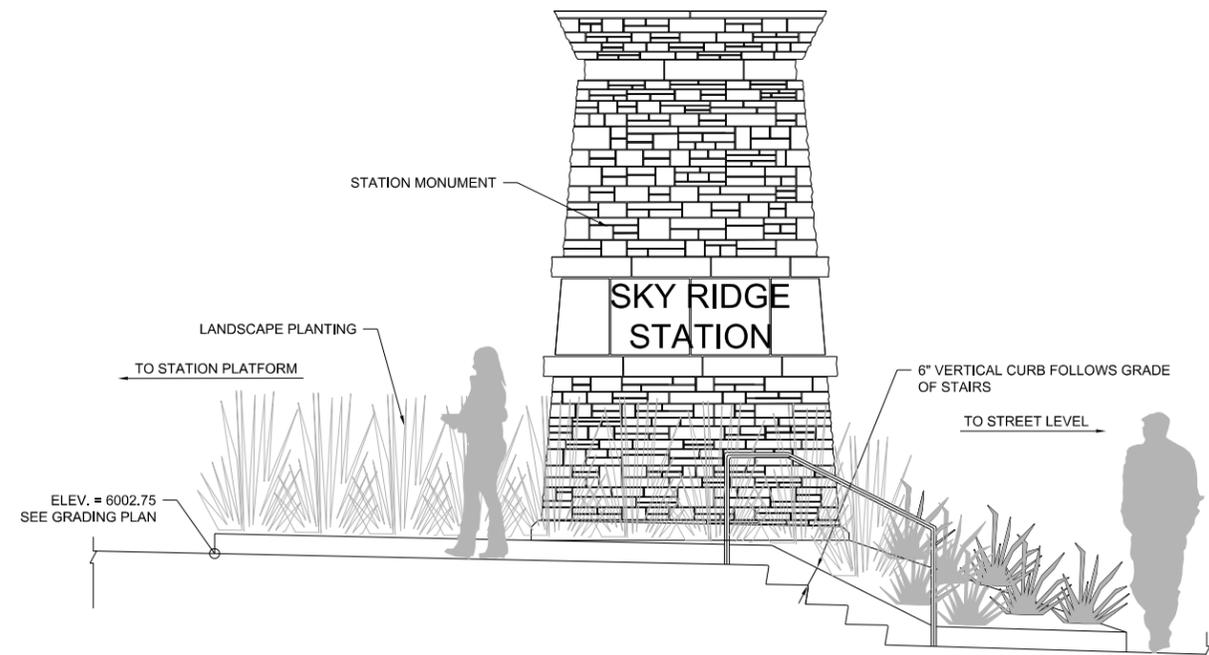
RIDGEGATE PDD, 4" AMENDMENT, PLANNING AREA C/M-U #2
 2.438 ACRES
 SP16-09R
 (SKY RIDGE STATION)



2" RED SANDSTONE CAP RUNS CONTINUOUS ALONG TOP OF MONUMENT, ALL VISIBLE EDGES SHALL HAVE SPLIT FACE FINISH, NO CLEAN SAWN VISIBLE EDGES WILL BE ACCEPTED.
 1/8" x 1" STEEL ANGLE STRAP FABRICATE AS NEEDED LOCATE AT 6" O.C. 2 MIN. PRE STONE, SECURE TO BACK OF STONE AND CMU CORE WITH 3/16" x 1 1/4" HILTI KWIK-CON II+ FASTENERS OR APPROVED EQUAL, 4 PER STRAP.
 1/8" x 1" STEEL Z-TIES FABRICATE AS NEEDED LOCATE AT 6" O.C. 2 MIN. PRE STONE, SECURE TO BACK OF STONE AND CMU CORE WITH 3/16" x 1 1/4" HILTI KWIK-CON II+ FASTENERS OR APPROVED EQUAL, 2 PER STRAP.
 4" COLORADO ROSE STRIPSTONE FACING
 ADJUSTABLE STONE ANCHORING SYSTEM
 SUBMIT PRODUCT INFORMATION FOR APPROVAL
 #5 AT 18" O.C. EACH WAY. UTILIZE NOCK OUTS IN CMU BLOCKS FOR REBAR PLACEMENT AS SHOWN.
 1/8" x 1" x 1 1/2" STEEL STRAP LOCATE AT 6" O.C. 2 MIN. PRE STONE, SECURE TO BACK OF STONE WITH 3/16" x 1 1/4" HILTI KWIK-CON II+ FASTENERS OR APPROVED EQUAL, 3 PER STRAP. EMBED LOOP IN MORTARED CELL OF CMU.
 7" ARIAL FONT STAINLESS STEEL LETTING MOUNT TO MONUMENT WITH STAINLESS STEEL STUDS WELDED TO BACK OF LETTERING, EPOXY EMBED IN MONUMENT CORE 4" MIN PENETRATION.
 2" THICK COLORADO ROSE SANDSTONE SLAB CUT TO FIT MONUMENT 4 SLABS PER SIDE CENTER MIDDLE JOINT ON MONUMENT OUTSIDE JOINTS SHALL BE SPACED EQUALLY FROM CENTER OF MONUMENT.
 COLORADO ROSE SANDSTONE LEDGE 8" MINIMUM LENGTHS
 1/8" x 1" x 1 1/2" STEEL STRAP LOCATE AT 6" O.C. 2 MIN. PRE STONE, SECURE TO BACK OF STONE AND CMU CORE WITH 3/16" x 1 1/4" HILTI KWIK-CON II+ FASTENERS OR APPROVED EQUAL, 6 PER STRAP.



1 SKY RIDGE SECTION AT SECONDARY STAIR FACING NORTH



2 SKY RIDGE ELEVATION AT SECONDARY STAIR FACING SOUTH

3 SKY RIDGE MONUMENT

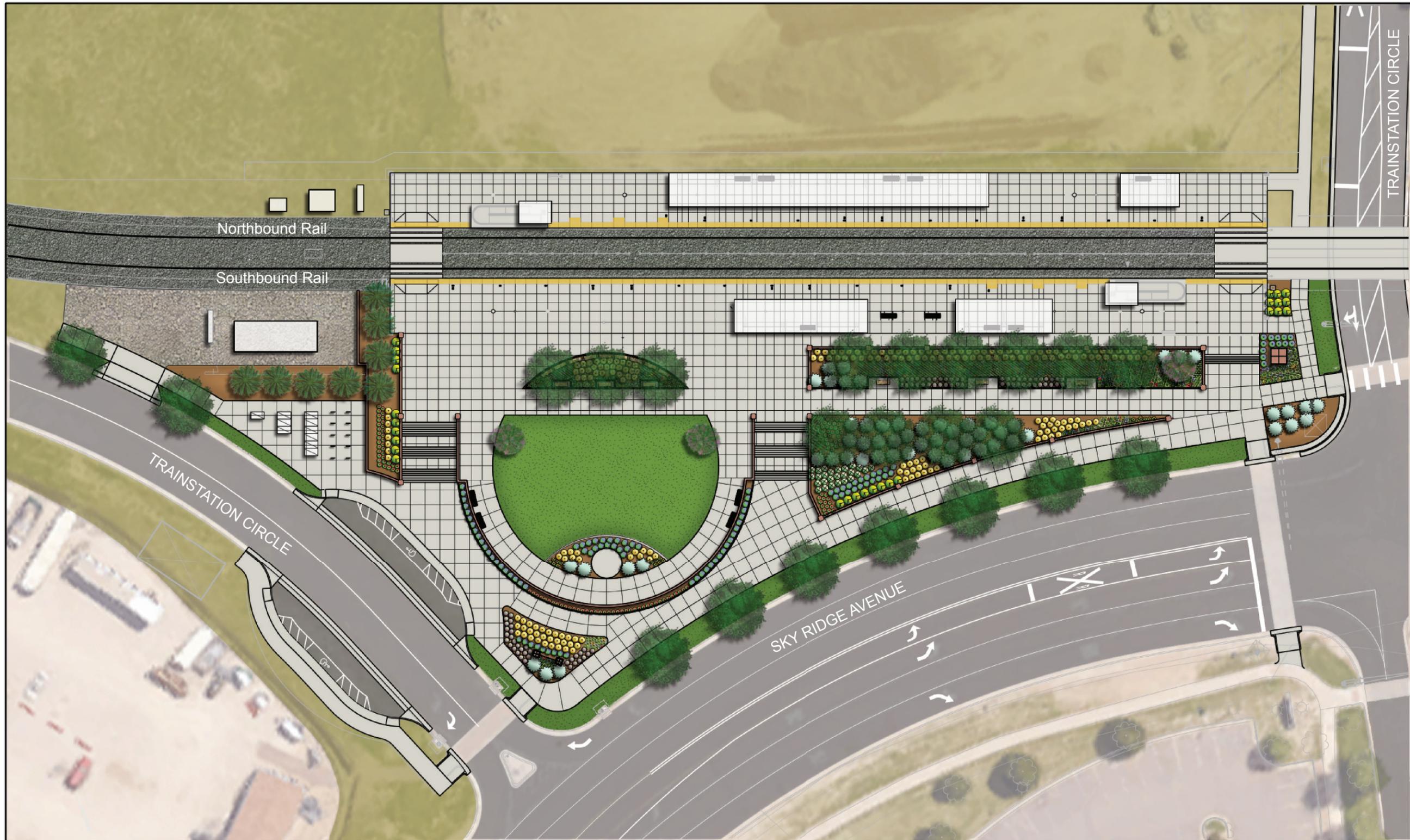
RTD FasTracks
Southeast Rail Extension
 1560 BROADWAY, SUITE 700
 DENVER, COLORADO 80202
 (303) 299-6990

Balfour Beatty
 555 17TH STREET, SUITE 400
 DENVER, CO 80202

WSP | PARSONS BRINCKERHOFF
 555 17TH STREET, SUITE 500
 DENVER, CO 80202
 P: 303.832.9091

SECONDARY STAIR DETAILS

SHEET 12 OF 12



RTD FasTracks
Southeast Rail Extension
 1560 BROADWAY, SUITE 700
 DENVER, COLORADO 80202
 (303) 299-6990

Balfour Beatty
 555 17TH STREET, SUITE 400
 DENVER, CO 80202

WSP | PARSONS BRINCKERHOFF
 555 17TH STREET, SUITE 500
 DENVER, CO 80202
 P: 303.832.9091

IRON HORSE ARCHITECTS
 RESPONSIBLE DESIGN
 475 17TH STREET, SUITE 720
 DENVER, CO 80202

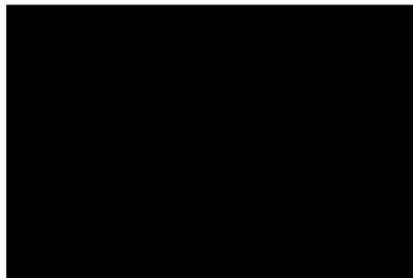


BRITINA
 design group
 1615 CALIFORNIA STREET, SUITE 411
 DENVER, CO 80202
 P: 303.456.2887



**Pre-Finished Standing Seam
Berridge Shasta White**

Canopy Roof



**Painted/Pre-Finished Metal
Black RAL 9005**

Windscreen Structure and Panels,
Railings, Benches, Light Poles,
Trash Receptacles

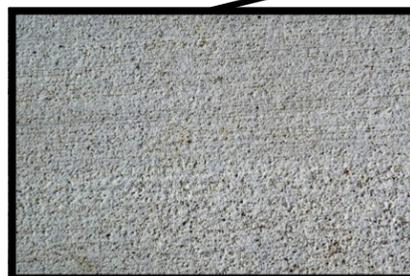


**Painted/Pre-Finished Metal
Tnemec White 00WH**

Canopy Structure
VMS
Canopy Deck



**Tactile Warning Strip
Federal Standard #33538**



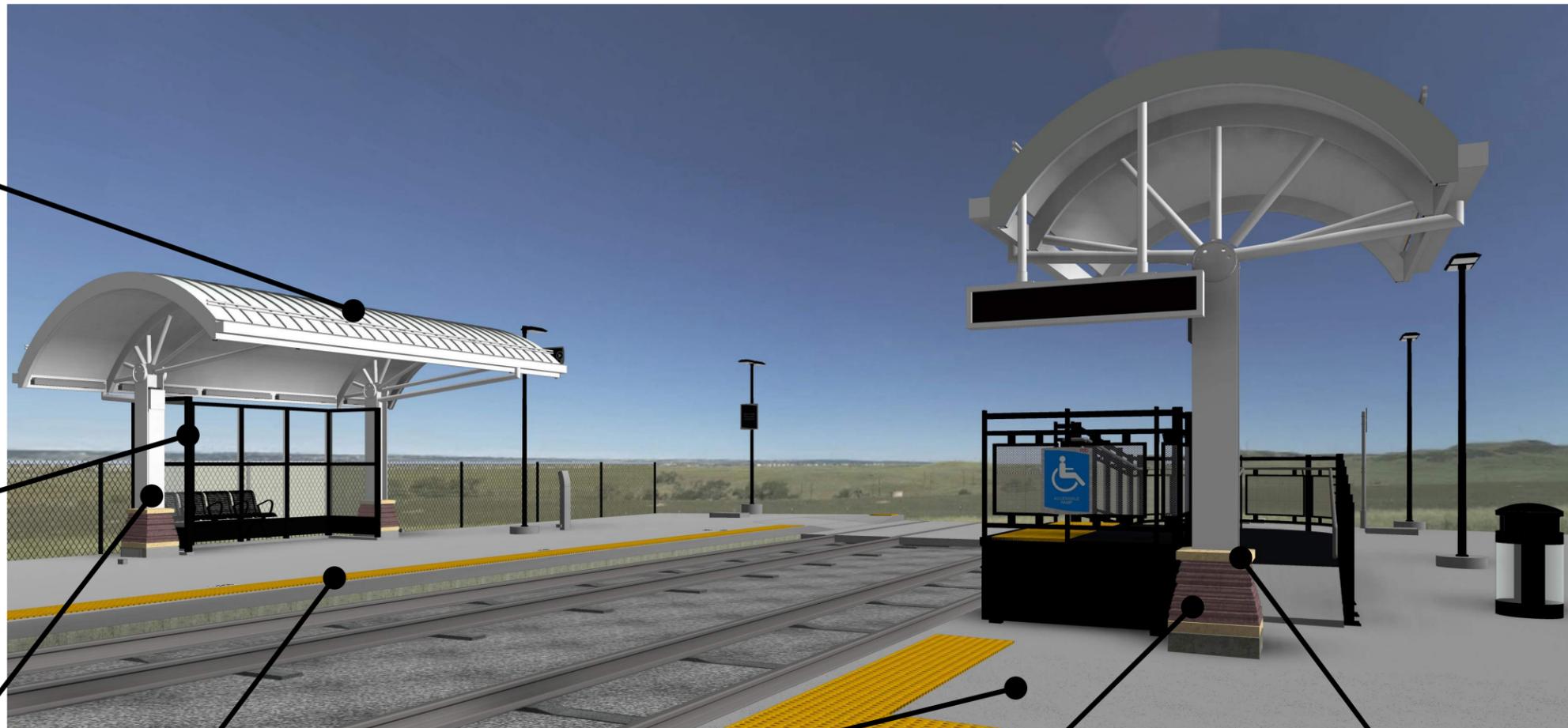
**Concrete
Medium Broom Finish**
Platform, Plaza, Sidewalks



**Stone Veneer
Colorado Rose Sandstone, Buff Finish**
Canopy Column Bases



**Stone Veneer
Colorado Sandstone, Buff Finish**
Canopy Column Bases



RTD FasTracks
**Southeast Rail
Extension**
1560 BROADWAY, SUITE 700
DENVER, COLORADO 80202
P: 303.299.6890

Balfour Beatty
555 17TH STREET, SUITE 400
DENVER, CO 80202

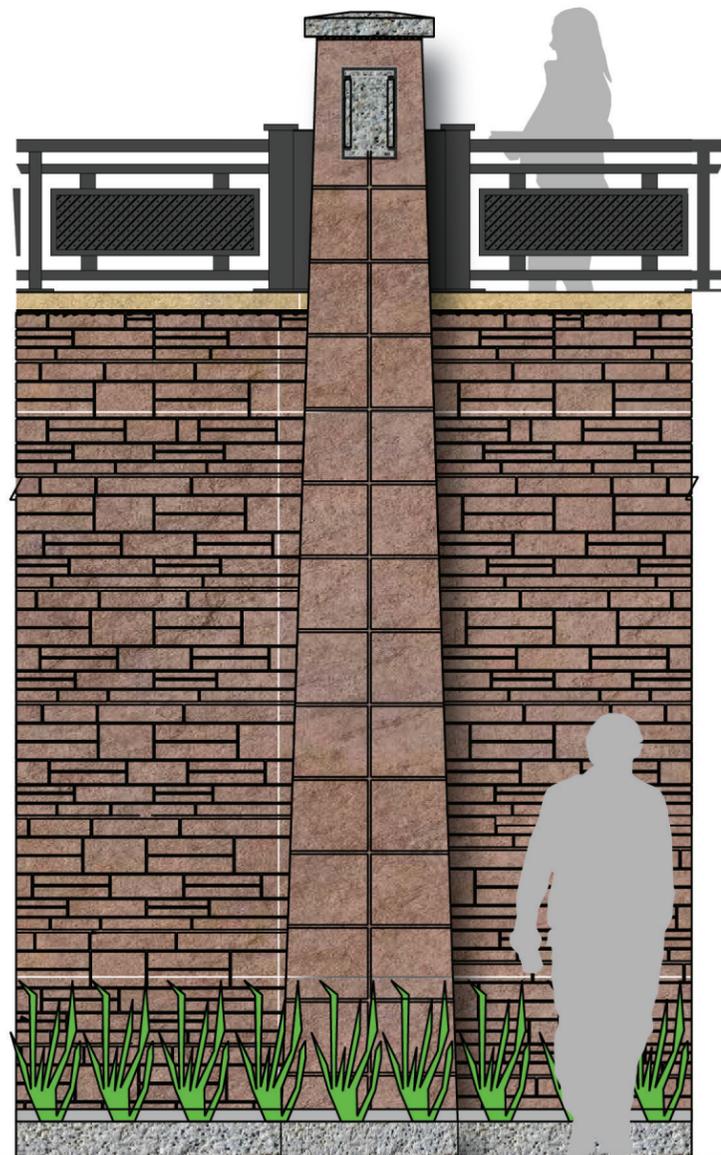
**WSP | PARSONS
BRINCKERHOFF**
555 17TH STREET, SUITE 500
DENVER, CO 80202
P: 303.832.9091

IRON HORSE
ARCHITECTS
RESPONSIBLE DESIGN
475 17TH STREET, SUITE 720
DENVER, CO 80202

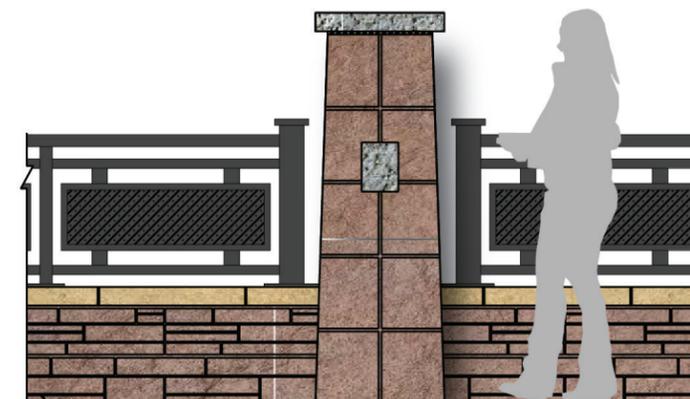


BRITINA
design group
1615 CALIFORNIA STREET, SUITE 411
DENVER, CO 80202
P: 303.456.2887

Canopy and Platform Materials
Southeast Rail Extension - Sky Ridge Station



Sky Ridge Feature Wall - Street Level Elevation



Sky Ridge Feature Wall - Platform Level Elevation

RTD FasTracks
Southeast Rail Extension
 1560 BROADWAY, SUITE 700
 DENVER, COLORADO 80202
 (303) 299-6890

Balfour Beatty
 555 17TH STREET, SUITE 400
 DENVER, CO 80202

WSP | PARSONS BRINCKERHOFF
 555 17TH STREET, SUITE 500
 DENVER, CO 80202
 P: 303.832.9091

IRON HORSE ARCHITECTS
 RESPONSIBLE DESIGN
 475 17TH STREET, SUITE 720
 DENVER, CO 80202



BRITINA
 design group
 1615 CALIFORNIA STREET, SUITE 411
 DENVER, CO 80202
 P: 303.456.2887

Wall Details



CITY OF LONE TREE

STAFF REPORT

TO: City of Lone Tree Planning Commission
FROM: Kelly First, Community Development Director
DATE: April 6, 2016
FOR: April 12, 2016 Planning Commission Meeting
SUBJECT: Lone Tree City Center Station
Site Improvement Plan SP16-10R

Owner

RidgeGate Investments, Inc
Keith Simon
10270 Commonwealth Street
Lone Tree, CO 80124
720-279-2581

Representative

Regional Transportation District
Nathan Herman
1600 Blake Street
Denver, CO 80202
303-628-9000

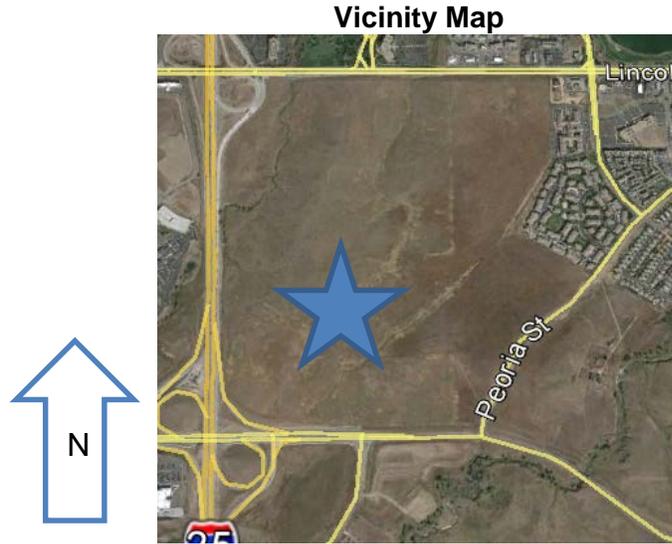
Planning Commission Meeting Date: April 12, 2016
City Council Meeting Date: April 19, 2016

A. REQUEST:

Approval of a Site Improvement Plan (SIP) for the Lone Tree City Center Light Rail Station.

LOCATION:

The site is located east of I-25 and approximately 1,700 feet north of RidgeGate Parkway.



B. SITE CHARACTERISTICS:

The site has no distinctive topographical or vegetative features. The site and surrounding area are currently undeveloped. As part of the Southeast Extension project, RTD prepared an Environmental Assessment, which resulted in a Finding of No Significant Impact.

C. SERVICE PROVIDERS:

Water:	Parker Water District
Sanitation:	Parker Sanitation District
Police:	Lone Tree Police
Fire:	South Metro Fire Rescue Authority
Metro District:	Rampart Range Metropolitan District

D. BACKGROUND:

The 2.3 mile-long southeast extension project, from Lincoln Avenue to RidgeGate Parkway, is part of the RTD's 2004 voter-approved FasTracks Program. The Southeast Line includes 19.1 miles extending from I-25/Broadway to Lincoln Avenue, with an additional connection along I-225 from I-25 to Parker Road. Construction on the extension is expected to begin this spring with service beginning in 2019. When completed, residents will be able to travel from the RidgeGate station on the new R Line through Aurora to Peoria Station and connect to the commuter train to Denver International Airport. They will be able to travel from RidgeGate to Union Station on the E Line and from RidgeGate to 18th and California in downtown Denver on the F Line.

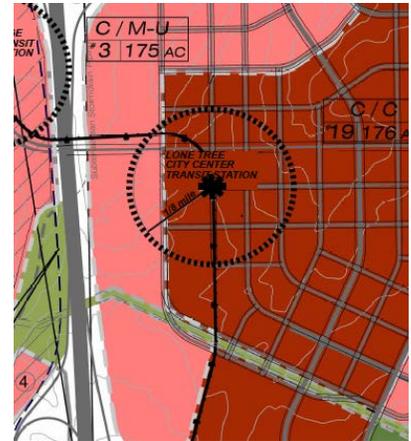
This SIP is one of three station SIPs that comprise the RTD Southeast Rail Extension project. Additional stations are Sky Ridge (SP16-09R) and RidgeGate (SP16-11R).

In January of 2015, City staff and members of the RTD design team presented an overview of the project to the Planning Commission and City Council. The purpose of the work sessions was to seek feedback about key design elements and address them prior to the project going out for bid, thereby reducing the need for potentially costly changes later on. Comments regarding aesthetic elements like canopies, walls, fencing and landscaping were provided and have been incorporated into SIP. In March, 2016, RTD again provided an overview of the stations to the Planning Commission and City Council, in anticipation of the SIPs coming before the City Council for action in April.

E. DESCRIPTION:

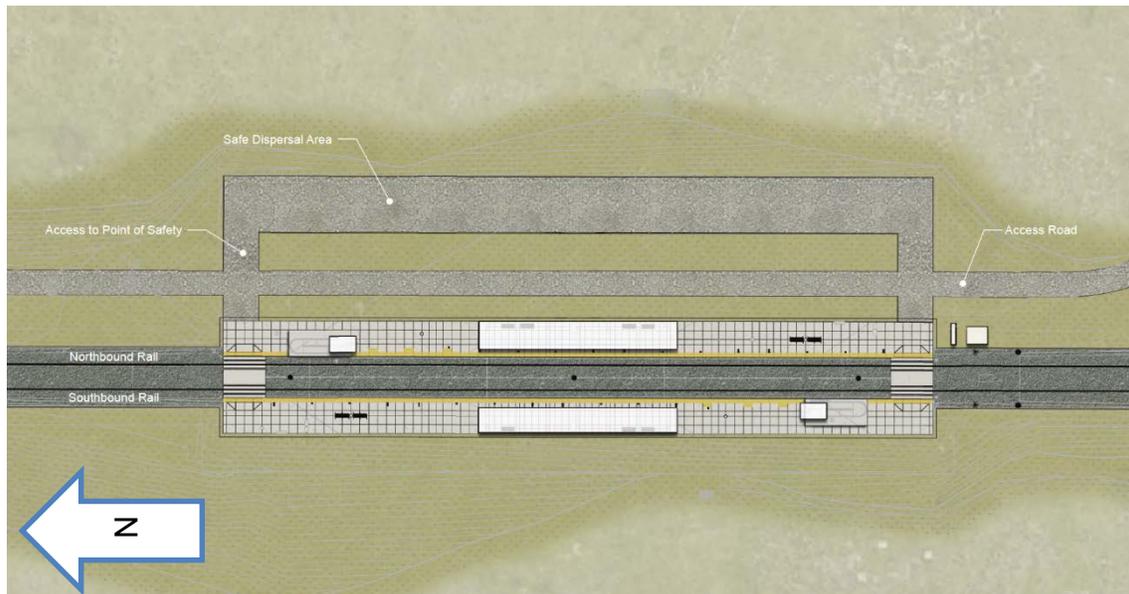
Zoning. The site is zoned Planned Development (PD) under the RidgeGate Planned Development 4th Amendment. The proposed use is in conformance with zoning, and is located within the City Center Planning Area #19.

A sub-area plan for the property is currently being developed by the owner, with participation by the City of Lone Tree staff. The sub-area plan will incorporate the station design and provide guidance for how the surrounding City Center area will be developed.



Site Design. The initial scope of this station is limited only to the platform on either side of the light rail tracks. Lighting, cameras and windscreens/benches will be provided on the platform to create a safe and inviting environment. Other station features including transition plazas, multi-modal interfaces and pedestrian amenities will be added as the adjacent areas develop. Build-out of the station as part of the future overall development of the City Center creates a unique opportunity to foster an integrated, compact, mixed-use environment.

Site Plan



F. REFERRALS:

The project was sent on referral to all key agencies and subscribers to the City's referral list. All responses of substance are attached.

G. STAFF FINDINGS AND RECOMMENDATION:

Staff finds that the application is in conformance with the SIP requirements of the Lone Tree Zoning Code, the Subdivision Code, the Comprehensive Plan, and the RidgeGate PDD, 4th Amendment.

Staff recommends the Planning Commission recommend approval of the SIP to City Council, subject to the following conditions:

1. Final approval of the Site Improvement Plan is subject to City of Lone Tree Public Works approval.

H. ATTACHMENTS:

1. Letter of Authorization
2. Development Application
3. Narrative & Statement of Design Intent
4. Referral Comments
5. SIP
6. Renderings

END

Letter of Authorization

Regarding Development Applications for Land Use Entitlements

City of Lone Tree
9220 Kimmer Drive #100
Lone Tree, CO 80124

RE: Property Address: Section 14, Township 6 South, Range 67 West of the Sixth Principal Meridian
Assessor's Parcel Number (SPN): _____

To Whom It May Concern:

I/We, the owner(s) of the above described real property, authorize Nathan Herman
of Regional Transportation District to act as an agent on my/our behalf for the purpose of
creating, filing and/or managing an application for Lone Tree City Center Site Improvement Plan
_____ (type of development or permit application).

The undersigned hereby certifies to being the fee owner(s) or legally authorized representative of the fee owner(s) of the real property described above.

Keith Simon, RidgeGate Investments, Inc.
(Print Name of Owner)

Keith Simon, vice President
(Signature of Owner or Authorized Representative)

State of Colorado
County of Douglas

The foregoing instrument was acknowledged before me this 17th day of March, 2016
by Keith D. Simon.

Elizabeth Matthews
(Notary's official signature)

5-12-2016
(Commission expiration date)

NOTARY SEAL

**ELIZABETH MATTHEWS
NOTARY PUBLIC
STATE OF COLORADO
NOTARY ID 20004014222
MY COMMISSION EXPIRES 05/12/2016**



CITY OF LONE TREE

Site Improvement Plan Project Narrative & Statement of Design Intent Template

Planning Division

9220 Kimmer Drive, Lone Tree, Colorado 80124

303.708.1818 | www.cityoflonetree.com

Project Name RTD – Lone Tree City Center Station

Project # SP16-10R

Project Location Section 14 approximately 1,700 feet north of RidgeGate Parkway

Date March 17, 2016

Project Narrative

[ARTICLE XXVII - Site Improvement Plan \(SIP\) Project Narrative](#). The SIP process is intended to provide for development that enhances the quality of life in the City by promoting high-quality design and a strong economy, and by fostering a sustainable and healthy community. The SIP process is required to ensure the development will be in conformance with the [Comprehensive Plan](#), the [Design Guidelines](#), applicable chapters of this [Code](#) and applicable Planned Developments and Sub-Area Plans.

Using this form or a separate page(s), the applicant shall provide a written narrative describing their project. Use the following outline (Sec. 16-27-60) as a guide when formulating your narrative – please disregard sections that do not apply to your project:

1. General information.

- a. Provide the subdivision name, filing number, planning area number when located in a Planned Development, lot and block number or street address and section, township and range if not in a subdivision, and name of project.

Section 14, Township 6 South, Range 67 West of the Sixth Principal Meridian
RidgeGate PDD 4th Amendment, Planning Area C/M-U #3
Lone Tree City Center Station

- b. Indicate zoning of the site and the zoning and current uses of adjacent land.

RidgeGate Planned Development District, 4th Amendment

The site and surrounding area are undeveloped.

2. Development impacts. Describe overall impacts of the proposed development on adjacent lands and methods for mitigating those impacts.

The proposed Lone Tree City Center station is part of RTD's Southeast Rail Extension Project. RTD prepared an Environmental Assessment (EA) for the project which resulted in a Finding of No Significant Impact (October 2014). The EA addressed impacts to the adjacent lands and mitigation measures. For the Lone Tree City Center Station, no impacts to adjacent lands were identified in the EA.

3. Compliance with Intent and Approval Standards. Describe how the development complies with the Intent (Section 16-27-10) and Approval Standards (Subsection 16-27-90(a)) of this Article.

The Lone Tree City Center station is in compliance with the City's Comprehensive Plan, the Design Guidelines, and the RidgeGate PDD, 4th Amendment. The station design is following the standards set forth by the RidgeGate development to ensure it will be integrated both functionally and aesthetically with adjacent development.

4. Development phasing. Describe the proposed development schedule and phases of development for all proposed construction.

Construction of the Southeast Rail Extension is expected to start in 2016 and be completed in 2019. No phasing of the project is planned.

5. Other project data.

- a. Total number of employees on maximum shift when known (for parking purposes).

There will be no employees on site except for periodic maintenance personnel.

- b. Square footage of building.

No buildings are planned at the station.

- c. Lot area.

1.420 acres

- d. Anticipated opening date.

2019

6. **Sustainability.** Highlight ways in which the project furthers the City's environmental goals regarding sustainability. This may include a general description of the project location relative to other uses, public transit and trails; ease of travel to key destinations on foot or bicycle; water conservation and water quality measures; site layout; green building practices; or operational aspects of the use such as waste reduction, recycling or commuter trip reduction programs.

The Lone Tree City Center Station is being designed to safely and efficiently meet the needs of all users, regardless of age and ability. High capacity transit service will provide a viable option to the private automobile, resulting in a more balanced transportation system for Lone Tree residents.

7. **Variations if applicable.** For those SIPs for which a variance from the standards in this Chapter, the Design Guidelines or Sub-Area Plans is requested, the narrative shall also explain the need for the variance. (Public notice may be required, see Section 16-26-60).

No variances are required.

Statement of Design Intent

Please describe how the project meets the intent of the [City of Lone Tree Design Guidelines](#), including the city's Core Design Principles (p. 11). If the project is located within a Planned Development that is governed by additional design standards or guidelines, please address how the project satisfies the intent of those standards and guidelines as well.

Please use the outline below as a guide in formulating your response. You may also use this opportunity describe particular strengths, unique features, sustainable practices, or innovations that distinguish the design of the project, as well as any particular opportunities or challenges that should be considered. This Statement of Design Intent is intended to encourage thoughtful consideration of design guidelines and to give project reviewers and decision makers a more thorough understanding of the project.

1. **Overall Design Concept.** Briefly describe the use and overall concept for the project as a whole.

The Lone Tree City Center Station is a proposed transit station located along RTD's proposed Southeast Rail Extension. The Southeast Rail extension extends light rail transit service 2.3 miles south of the existing Lincoln Station. Initially, the Lone Tree City Center station will consist of just a platform located on each side of the LRT tracks. Other station features such as transition plazas, multi-modal interfaces, and pedestrian amenities will be added as the adjacent areas develop.

2. **Context and Site.** Describe how the project relates functionally and visually to the context of the surrounding area. Consider issues of form and character, the natural environment, vehicular and pedestrian access and circulation, etc.

The Lone Tree City Center station is located on the western half of the Lone Tree City Center. Based on the City's Comprehensive Plan, the City Center is proposed to be a compact, high density pedestrian-oriented development anchored by transit. Build-out of the station as part of the overall development of the City Center provides a unique opportunity to create a civic destination, resulting in full and complementary integration in the community.

3. **Public Realm.** Describe how the project contributes to an inviting, safe and functional public realm. Consider public spaces, street/sidewalk – level experience, lighting, landscaping, and signage.

The focus of the Lone Tree City Center Station is pedestrians. The station must accommodate all potential users, regardless of age or mobility. Ultimately, the simplicity and clarity required to make the station friendly, accessible and understandable demands an integration with adjacent activities and properties. Initially, lighting, cameras, and windscreens/benches are provided on the light rail platform to create a safe and inviting environment.

4. **Architectural Design.** Describe how the architectural design contributes to the unique qualities of the area and how design concepts result in a unified, functional and high-quality design. Consider building form and composition, façade composition and articulation, and materials, colors, and lighting.

The station platform overhead canopy structures are white, barrel shaped, with a standing seam roof.

Applicant Contact Information

Name: Nathan Herman
Business: Regional Transportation District
Address: 1600 Blake Street, Denver, CO 8020
Phone: 303.628.9000
Email: Nathan.Herman@RTD-Denver.com

Preparer Contact Information

Name: Lee Kellar
Business: WSP | Parsons Brinckerhoff
Address: 555 17th Street, Suite 500, Denver, CO 80202
Phone: 303.728.3047
Email: kellarlw@pbworld.com

Owner Contact Information

Name: Keith Simon
Business: RidgeGate Investments, Inc.
Address: 10270 Commonwealth Street, Suite B, Lone Tree, CO 80124
Phone: 720.279.2581
Email: KSimon@coventrydevelopment.com

Kelly First

From: Linda Langewisch <llangewisch@msiho.com>
Sent: Wednesday, March 30, 2016 12:46 PM
To: Kelly First
Subject: FW: City of Lone Tree referral request

Good Afternoon:

The RidgeGate West Village HOA has no comment to the referral re: light rail stations. They seem to be well thought out and accessible.

Thank you,

Linda Langewisch, CMCA
Community Manager
MSI, LLC
6892 So. Yosemite Court Suite 2.101
Centennial, Co 80112
720.974.4273
Fax 303.751.7396
LLangewisch@msiho.com

From: Kelly First [<mailto:Kelly.First@cityoflonetree.com>]
Sent: Monday, March 28, 2016 4:40 PM
Subject: City of Lone Tree referral request

Good Afternoon,

You are invited to review and comment on a referral request from the City of Lone Tree. There are three separate, but related, Site Improvement Plans (SIPs) for each of the three future light rail stations in Lone Tree as part of RTD's Southeast Rail Extension Project. Please forward any comments to kelly.first@cityoflonetree.com by **April 12th, 2016 via email or in writing to the address below.**

[Sky Ridge Station](#)
[Lone Tree City Center Station](#)
[RidgeGate TOD Station](#)

If you have already received this information through the City's website subscription list, please disregard this email. I apologize for any duplicates.
Thank you for your time!

Kelly A. First, AICP, LEED® AP
Community Development Director

City of Lone Tree
9220 Kimmer Drive, Suite 100
Lone Tree, CO 80124
Main ph: 303-708-1818
Direct line: 720-509-1274



SOUTH METRO FIRE RESCUE

LIFE SAFETY BUREAU

9195 E Mineral Ave, Centennial, CO 80112

PHONE: 720.989.2230 www.southmetro.org FAX: 720.989.2130

Jennifer Drybread
Senior Planner
City of Lone Tree
Community Development
9220 Kimmer Dr.
Lone Tree, CO 80124
303-708-1818

File #/Name: SP16-~~09~~¹⁰R
Project Type: RidgeGate Section 15
Referral Received: March 25, 2016
Comments Due: April 12, 2016

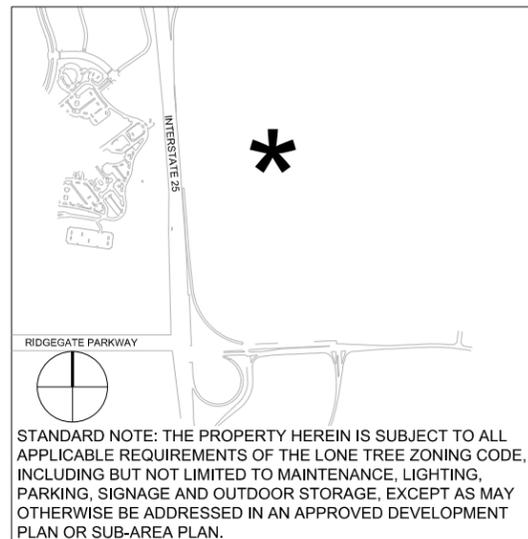
SMFR Record #: REFSI16-00030
Review Date: April 1, 2016
Plan Reviewer: Jeff Sceili
720-989-2244
Jeff.sceili@southmetro.org

South Metro Fire Rescue has reviewed the above referenced referral submittal for compliance with the International Fire Code and other applicable standards. The SIP is approved as submitted.

SIP SHEET INDEX		
NUMBER	Sheet Name	Sheet Issue Date
1 of 8	SITE PLAN	03/18/16
2 of 8	GRADING PLAN	03/18/16
3 of 8	SITE FURNISHINGS	03/18/16
4 of 8	SITE FURNISHINGS	03/18/16
5 of 8	PHOTOMETRICS PLAN	03/18/16
6 of 8	LIGHTING FIXTURES	03/18/16
7 of 8	EAST PLATFORM ELEVATION	03/18/16
8 of 8	WEST PLATFORM ELEVATION	03/18/16

SECTION 14, TOWNSHIP 6 SOUTH, RANGE 67 WEST OF THE SIXTH PRINCIPAL MERIDIAN

RIDGEGATE PDD, 4TH AMENDMENT, PLANNING AREA C/M-U #3
1.423 ACRES
SP16-10R
(LONE TREE CITY CENTER STATION)



Vicinity Map

STANDARD NOTE: THE PROPERTY HEREIN IS SUBJECT TO ALL APPLICABLE REQUIREMENTS OF THE LONE TREE ZONING CODE, INCLUDING BUT NOT LIMITED TO MAINTENANCE, LIGHTING, PARKING, SIGNAGE AND OUTDOOR STORAGE, EXCEPT AS MAY OTHERWISE BE ADDRESSED IN AN APPROVED DEVELOPMENT PLAN OR SUB-AREA PLAN.

<p>OWNER RIDGEGATE INVESTMENTS, INC. 10270 COMMONWEALTH STREET, SUITE B LONE TREE, CO 80124 P: 720.279.2581 CONTACT: KEITH SIMON</p>	<p>LANDSCAPE DESIGN BRITINA DESIGN GROUP 1615 CALIFORNIA STREET, SUITE 411 DENVER, CO 80202 P: 303.456.2887 CONTACT: CHRIS COOPER</p>
<p>APPLICANT REGIONAL TRANSPORTATION DISTRICT SOUTHEAST RAIL EXTENSION 1560 BROADWAY DENVER, CO 80202 P: 303.299.6965 CONTACT: ANDY MUTZ</p>	<p>STRUCTURAL ENGINEERING SAN ENGINEERING LLC 801 WEST MINERAL AVENUE, SUITE 200 LITTLETON, CO 80120 P: 303.953.9014 CONTACT: STEVE HAKES</p>
<p>LEAD DESIGN FIRM WSP PARSONS BRINCKERHOFF 555 17TH STREET, SUITE 500 DENVER, CO 80202 P: 303.832.9091 CONTACT: KAREN CREAMER</p>	<p>LIGHTING AND ELECTRICAL DESIGN PK ELECTRICAL, INC. 5105 DTC PARKWAY, SUITE 420 GREENWOOD VILLAGE, COLORADO 80111 P: 720.481.3290 CONTACT: ALAN WISKUS</p>
<p>ARCHITECT IRON HORSE ARCHITECTS 475 17TH STREET, SUITE 720 DENVER, CO 80202 P: 720.855.7572 CONTACT: TARA BURKE</p>	<p>GEOTECHNICAL ENGINEERING YEH AND ASSOCIATES, INC. 2000 CLAY STREET, SUITE 200 DENVER, CO 80211 P: 303.781.9590 CONTACT: MICHAEL KIEFER</p>

THIS SIP HAS BEEN REVIEWED AND FOUND TO BE COMPLETE AND IN ACCORD WITH CITY REGULATIONS, AS APPROVED BY THE CITY ON (DATE).

BY: _____

NAME: _____
TITLE: COMMUNITY DEVELOPMENT DIRECTOR

DATE: _____

BY: _____

NAME: _____
TITLE: CITY ENGINEER

DATE: _____

BY: _____

NAME: _____
TITLE: MAYOR

DATE: _____

THE OWNER(S) OF THE LANDS DESCRIBED HEREIN, HEREBY AGREE(S) (1) TO DEVELOP AND MAINTAIN THE PROPERTY DESCRIBED HEREON IN ACCORDANCE WITH THIS APPROVED SITE IMPROVEMENT PLAN AND IN COMPLIANCE WITH CHAPTER 16 OF THE LONE TREE MUNICIPAL CODE AND THAT (2) THE HEIRS, SUCCESSORS AND ASSIGNS OF THE OWNER(S) SHALL ALSO BE BOUND. THE SIGNATURES OF THE OWNERS(S)/(S) REPRESENTATIVE(S) BELOW INDICATE THAT ANY REQUIRED AUTHORIZATIONS TO ENTER THIS AGREEMENT, INCLUDING ANY CORPORATE AUTHORIZATIONS, HAVE BEEN OBTAINED.

(NAME OF OWNER) _____

(SIGNATURE OF OWNER) _____

(PRINTED NAME AND TITLE) _____

STATE OF _____)
) ss.

COUNTY OF _____)

SUBSCRIBED AND SWORN TO BEFORE ME THIS ____ DAY OF _____, 20____, BY _____.

WITNESS MY HAND AND OFFICIAL SEAL.
MY COMMISSION EXPIRES: _____

(NOTARY PUBLIC)
APPROVAL BY THE CITY OF LONE TREE DOES NOT SIGNIFY THAT THE REQUIREMENTS OF THE AMERICANS WITH DISABILITIES ACT (ADA) HAVE BEEN SATISFIED. THE APPLICANT IS RESPONSIBLE TO ENSURE THAT SAID ADA REQUIREMENTS HAVE BEEN MET.

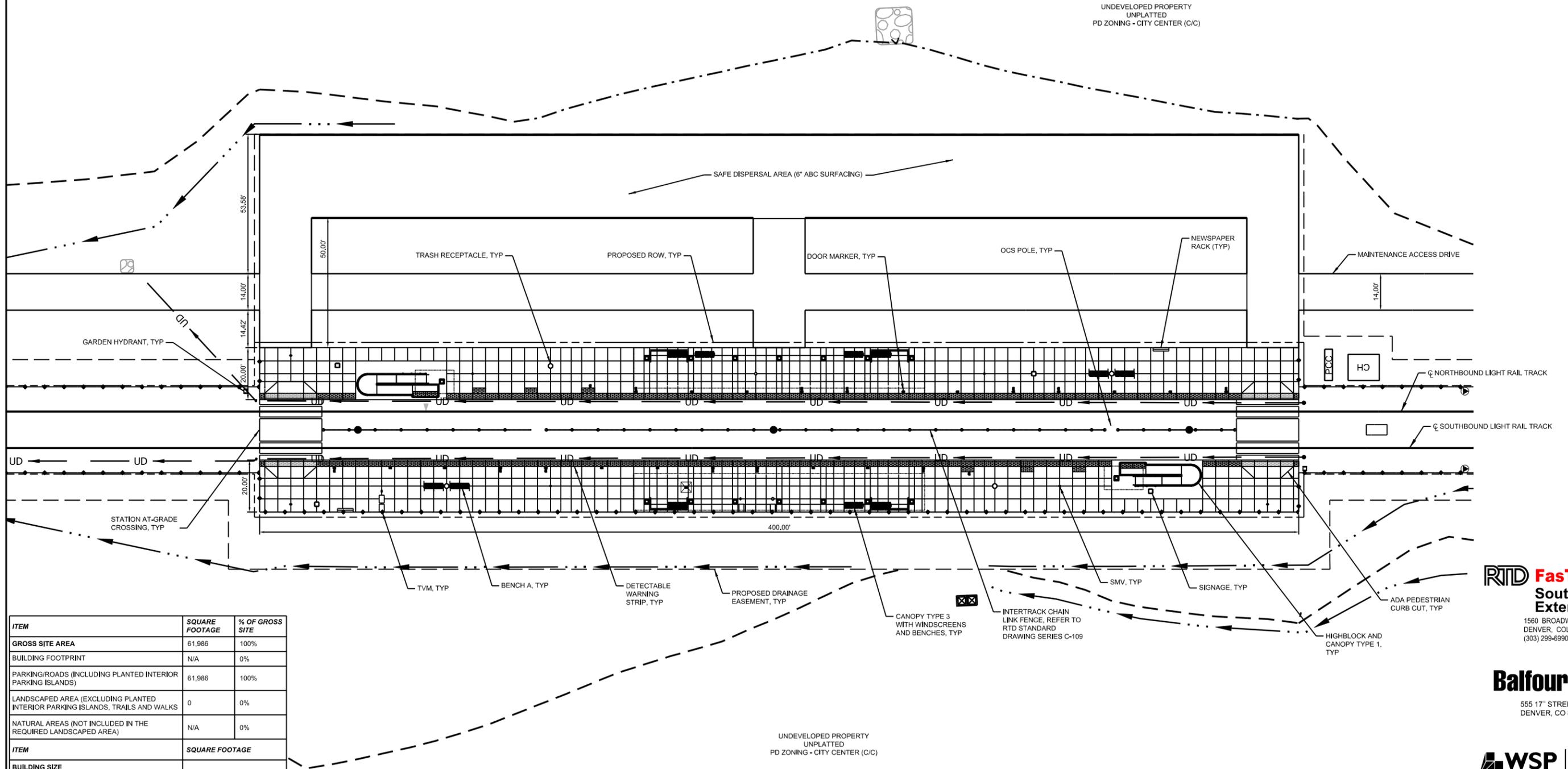


March 18, 2016

COVER

SECTION 14, TOWNSHIP 6 SOUTH, RANGE 67 WEST
OF THE SIXTH PRINCIPAL MERIDIAN

RIDGEGATE PDD, 4TH AMENDMENT, PLANNING AREA C/M-U #3
1.423 ACRES
SP16-10R
(LONE TREE CITY CENTER STATION)

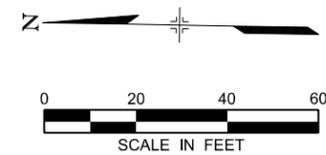


ITEM	SQUARE FOOTAGE	% OF GROSS SITE
GROSS SITE AREA	61,986	100%
BUILDING FOOTPRINT	N/A	0%
PARKING/ROADS (INCLUDING PLANTED INTERIOR PARKING ISLANDS)	61,986	100%
LANDSCAPED AREA (EXCLUDING PLANTED INTERIOR PARKING ISLANDS, TRAILS AND WALKS)	0	0%
NATURAL AREAS (NOT INCLUDED IN THE REQUIRED LANDSCAPED AREA)	N/A	0%
ITEM	SQUARE FOOTAGE	
BUILDING SIZE		
MAXIMUM HEIGHT (AS MEASURED BY THE CITY'S BUILDING CODE)	N/A	
TOTAL FLOOR AREA (SEE DEFINITION OF FLOOR AREA IN ARTICLE XXXVI)	N/A	
ITEM	SPACES REQUIRED	
PARKING		
REQUIRED	0 (KISS N RIDE PER EIA)	
PROVIDED	0	
BICYCLE PARKING		
REQUIRED	0	
PROVIDED	0	

RTD FasTracks
Southeast Rail Extension
1560 BROADWAY, SUITE 700
DENVER, COLORADO 80202
(303) 299-6990

Balfour Beatty
555 17TH STREET, SUITE 400
DENVER, CO 80202

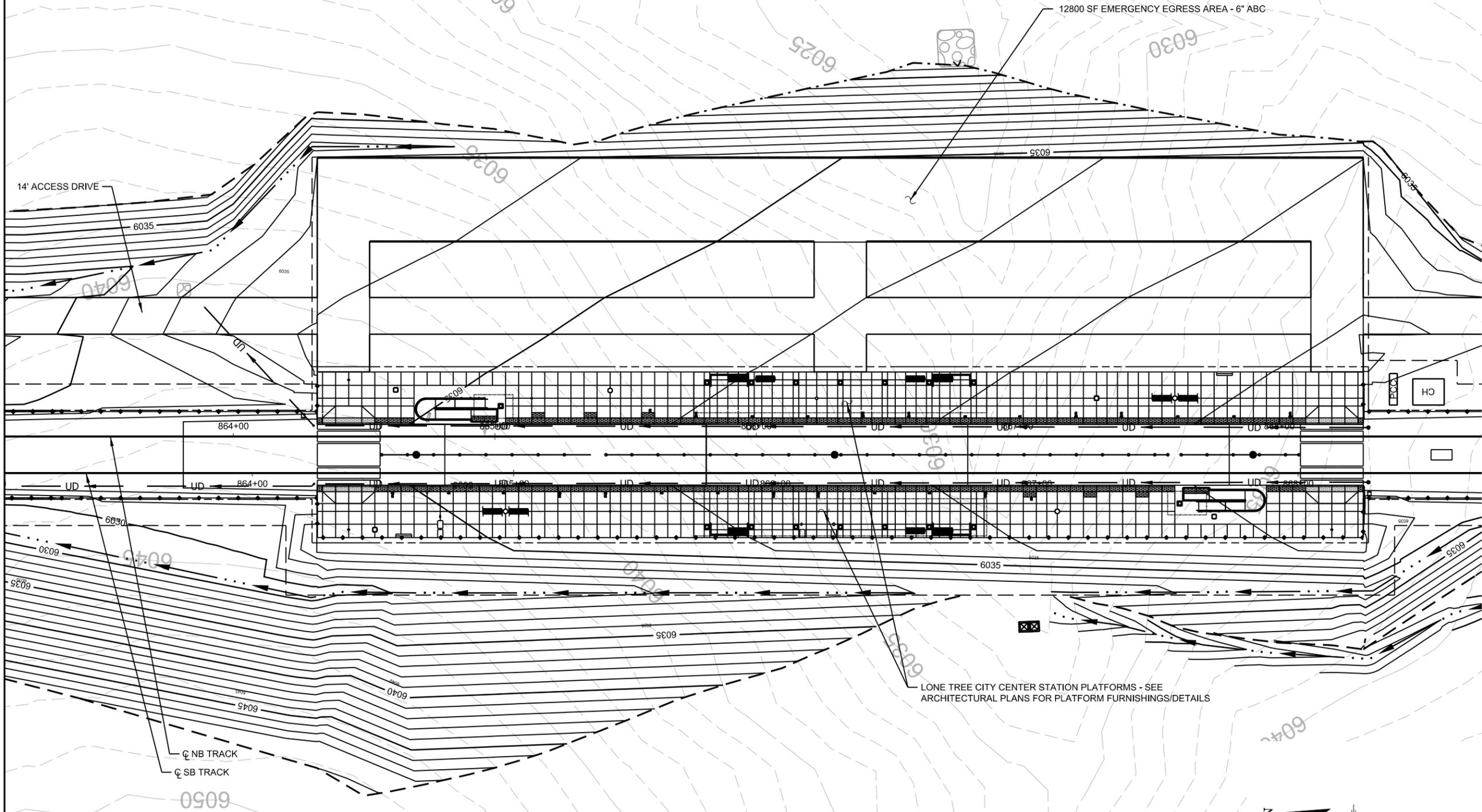
WSP | PARSONS BRINCKERHOFF
555 17TH STREET, SUITE 500
DENVER, CO 80202
P: 303.832.9091



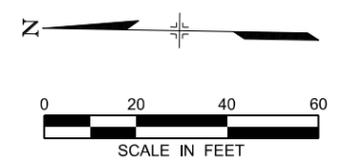
SITE PLAN

SECTION 14, TOWNSHIP 6 SOUTH, RANGE 67 WEST
OF THE SIXTH PRINCIPAL MERIDIAN

RIDGEGATE PDD, 4" AMENDMENT, PLANNING AREA C/M-U #3
1.423 ACRES
SP16-10R
(LONE TREE CITY CENTER STATION)



LONE TREE CITY CENTER STATION PLATFORMS - SEE ARCHITECTURAL PLANS FOR PLATFORM FURNISHINGS/DETAILS



RTD FasTracks
Southeast Rail Extension
1560 BROADWAY, SUITE 700
DENVER, COLORADO 80202
(303) 299-6990

Balfour Beatty
555 17TH STREET, SUITE 400
DENVER, CO 80202

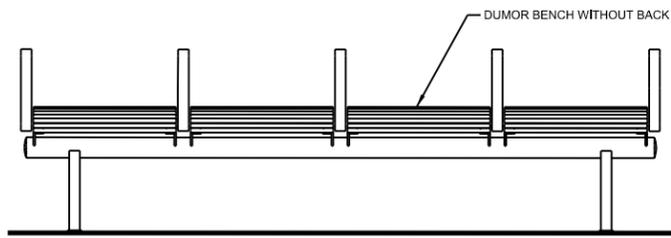
WSP | PARSONS BRINCKERHOFF
555 17TH STREET, SUITE 500
DENVER, CO 80202
P: 303.832.9091

OVERALL GRADING PLAN

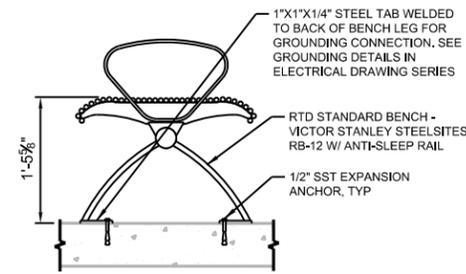
SHEET 2 OF 8

SECTION 14, TOWNSHIP 6 SOUTH, RANGE 67 WEST
OF THE SIXTH PRINCIPAL MERIDIAN

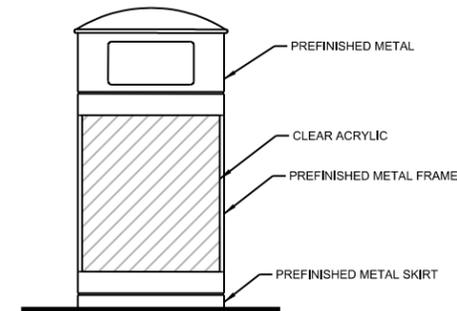
RIDGEGATE PDD, 4TH AMENDMENT, PLANNING AREA C/M-U #3
1.423 ACRES
SP16-10R
(LONE TREE CITY CENTER STATION)



BENCH A FRONT ELEVATION



BENCH A SIDE ELEVATION



LITTER RECEPTACLE ELEVATION

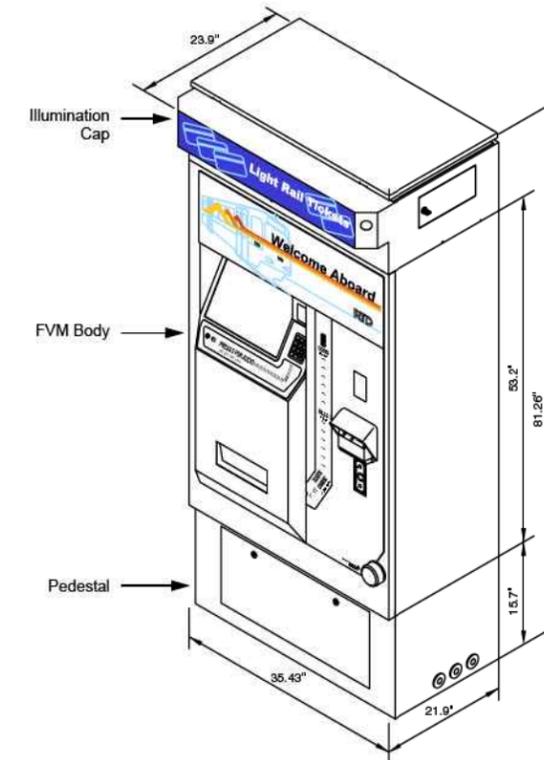
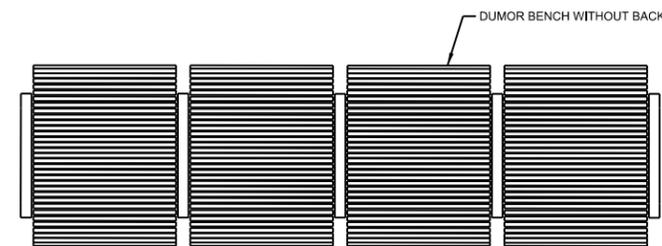


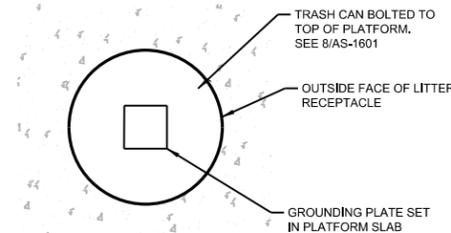
Figure 11-2 FVM Dimensions

FVM	FVM Unpacked	FVM Packed
Width	35.4"	Approximately 44"
Height	53.2"	Approximately 63"
Depth	21.9"	Approximately 31"
Weight	Approximately 551 lbs.	Approximately 591 lbs

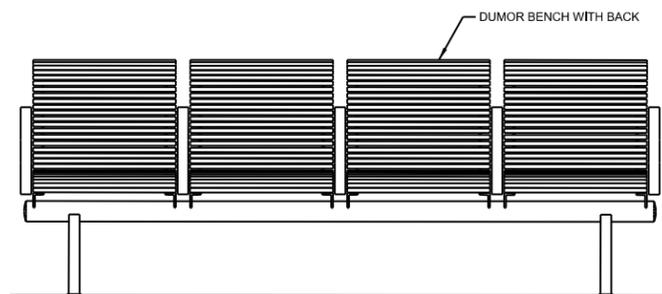
Table 11-1 FVM Dimensions and Weight



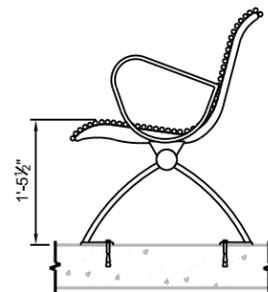
BENCH A PLAN



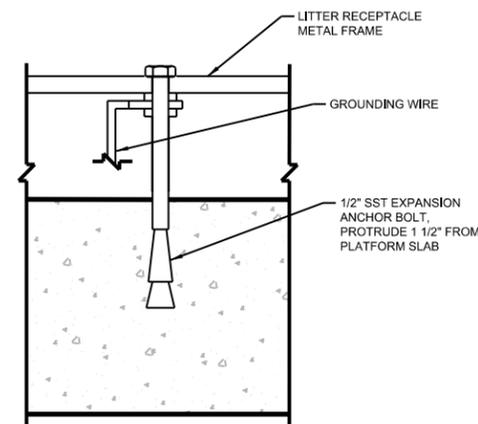
LITTER RECEPTACLE PLAN



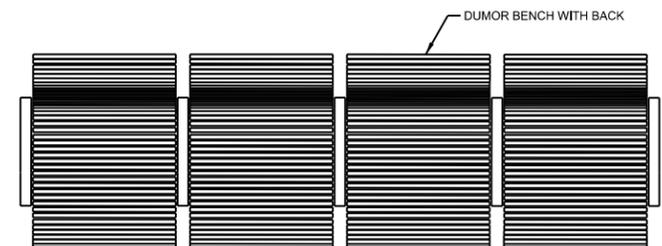
BENCH B FRONT ELEVATION



BENCH B SIDE ELEVATION



LITTER RECEPTACLE GROUNDING CONNECTION



BENCH B PLAN

TICKET VENDING MACHINE (TVM)

RTD FasTracks
Southeast Rail Extension
1560 BROADWAY, SUITE 700
DENVER, COLORADO 80202
(303) 299-6990

Balfour Beatty
555 17TH STREET, SUITE 400
DENVER, CO 80202

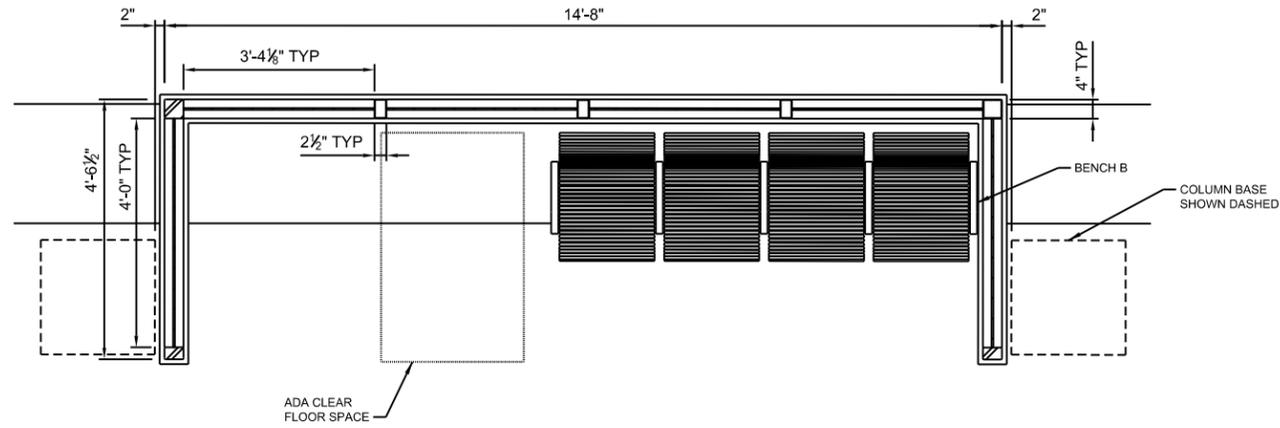
WSP | PARSONS BRINCKERHOFF
555 17TH STREET, SUITE 500
DENVER, CO 80202
P: 303.832.9091

SITE FURNISHINGS

SHEET 3 OF 8

SECTION 14, TOWNSHIP 6 SOUTH, RANGE 67 WEST
OF THE SIXTH PRINCIPAL MERIDIAN

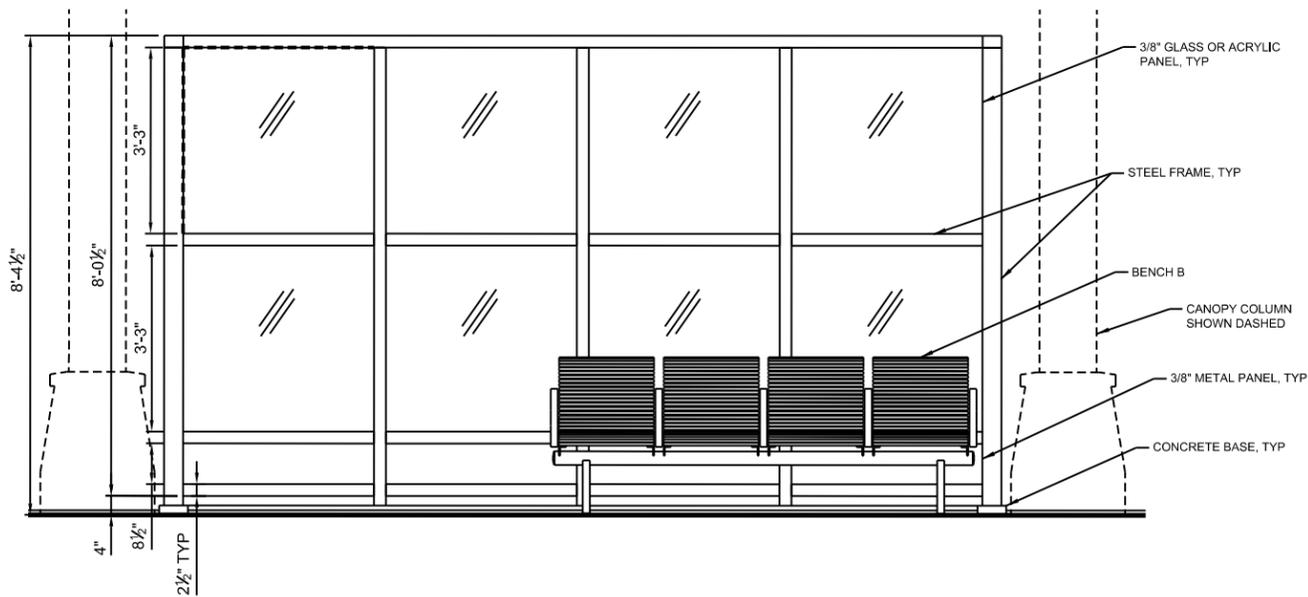
RIDGEGATE PDD, 4TH AMENDMENT, PLANNING AREA C/M-U #3
1.423 ACRES
SP16-10R
(LONE TREE CITY CENTER STATION)



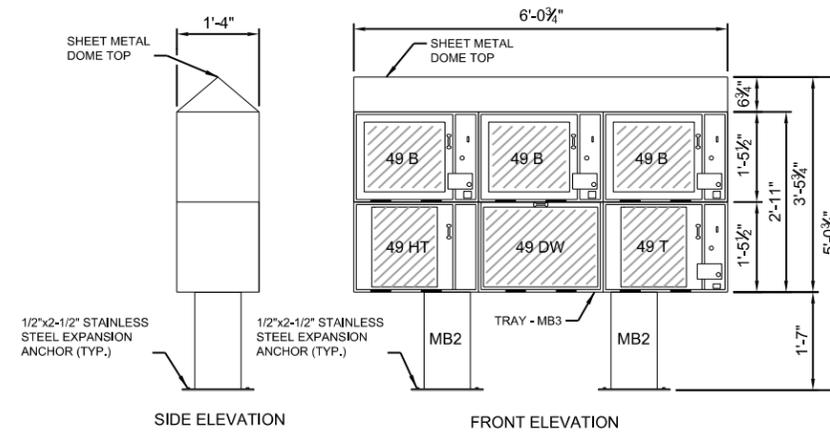
WINDSCREEN TYPE 1 PLAN

NOTES:

1. STANDARD COLOR SHALL BE RAL 6009 FIR GREEN
2. ALL FABRICATED COMPONENTS ARE STEEL SHOT-BLASTED, ETCHED, PHOSPHATIZED AND ELECTROSTATICALLY POWDER-COATED WITH TGIC POLYESTER POWDER COATING.
3. ALL SPECIFICATIONS ARE SUBJECT TO CHANGE. PLEASE CONTACT FACTORY FOR DETAILS.
4. ALL DIMENSIONS ARE IN INCHES.
5. AVAILABLE WITH LOCK OR LATCH
6. "RAK" SYSTEM MODEL NOS. AND DESCRIPTIONS AVAILABLE FROM:
RAK SYSTEMS, INC.
5500 PLANTATION RD.
THEODORE, AL 36582 U.S.A.
TOLL FREE: (800) 467-1725
LOCAL: (251) 653-4080
FAX: (251) 653-1014



WINDSCREEN TYPE 1 FRONT ELEVATION



NEWSPAPER "RAK" SYSTEMS

RTD FasTracks
Southeast Rail Extension
1560 BROADWAY, SUITE 700
DENVER, COLORADO 80202
(303) 299-6990

Balfour Beatty
555 17TH STREET, SUITE 400
DENVER, CO 80202

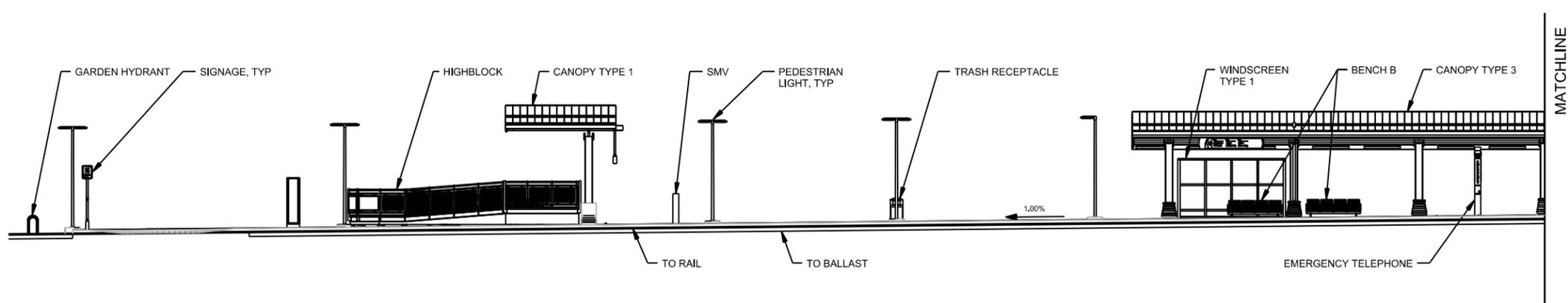
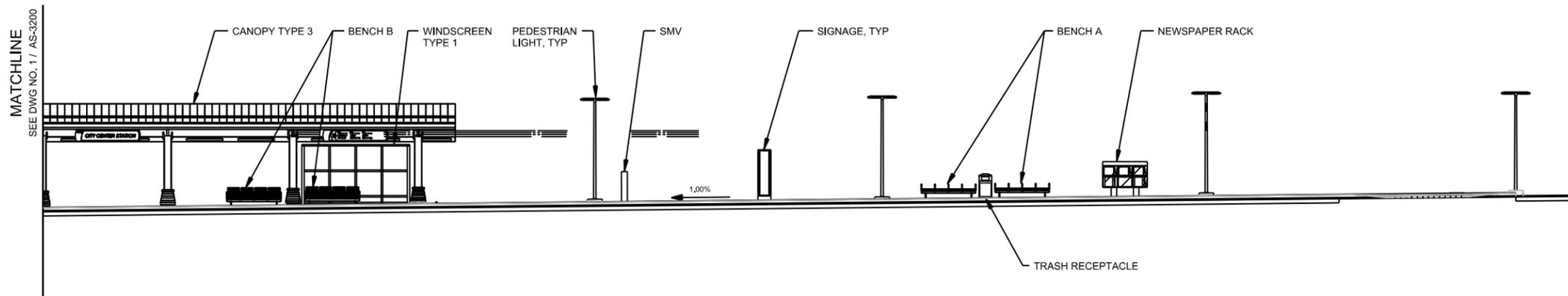
WSP | PARSONS BRINCKERHOFF
555 17TH STREET, SUITE 500
DENVER, CO 80202
P: 303.832.9091

SITE FURNISHINGS

SHEET 4 OF 8

SECTION 14, TOWNSHIP 6 SOUTH, RANGE 67 WEST
OF THE SIXTH PRINCIPAL MERIDIAN

RIDGEGATE PDD, 4TH AMENDMENT, PLANNING AREA C/M-U #3
1.423 ACRES
SP16-10R
(LONE TREE CITY CENTER STATION)



RTD FasTracks
Southeast Rail Extension
1560 BROADWAY, SUITE 700
DENVER, COLORADO 80202
(303) 299-6990

Balfour Beatty
555 17TH STREET, SUITE 400
DENVER, CO 80202

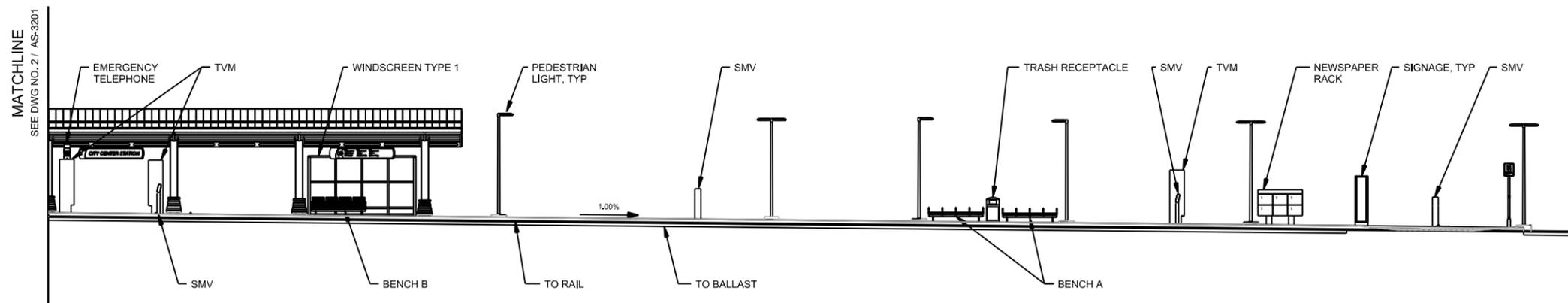
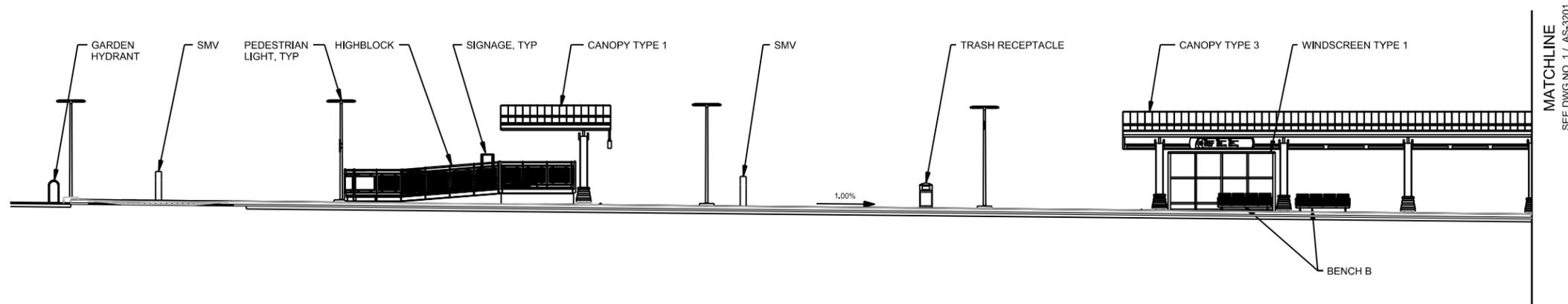
WSP | PARSONS BRINCKERHOFF
555 17TH STREET, SUITE 500
DENVER, CO 80202
P: 303.832.9091

EAST PLATFORM ELEVATION

SHEET 7 OF 8

SECTION 14, TOWNSHIP 6 SOUTH, RANGE 67 WEST
OF THE SIXTH PRINCIPAL MERIDIAN

RIDGEGATE PDD, 4th AMENDMENT, PLANNING AREA C/M-U #3
1.423 ACRES
SP16-10R
(LONE TREE CITY CENTER STATION)



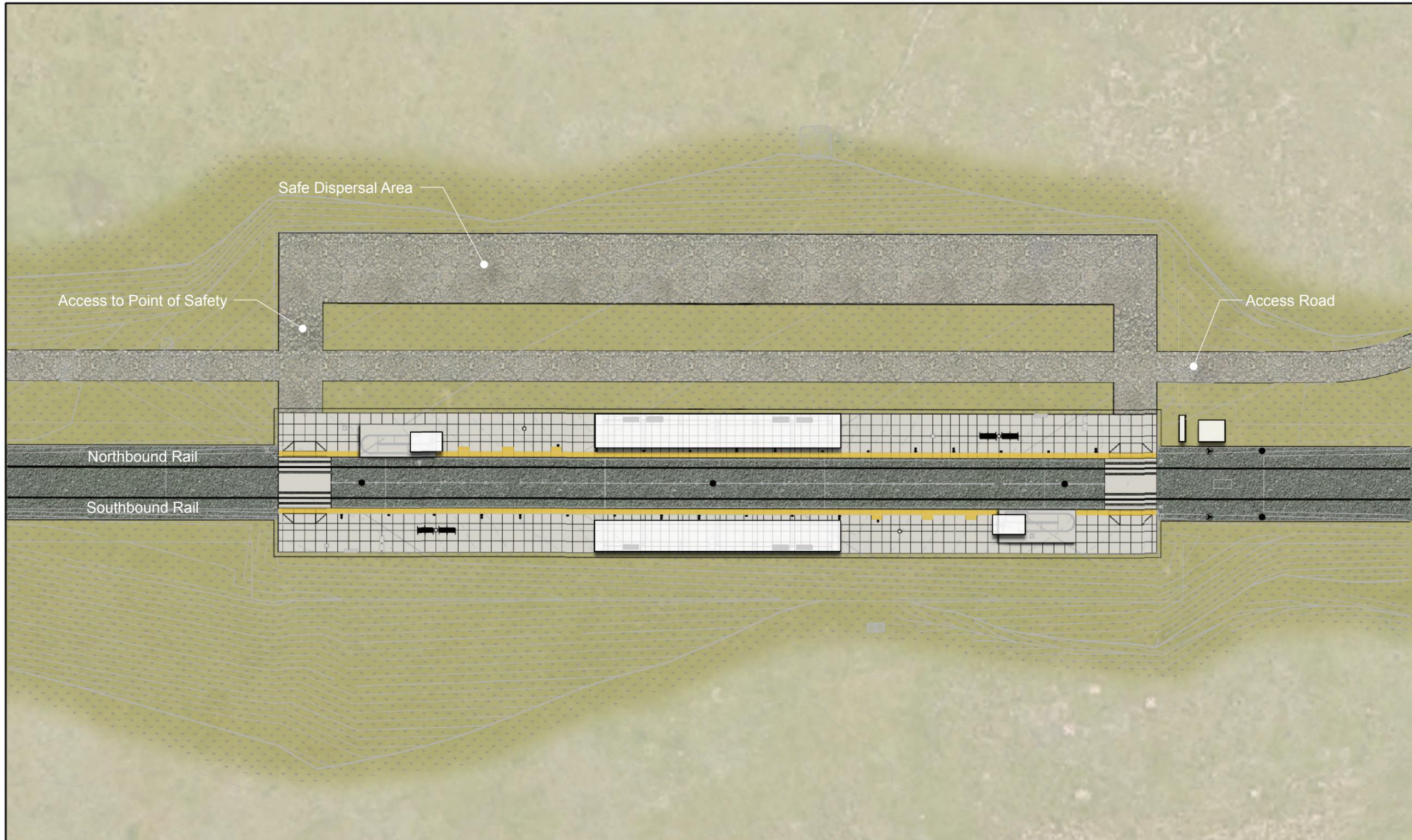
RTD FasTracks
Southeast Rail Extension
1560 BROADWAY, SUITE 700
DENVER, COLORADO 80202
(303) 299-6990

Balfour Beatty
555 17th STREET, SUITE 400
DENVER, CO 80202

WSP | PARSONS BRINCKERHOFF
555 17TH STREET, SUITE 500
DENVER, CO 80202
P: 303.832.9091

WEST PLATFORM ELEVATION

SHEET 8 OF 8



Safe Dispersal Area

Access to Point of Safety

Access Road

Northbound Rail

Southbound Rail

RTD FasTracks
Southeast Rail Extension
 1560 BROADWAY, SUITE 700
 DENVER, COLORADO 80202
 (303) 299-6990

Balfour Beatty
 555 17TH STREET, SUITE 400
 DENVER, CO 80202

WSP | PARSONS BRINCKERHOFF
 555 17TH STREET, SUITE 500
 DENVER, CO 80202
 P: 303.832.9091

IRON HORSE ARCHITECTS
 RESPONSIBLE DESIGN
 475 17TH STREET, SUITE 720
 DENVER, CO 80202

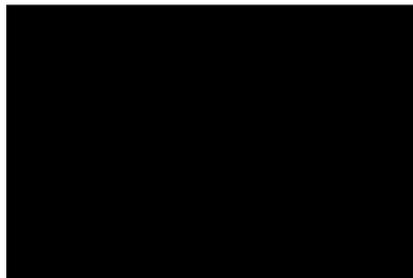


BRITINA
 design group
 1615 CALIFORNIA STREET, SUITE 411
 DENVER, CO 80202
 P: 303.456.2887



**Pre-Finished Standing Seam
Berridge Shasta White**

Canopy Roof



**Painted/Pre-Finished Metal
Black RAL 9005**

Windscreen Structure and Panels,
Railings, Benches, Light Poles,
Trash Receptacles



**Painted/Pre-Finished Metal
Tnemec White 00WH**

Canopy Structure
VMS
Canopy Deck



**Tactile Warning Strip
Federal Standard #33538**



**Concrete
Medium Broom Finish**
Platform, Plaza, Sidewalks



**Stone Veneer
Colorado Rose Sandstone, Buff Finish**
Canopy Column Bases



**Stone Veneer
Colorado Sandstone, Buff Finish**
Canopy Column Bases



RTD FasTracks
Southeast Rail Extension
1560 BROADWAY, SUITE 700
DENVER, COLORADO 80202
P: (303) 299-6890

Balfour Beatty
555 17TH STREET, SUITE 400
DENVER, CO 80202

**WSP | PARSONS
BRINCKERHOFF**
555 17TH STREET, SUITE 500
DENVER, CO 80202
P: 303.832.9091

IRON HORSE
ARCHITECTS
RESPONSIBLE DESIGN
475 17TH STREET, SUITE 720
DENVER, CO 80202



BRITINA
design group
1615 CALIFORNIA STREET, SUITE 411
DENVER, CO 80202
P: 303.456.2887



CITY OF LONE TREE

STAFF REPORT

TO: City of Lone Tree Planning Commission
FROM: Kelly First, Community Development Director
DATE: April 6, 2016
FOR: April 12, 2016 Planning Commission Meeting
SUBJECT: RidgeGate East Filing 1, (1st Amendment)
Site Improvement Plan SP16-11R
RidgeGate Station

Owner

RidgeGate Investments, Inc
Keith Simon
10270 Commonwealth Street
Lone Tree, CO 80124
720-279-2581

Representative

Regional Transportation District
Nathan Herman
1600 Blake Street
Denver, CO 80202
303-628-9000

Planning Commission Meeting Date: April 12, 2016
City Council Meeting Date: April 19, 2016

A. REQUEST:

Approval of a Site Improvement Plan (SIP) for the RidgeGate Light Rail Station.

This station is the end of the Regional Transportation District (RTD) Southeast Rail Extension project. It includes parking for 1,300 vehicles, a bus transfer facility, a Kiss-n-Ride, and bicycle storage.

A separate application is in process that will replat the property to create access and lots that coincide with the proposed station design. Pursuant to the City's Subdivision Regulations, the replat is considered by the City Council.

LOCATION:

The overall site is 12 acres, and is bordered by I-25 on the west, Havana Street on the east, and RidgeGate Parkway on the north. Schweiger Ranch is located to the south, across Havana Street.

Vicinity Map



B. SITE CHARACTERISTICS:

The site has no distinctive topographical or vegetative features. As part of the Southeast Extension project, RTD prepared an Environmental Assessment, which resulted in a Finding of No Significant Impact.

C. SERVICE PROVIDERS:

Water:	Parker Water District
Sanitation:	Parker Sanitation District
Police:	Lone Tree Police
Fire:	South Metro Fire Rescue Authority
Metro District:	Rampart Range Metropolitan District

D. BACKGROUND:

The 2.3 mile-long southeast extension project, from Lincoln Avenue to RidgeGate Parkway, is part of the RTD's 2004 voter-approved FasTracks Program. The Southeast Line includes 19.1 miles extending from I-25/Broadway to Lincoln Avenue, with an

additional connection along I-225 from I-25 to Parker Road. Construction on the extension is expected to begin this spring with service beginning in 2019. When completed, residents will be able to travel from the RidgeGate station on the new R Line through Aurora to Peoria Station and connect to the commuter train to Denver International Airport. They will be able to travel from RidgeGate to Union Station on the E Line and from RidgeGate to 18th and California in downtown Denver on the F Line.

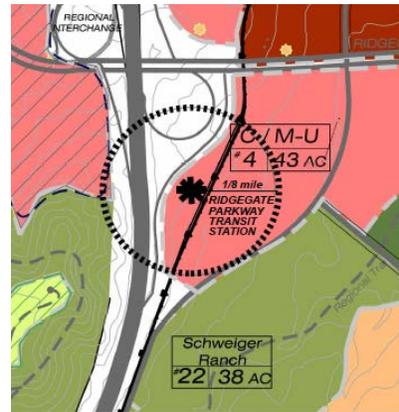
This SIP is one of three station SIPs that comprise the RTD Southeast Rail Extension project. Additional stations are Sky Ridge (SP16-09R) and Lone Tree City Center (SP16-10R).

In January of 2015, City staff and members of the RTD design team presented an overview of the project to the Planning Commission and City Council. The purpose of the work sessions was to seek feedback about key design elements and address them prior to the project going out for bid, thereby reducing the need for potentially costly changes later on. Comments regarding aesthetic elements like canopies, walls, fencing and landscaping were provided and have been incorporated into SIP. In March, 2016, RTD again provided an overview of the stations to the Planning Commission and City Council, in anticipation of the SIPs coming before the City Council for action in April.

E. DESCRIPTION:

Zoning. The site is zoned Planned Development (PD) under the RidgeGate Planned Development 4th Amendment. The proposed use is in conformance with zoning, and is located within Planning Area C/M-U #4.

A sub-area plan for the property is currently being developed by the owner, with participation by the City of Lone Tree staff. The sub-area plan will incorporate the station design and provide guidance for how the balance of the property will be transit-oriented development.

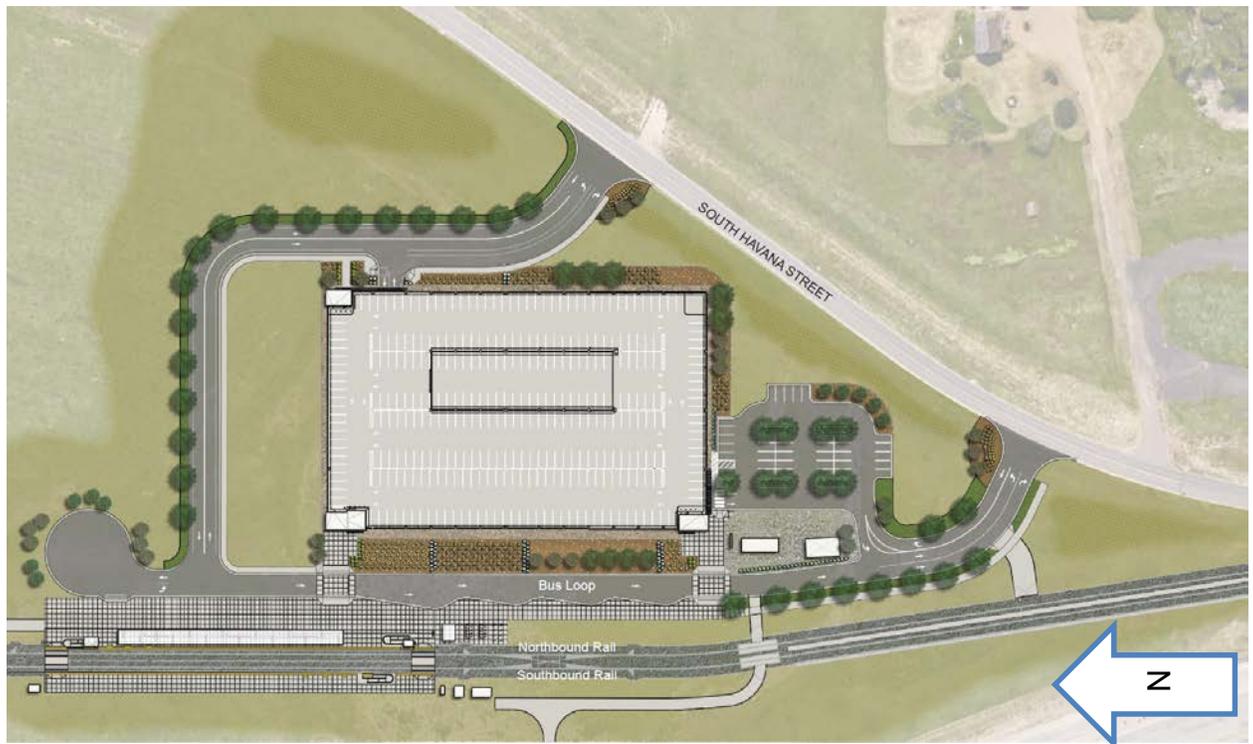


Site Design. The site is designed to provide convenient and safe access and circulation, and contains design features that create an inviting and safe public realm. The light rail line track extends down along the western side of the site. The station is located at the southern end of the property, with the platform on the west side. The balance of the 12-acre property will be developed in the future by others.

The space immediately north of the parking garage is envisioned as a passive open space and park area. This location was originally considered for a retail building; however, the property owner has re-evaluated that concept and thinks retail will be better suited north of the road.

A detention/water quality pond is located on the east side of the site.

Site Plan



Access. Vehicular and pedestrian access will be from two points along Havana Street. A circular loop will provide car, bus, bicycle and pedestrian access to the station, with the bus operations taking place near the platform.

Vehicles access the 1st level of the parking garage on the east side, or the 2nd level on the south side. A temporary cul-de-sac to accommodate kiss-n-ride drop off and loading is located on the north side of the site, near the platform.

Parking. There are 1,264 parking space within the parking garage and 52 surface spaces to the south of the garage.

Design Elements. The station platform includes overhead canopy structures that are white, barrel shaped, with a standing seam metal roof. Benches, bike racks, trash receptacles, and other site features are detailed in the SIP.

Landscaping. The site will be landscaped as generally shown on the colored site plan and landscape plan. Details are still in progress relative to specific landscape species, sizes and quantities date. It is recommended that the final landscape plan be subject to staff approval prior to final approval of the SIP.

Building Design. The parking garage is four levels at the eastern end, transitioning with grade to three levels along the west. The structure is comprised on precast concrete and metal panels, with sandstone on the stair and elevator towers are located on the northeast, northwest, and southwest corners, and will consist of Colorado Rose sandstone, similar in color and appearance to that used elsewhere in Lone Tree. The west and south facades are clad in metal mesh framework for screening and visual interest.

Lighting. The location of street, parking and pedestrian lighting and associated specifications are included on the SIP. Lighting is compliant with City requirements.

F. REFERRALS:

The project was sent on referral to all key agencies and subscribers to the City's referral list. All responses of substance are attached. The Fire District notes an outstanding issue relative to water supply. Water service is not yet available to the site from Parker Water District. In the interim, water will be made available through a separate agreement between RTD and the Meridian Metropolitan District. Once that agreement is finalized, all required submittals will be made to South Metro Fire.

The City Forester made recommendations relative to landscaping that are noted and will be addressed as part of the final landscape plan review.

G. STAFF FINDINGS AND RECOMMENDATION:

Staff finds that the application is in conformance with the SIP requirements of the Lone Tree Zoning Code, the Subdivision Code, the Comprehensive Plan, and the RidgeGate PDD, 4th Amendment.

Staff recommends the Planning Commission recommend approval of the SIP to City Council, subject to the following conditions:

1. Final approval of the Site Improvement Plan is subject to City of Lone Tree Public Works approval.
2. Prior to final SIP approval, the related replat (SB16-23R) shall be approved by City staff and recorded.
3. Prior to final SIP approval, the landscape plan shall be further detailed per City Zoning requirements to specify proposed plant types, quantities and sizes.

H. ATTACHMENTS:

1. Development Application
2. Letter of Authorization

RidgeGate East Filing 1 (1st amendment)
Site Improvement Plan (RidgeGate Light Rail Station)
Project SP16-11R

3. Narrative & Statement of Design Intent
4. Referral Comments
5. SIP
6. Renderings

END

Letter of Authorization

Regarding Development Applications for Land Use Entitlements

City of Lone Tree
9220 Kimmer Drive #100
Lone Tree, CO 80124

RE: Property Address: Ridgegate East Filing 1

Assessor's Parcel Number (SPN): _____

To Whom It May Concern:

I/We, the owner(s) of the above described real property, authorize Nathan Herman
of Regional Transportation District to act as an agent on my/our behalf for the purpose of
creating, filing and/or managing an application for Ridgegate Station Site Improvement Plan
_____ (type of development or permit application).

The undersigned hereby certifies to being the fee owner(s) or legally authorized representative of the fee owner(s) of the real property described above.

~~Keith Simon~~ RidgeGate Investments, Inc.
(Print Name of Owner)

CVT, vice President
(Signature of Owner or Authorized Representative)

State of Colorado
County of Douglas

The foregoing instrument was acknowledged before me this 17th day of March, 20 16
by ~~Elizabeth Matthews~~ Keith D. Simon.

Elizabeth Matthews
(Notary's official signature)

NOTARY SEAL

5-12-2016
(Commission expiration date)

ELIZABETH MATTHEWS
NOTARY PUBLIC
STATE OF COLORADO
NOTARY ID 20004014222
MY COMMISSION EXPIRES 05/12/2016



CITY OF LONE TREE

Site Improvement Plan Project Narrative & Statement of Design Intent Template

Planning Division

9220 Kimmer Drive, Lone Tree, Colorado 80124

303.708.1818 | www.cityoflonetree.com

Project Name RTD – RidgeGate Station

Project # SP16-11R

Project Location NW Quadrant of Section 23 (West of Havana Street)

Date March 17, 2016

Project Narrative

[ARTICLE XXVII - Site Improvement Plan \(SIP\) Project Narrative](#). The SIP process is intended to provide for development that enhances the quality of life in the City by promoting high-quality design and a strong economy, and by fostering a sustainable and healthy community. The SIP process is required to ensure the development will be in conformance with the [Comprehensive Plan](#), the [Design Guidelines](#), applicable chapters of this [Code](#) and applicable Planned Developments and Sub-Area Plans.

Using this form or a separate page(s), the applicant shall provide a written narrative describing their project. Use the following outline (Sec. 16-27-60) as a guide when formulating your narrative – please disregard sections that do not apply to your project:

1. General information.

- a. Provide the subdivision name, filing number, planning area number when located in a Planned Development, lot and block number or street address and section, township and range if not in a subdivision, and name of project.

RidgeGate East Filing 1
RidgeGate PDD, 4th Amendment, Planning Area C/M-U #4
RidgeGate Station

- b. Indicate zoning of the site and the zoning and current uses of adjacent land.

RidgeGate Planned Development District, 4th Amendment

The site and surrounding area are undeveloped.

2. Development impacts. Describe overall impacts of the proposed development on adjacent lands and methods for mitigating those impacts.

The proposed RidgeGate Station is part of RTD's Southeast Rail Extension Project. RTD prepared an Environmental Assessment (EA) for the project which resulted in a Finding of No Significant Impact (October 2014). The EA addressed impacts to the adjacent lands and mitigation measures. For the RidgeGate Station, no impacts to adjacent lands were identified in the EA.

3. Compliance with Intent and Approval Standards. Describe how the development complies with the Intent (Section 16-27-10) and Approval Standards (Subsection 16-27-90(a)) of this Article.

The RidgeGate station is in compliance with the City's Comprehensive Plan, the Design Guidelines, and the RidgeGate PDD, 4th Amendment. The station design is following the standards set forth by the RidgeGate development to ensure it will be integrated both functionally and aesthetically with adjacent development.

4. Development phasing. Describe the proposed development schedule and phases of development for all proposed construction.

Construction of the Southeast Rail Extension is expected to start in 2016 and be completed in 2019. No phasing of the project is planned.

5. Other project data.

- a. Total number of employees on maximum shift when known (for parking purposes).

There will be no employees on site except for periodic maintenance personnel

- b. Square footage of building.

Parking garage is approximately 90,700 square feet per level (4 levels)

- c. Lot area.

12.00 Acres

- d. Anticipated opening date.

2019

6. **Sustainability.** Highlight ways in which the project furthers the City's environmental goals regarding sustainability. This may include a general description of the project location relative to other uses, public transit and trails; ease of travel to key destinations on foot or bicycle; water conservation and water quality measures; site layout; green building practices; or operational aspects of the use such as waste reduction, recycling or commuter trip reduction programs.

The RidgeGate Station is being designed to safely and efficiently meet the needs of all users, regardless of age, ability or mode of transportation. High capacity transit service will provide a viable option to the private automobile, resulting in a more balanced transportation system for Lone Tree residents.

7. **Variations if applicable.** For those SIPs for which a variance from the standards in this Chapter, the Design Guidelines or Sub-Area Plans is requested, the narrative shall also explain the need for the variance. (Public notice may be required, see Section 16-26-60).

The southeast corner of the parking garage is 37 feet +/- from the Havana Street right-of-way line. The minimum setback for a major arterial is 75 feet (Section 16-12-120). The configuration of the parcel does not allow for the garage placement to be adjusted to meet the minimum setback requirement.

Statement of Design Intent

Please describe how the project meets the intent of the [City of Lone Tree Design Guidelines](#), including the city's Core Design Principles (p. 11). If the project is located within a Planned Development that is governed by additional design standards or guidelines, please address how the project satisfies the intent of those standards and guidelines as well.

Please use the outline below as a guide in formulating your response. You may also use this opportunity describe particular strengths, unique features, sustainable practices, or innovations that distinguish the design of the project, as well as any particular opportunities or challenges that should be considered. This Statement of Design Intent is intended to encourage thoughtful consideration of design guidelines and to give project reviewers and decision makers a more thorough understanding of the project.

1. **Overall Design Concept.** Briefly describe the use and overall concept for the project as a whole.

The RidgeGate Station will be the new end of line station that will serve as a Park-n-Ride for RTD's proposed Southeast Rail Extension. The Southeast Rail extension extends light rail transit service 2.3 miles south of the existing Lincoln Station. The proposed features of the station include parking garage, parking lot, circular access road, landscaping, bus transfer facility, Kiss-n-Ride, bicycle storage, pedestrian walkways and crossings, transition plaza, and light rail station platform.

2. **Context and Site.** Describe how the project relates functionally and visually to the context of the surrounding area. Consider issues of form and character, the natural environment, vehicular and pedestrian access and circulation, etc.

The RidgeGate Station is bordered by I-25 on the west, Havana Street on the east, and RidgeGate Parkway on the north. Vehicular and pedestrian access will be provided from Havana Street. Within the station footprint, a circular loop will provided car, pedestrian, bicycle, and bus access to the station.

3. **Public Realm.** Describe how the project contributes to an inviting, safe and functional public realm. Consider public spaces, street/sidewalk – level experience, lighting, landscaping, and signage.

Pedestrian access and connectivity is provided throughout that RidgeGate station. Sidewalks connect from both access points along Havana Street and follow the circulation road to the parking garage, bus drop off, kiss-n-ride and pedestrian plaza. Landscaping is proposed throughout the site. Trees line the pedestrian routes to provide shade. A mix of evergreen trees, ornamental trees, and shade trees surround the garage. Signage is proposed throughout the station to provide way finding and RTD system information. Cameras and site lighting will help ensure a safe and inviting environment.

4. **Architectural Design.** Describe how the architectural design contributes to the unique qualities of the area and how design concepts result in a unified, functional and high-quality design. Consider building form and composition, façade composition and articulation, and materials, colors, and lighting.

The design of the parking garage is aesthetically pleasing and incorporates the surrounding community. The configuration of the stair and elevator tower reflects the beauty of Douglas County's Bluffs Regional Park. Sandstone on the towers ties the structure to RidgeGate station, adding a rich texture that reflects the character of other facilities throughout the City of Lone Tree. The west and south facades are clad in a metal mesh framework, which brings the large scale of the structure in line with the pedestrian nature of the station.

The station platform includes overhead canopy structures that are white, barrel shaped, with a standing seam roof.

Applicant Contact Information

Name: Nathan Herman
Business: Regional Transportation District
Address: 1600 Blake Street, Denver, CO 8020
Phone: 303.628.9000
Email: Nathan.Herman@RTD-Denver.com

Preparer Contact Information

Name: Lee Kellar
Business: WSP | Parsons Brinckerhoff
Address: 555 17th Street, Suite 500, Denver, CO 80202
Phone: 303.728.3047
Email: kellarlw@pbworld.com

Owner Contact Information

Name: Keith Simon
Business: RidgeGate Investments, Inc.
Address: 10270 Commonwealth Street, Suite B, Lone Tree, CO 80124
Phone: 720.279.2581
Email: KSimon@coventrydevelopment.com

Kelly First

From: Linda Langewisch <llangewisch@msiho.com>
Sent: Wednesday, March 30, 2016 12:46 PM
To: Kelly First
Subject: FW: City of Lone Tree referral request

Good Afternoon:

The RidgeGate West Village HOA has no comment to the referral re: light rail stations. They seem to be well thought out and accessible.

Thank you,

Linda Langewisch, CMCA
Community Manager
MSI, LLC
6892 So. Yosemite Court Suite 2.101
Centennial, Co 80112
720.974.4273
Fax 303.751.7396
LLangewisch@msiho.com

From: Kelly First [<mailto:Kelly.First@cityoflonetree.com>]
Sent: Monday, March 28, 2016 4:40 PM
Subject: City of Lone Tree referral request

Good Afternoon,

You are invited to review and comment on a referral request from the City of Lone Tree. There are three separate, but related, Site Improvement Plans (SIPs) for each of the three future light rail stations in Lone Tree as part of RTD's Southeast Rail Extension Project. Please forward any comments to kelly.first@cityoflonetree.com by **April 12th, 2016 via email or in writing to the address below.**

[Sky Ridge Station](#)
[Lone Tree City Center Station](#)
[RidgeGate TOD Station](#)

If you have already received this information through the City's website subscription list, please disregard this email. I apologize for any duplicates.
Thank you for your time!

Kelly A. First, AICP, LEED® AP
Community Development Director

City of Lone Tree
9220 Kimmer Drive, Suite 100
Lone Tree, CO 80124
Main ph: 303-708-1818
Direct line: 720-509-1274

From: Julius Zsako
Sent: Thursday, March 31, 2016 4:13 PM
To: Kelly First <Kelly.First@cityoflonetree.com>
Subject: RidgeGate TOD Station Comments

Kelly:

The following are my comments and suggestions related to the RidgeGate TOD Station:

Garage roof snow storage:

Per sheet 1 of 11, landscaping will be damaged by snow storage. The snow storage is unavoidable. The remedy is to move the landscaping out of harm's way at an appropriate distance from the foundation. Plans show three deciduous shade trees, three ornamental trees as well as deciduous shrubs and ornamental grasses that would likely be crushed and or freeze damaged by snow storage.

No landscaping on north side of garage:

This north facing area will receive shade from the garage. Consequently, the adjacent grounds are an ideal location to add clusters of large evergreen trees for their appearance to onlookers and ideal microclimate for survival.

Water detention pond:

Even without formal irrigation, a wildflower theme or native landscaping plan, and or sculpture related to transit would be important for such a vital location.

Thanks.

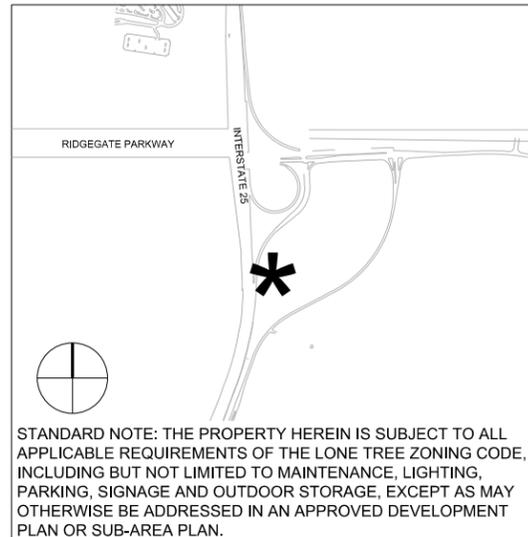
Julius Zsako
Zoning Enforcement Coordinator
& City Forester

City of Lone Tree
9220 Kimmer Drive, Suite 100
Lone Tree, CO 80124
Ph: 720-509-1269
www.cityoflonetree.com

SIP SHEET INDEX		
NUMBER	Sheet Name	Sheet Issue Date
1 of 11	SITE PLAN	03/18/16
2 of 11	GRADING PLAN	03/18/16
3 of 11	LANDSCAPE PLAN	03/18/16
4 of 11	SITE FURNISHINGS	03/18/16
5 of 11	SITE FURNISHINGS	03/18/16
6 of 11	PHOTOMETRICS PLAN	03/18/16
7 of 11	LIGHTING FIXTURES	03/18/16
8 of 11	EAST PLATFORM ELEVATION	03/18/16
9 of 11	WEST PLATFORM ELEVATION	03/18/16
10 of 11	PARKING GARAGE ELEVATIONS NORTH/EAST	03/18/16
11 of 11	PARKING GARAGE ELEVATIONS SOUTH/WEST	03/18/16

RIDGEGATE EAST FILING 1

RIDGEGATE PDD, 4" AMENDMENT, PLANNING AREA C/M-U #4
12.000 ACRES
SP16-11R
(RIDGEGATE STATION)



Vicinity Map

STANDARD NOTE: THE PROPERTY HEREIN IS SUBJECT TO ALL APPLICABLE REQUIREMENTS OF THE LONE TREE ZONING CODE, INCLUDING BUT NOT LIMITED TO MAINTENANCE, LIGHTING, PARKING, SIGNAGE AND OUTDOOR STORAGE, EXCEPT AS MAY OTHERWISE BE ADDRESSED IN AN APPROVED DEVELOPMENT PLAN OR SUB-AREA PLAN.

<p>OWNER RIDGEGATE INVESTMENTS, INC. 10270 COMMONWEALTH STREET, SUITE B LONE TREE, CO 80124 P: 720.279.2581 CONTACT: KEITH SIMON</p>	<p>LANDSCAPE DESIGN BRITINA DESIGN GROUP 1615 CALIFORNIA STREET, SUITE 411 DENVER, CO 80202 P: 303.456.2887 CONTACT: CHRIS COOPER</p>
<p>APPLICANT REGIONAL TRANSPORTATION DISTRICT SOUTHEAST RAIL EXTENSION 1560 BROADWAY DENVER, CO 80202 P: 303.299.6965 CONTACT: ANDY MUTZ</p>	<p>STRUCTURAL ENGINEERING SAN ENGINEERING LLC 801 WEST MINERAL AVENUE, SUITE 200 LITTLETON, CO 80120 P: 303.953.9014 CONTACT: STEVE HAKES</p>
<p>LEAD DESIGN FIRM WSP PARSONS BRINCKERHOFF 555 17TH STREET, SUITE 500 DENVER, CO 80202 P: 303.832.9091 CONTACT: KAREN CREAMER</p>	<p>LIGHTING AND ELECTRICAL DESIGN PK ELECTRICAL, INC. 5105 DTC PARKWAY, SUITE 420 GREENWOOD VILLAGE, COLORADO 80111 P: 720.481.3290 CONTACT: ALAN WISKUS</p>
<p>ARCHITECT IRON HORSE ARCHITECTS 475 17TH STREET, SUITE 720 DENVER, CO 80202 P: 720.855.7572 CONTACT: TARA BURKE</p>	<p>GEOTECHNICAL ENGINEERING YEH AND ASSOCIATES, INC. 2000 CLAY STREET, SUITE 200 DENVER, CO 80211 P: 303.781.9590 CONTACT: MICHAEL KIEFER</p>

THIS SIP HAS BEEN REVIEWED AND FOUND TO BE COMPLETE AND IN ACCORD WITH CITY REGULATIONS, AS APPROVED BY THE CITY ON (DATE).

BY: _____

NAME: _____
TITLE: COMMUNITY DEVELOPMENT DIRECTOR

DATE: _____

BY: _____

NAME: _____
TITLE: CITY ENGINEER

DATE: _____

BY: _____

NAME: _____
TITLE: MAYOR

DATE: _____

THE OWNER(S) OF THE LANDS DESCRIBED HEREIN, HEREBY AGREE(S) (1) TO DEVELOP AND MAINTAIN THE PROPERTY DESCRIBED HEREON IN ACCORDANCE WITH THIS APPROVED SITE IMPROVEMENT PLAN AND IN COMPLIANCE WITH CHAPTER 16 OF THE LONE TREE MUNICIPAL CODE AND THAT (2) THE HEIRS, SUCCESSORS AND ASSIGNS OF THE OWNER(S) SHALL ALSO BE BOUND. THE SIGNATURES OF THE OWNERS(S)/(S) REPRESENTATIVE(S) BELOW INDICATE THAT ANY REQUIRED AUTHORIZATIONS TO ENTER THIS AGREEMENT, INCLUDING ANY CORPORATE AUTHORIZATIONS, HAVE BEEN OBTAINED.

(NAME OF OWNER) _____

(SIGNATURE OF OWNER) _____

(PRINTED NAME AND TITLE) _____

STATE OF _____)

_____) ss.

COUNTY OF _____)

SUBSCRIBED AND SWORN TO BEFORE ME THIS ____ DAY OF

_____, 20____, BY _____.

WITNESS MY HAND AND OFFICIAL SEAL.
MY COMMISSION EXPIRES:

(NOTARY PUBLIC)
APPROVAL BY THE CITY OF LONE TREE DOES NOT SIGNIFY THAT THE REQUIREMENTS OF THE AMERICANS WITH DISABILITIES ACT (ADA) HAVE BEEN SATISFIED. THE APPLICANT IS RESPONSIBLE TO ENSURE THAT SAID ADA REQUIREMENTS HAVE BEEN MET.

RTD FasTracks
Southeast Rail Extension
1560 BROADWAY, SUITE 700
DENVER, COLORADO 80202
(303) 299-6990

Balfour Beatty
555 17th STREET, SUITE 400
DENVER, CO 80202

WSP | PARSONS BRINCKERHOFF
555 17TH STREET, SUITE 500
DENVER, CO 80202
P: 303.832.9091

March 18, 2016

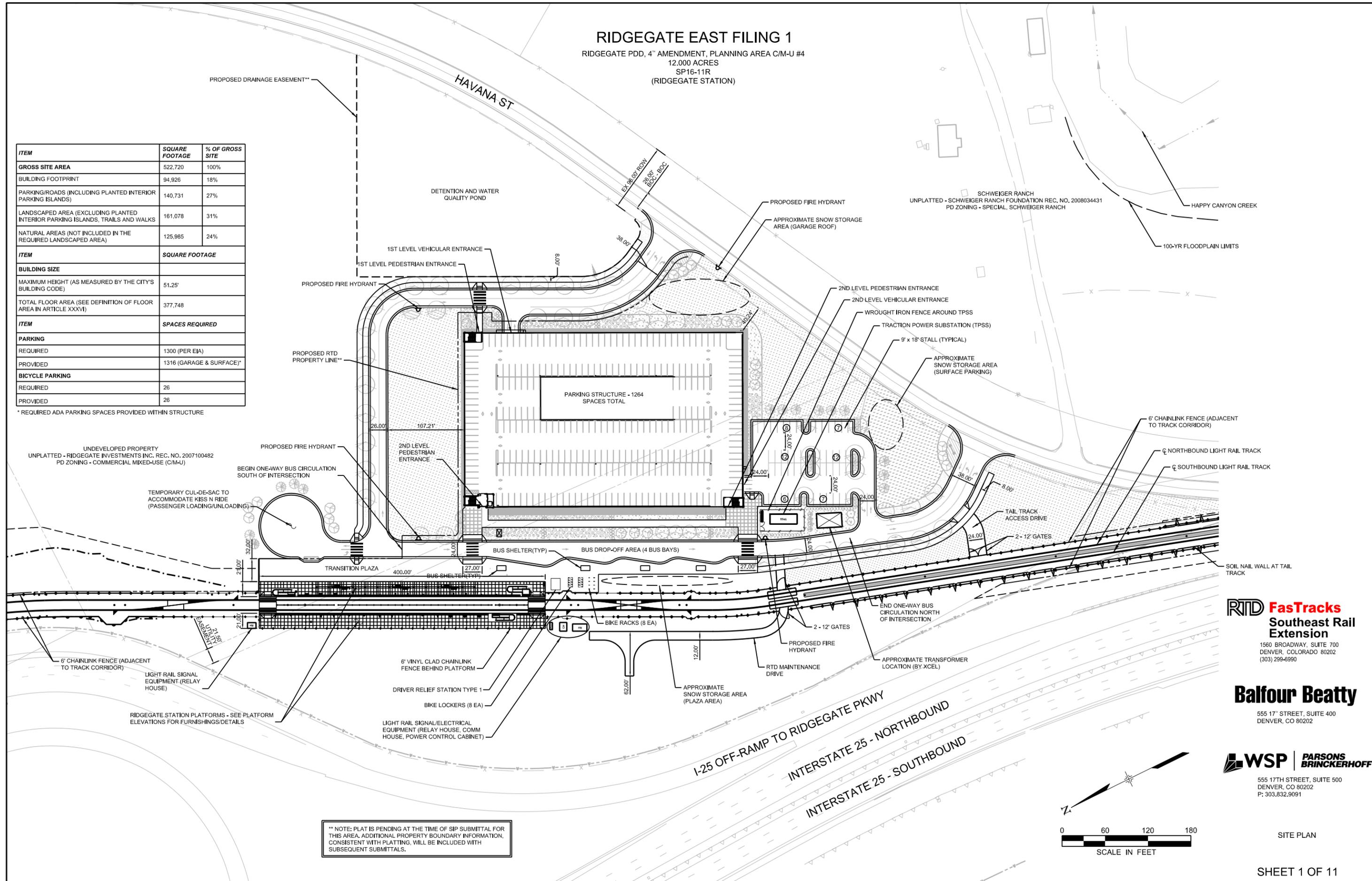
COVER

RIDGEGATE EAST FILING 1

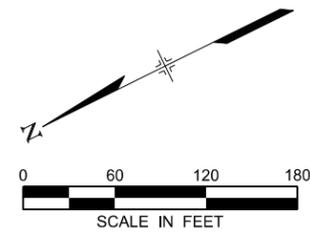
RIDGEGATE PDD, 4" AMENDMENT, PLANNING AREA C/M-U #4
12.000 ACRES
SP16-11R
(RIDGEGATE STATION)

ITEM	SQUARE FOOTAGE	% OF GROSS SITE
GROSS SITE AREA	522,720	100%
BUILDING FOOTPRINT	94,926	18%
PARKING/ROADS (INCLUDING PLANTED INTERIOR PARKING ISLANDS)	140,731	27%
LANDSCAPED AREA (EXCLUDING PLANTED INTERIOR PARKING ISLANDS, TRAILS AND WALKS)	161,078	31%
NATURAL AREAS (NOT INCLUDED IN THE REQUIRED LANDSCAPED AREA)	125,985	24%
ITEM	SQUARE FOOTAGE	
BUILDING SIZE		
MAXIMUM HEIGHT (AS MEASURED BY THE CITY'S BUILDING CODE)	51.25'	
TOTAL FLOOR AREA (SEE DEFINITION OF FLOOR AREA IN ARTICLE XXXVI)	377,748	
ITEM	SPACES REQUIRED	
PARKING		
REQUIRED	1300 (PER EIA)	
PROVIDED	1316 (GARAGE & SURFACE)*	
BICYCLE PARKING		
REQUIRED	26	
PROVIDED	26	

* REQUIRED ADA PARKING SPACES PROVIDED WITHIN STRUCTURE



** NOTE: PLAT IS PENDING AT THE TIME OF SIP SUBMITTAL FOR THIS AREA. ADDITIONAL PROPERTY BOUNDARY INFORMATION, CONSISTENT WITH PLATTING, WILL BE INCLUDED WITH SUBSEQUENT SUBMITTALS.



RTD FasTracks
Southeast Rail Extension
1560 BROADWAY, SUITE 700
DENVER, COLORADO 80202
(303) 299-6990

Balfour Beatty
555 17TH STREET, SUITE 400
DENVER, CO 80202

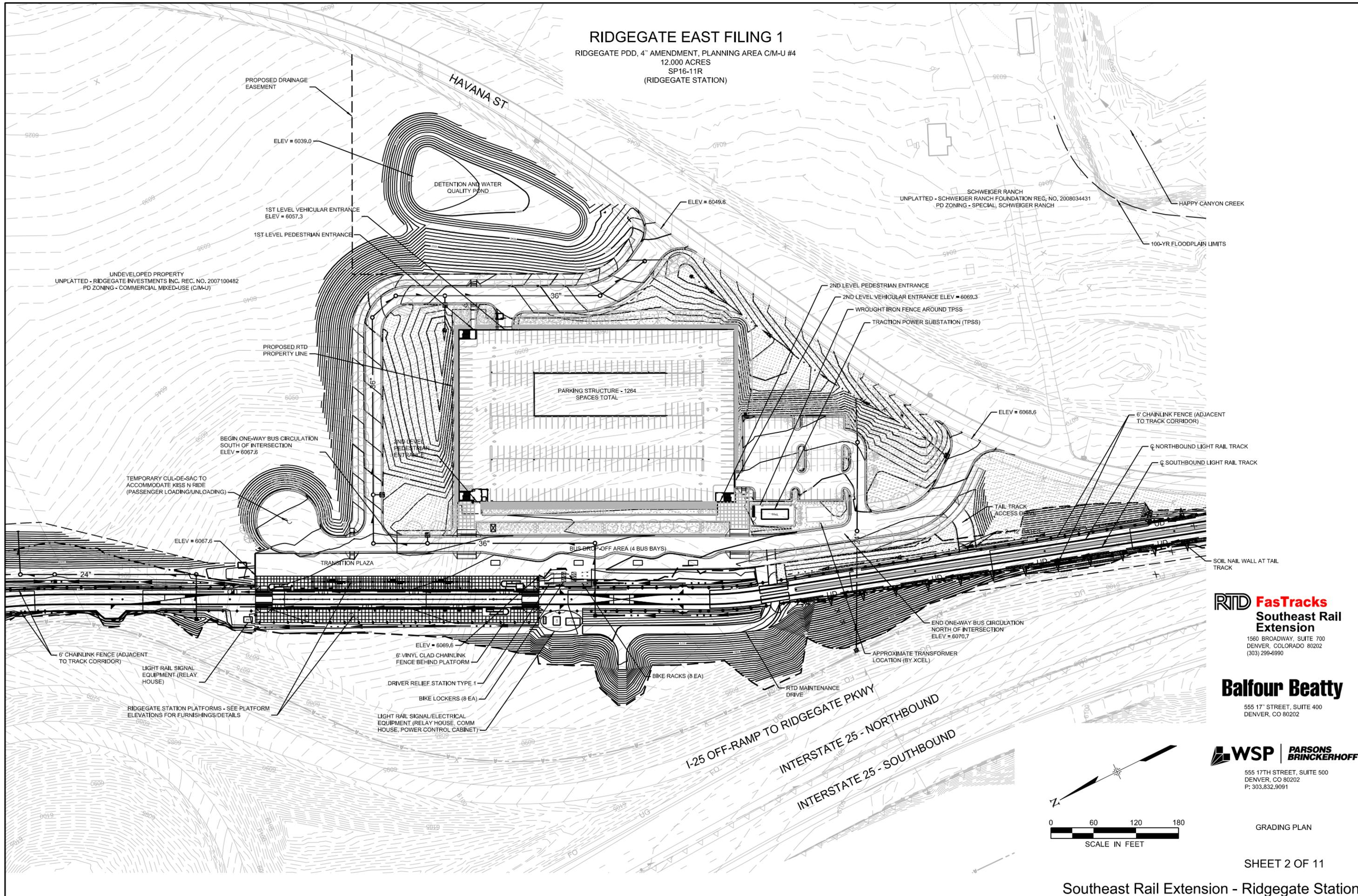
WSP | PARSONS BRINCKERHOFF
555 17TH STREET, SUITE 500
DENVER, CO 80202
P: 303.832.9091

SITE PLAN

SHEET 1 OF 11

RIDEGATE EAST FILING 1

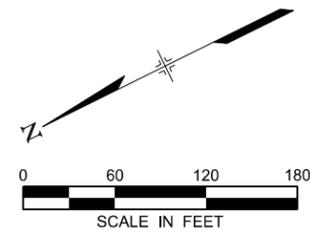
RIDEGATE PDD, 4" AMENDMENT, PLANNING AREA C/M-U #4
 12.000 ACRES
 SP16-11R
 (RIDEGATE STATION)



RTD FasTracks
Southeast Rail Extension
 1560 BROADWAY, SUITE 700
 DENVER, COLORADO 80202
 (303) 299-6990

Balfour Beatty
 555 17th STREET, SUITE 400
 DENVER, CO 80202

WSP | PARSONS BRINCKERHOFF
 555 17th STREET, SUITE 500
 DENVER, CO 80202
 P: 303.832.9091



GRADING PLAN

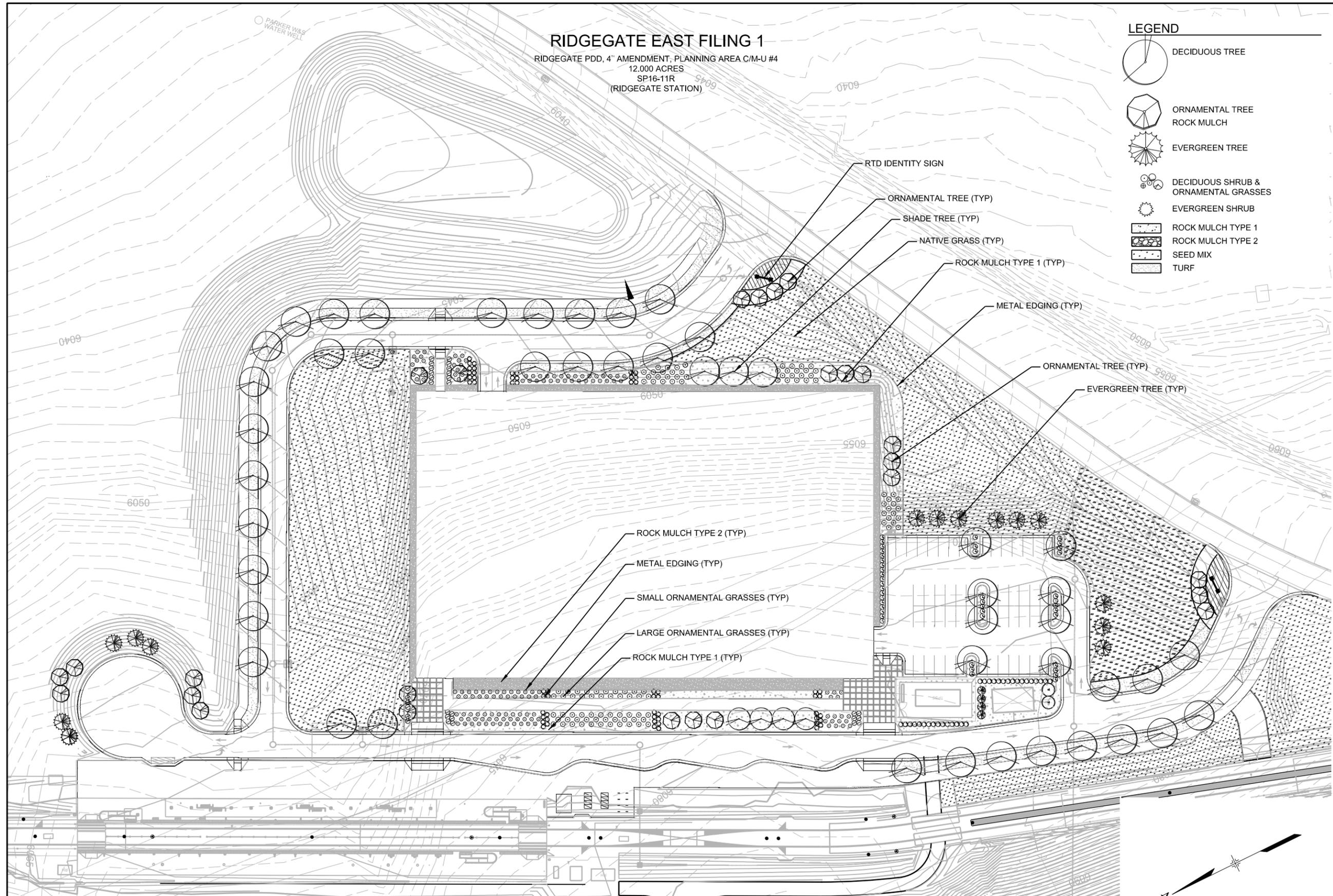
SHEET 2 OF 11

RIDGEGATE EAST FILING 1

RIDGEGATE PDD, 4" AMENDMENT, PLANNING AREA C/M-U #4
 12,000 ACRES
 SP16-11R
 (RIDGEGATE STATION)

LEGEND

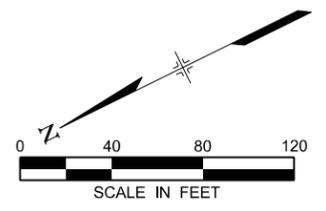
-  DECIDUOUS TREE
-  ORNAMENTAL TREE
-  ROCK MULCH
-  EVERGREEN TREE
-  DECIDUOUS SHRUB & ORNAMENTAL GRASSES
-  EVERGREEN SHRUB
-  ROCK MULCH TYPE 1
-  ROCK MULCH TYPE 2
-  SEED MIX
-  TURF



RTD FasTracks
Southeast Rail Extension
 1560 BROADWAY, SUITE 700
 DENVER, COLORADO 80202
 (303) 299-6990

Balfour Beatty
 555 17TH STREET, SUITE 400
 DENVER, CO 80202

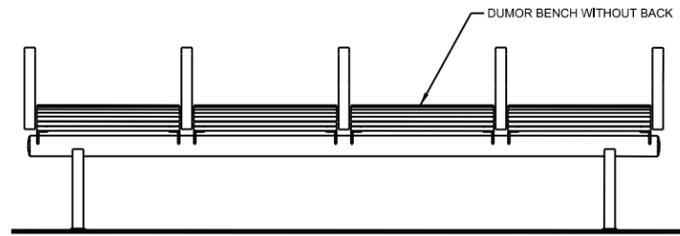
WSP | PARSONS BRINCKERHOFF
 555 17TH STREET, SUITE 500
 DENVER, CO 80202
 P: 303.832.9091



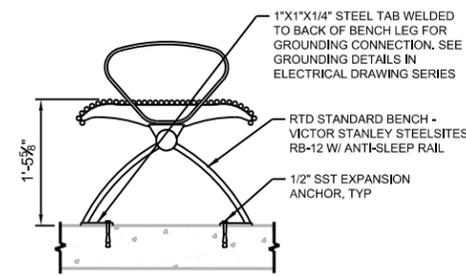
LANDSCAPE PLAN

SHEET 3 OF 11

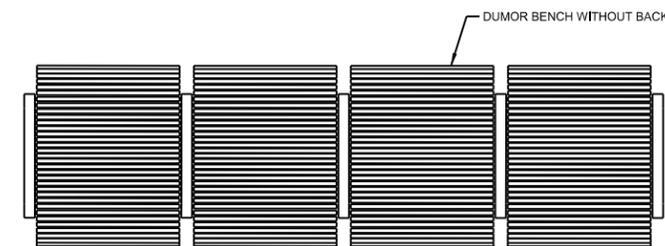
RIDGEGATE EAST FILING 1
 RIDGEGATE PDD, 4" AMENDMENT, PLANNING AREA C/M-U #4
 12.000 ACRES
 SP16-11R
 (RIDGEGATE STATION)



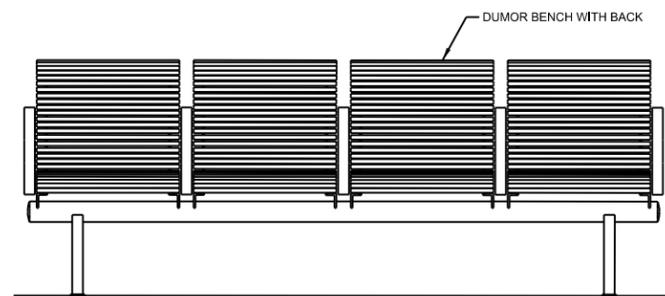
BENCH A FRONT ELEVATION



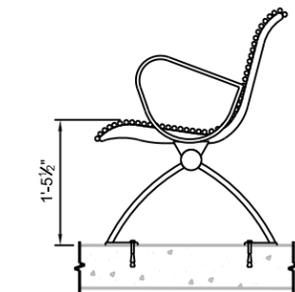
BENCH A SIDE ELEVATION



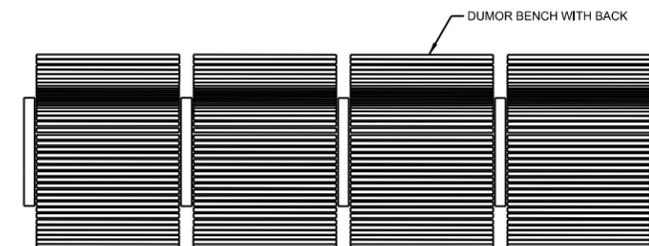
BENCH A PLAN



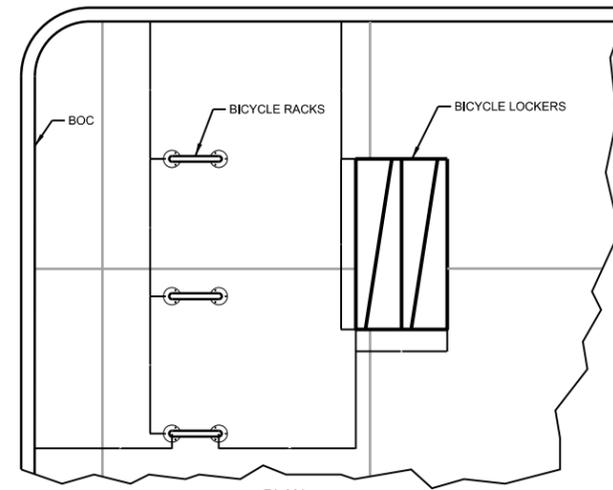
BENCH B FRONT ELEVATION



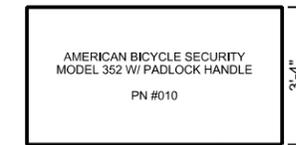
BENCH B SIDE ELEVATION



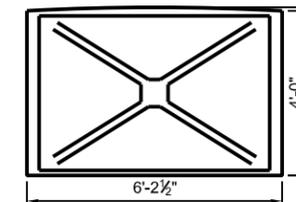
BENCH B PLAN



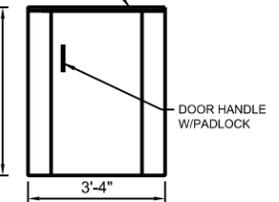
BICYCLE RACK & LOCKER LAYOUT (TYP)



TOP VIEW



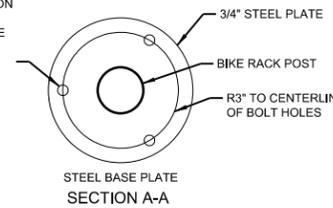
SIDE VIEW



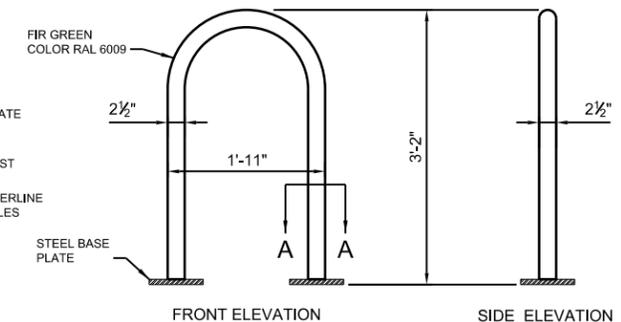
FRONT VIEW

BICYCLE LOCKERS

1/2" X 3" S.S. HILTI EXPANSION ANCHOR (TYP.) (APPLY SILICON OR POLYURETHANE TO BOLT HOLES BEFORE PLACING ANCHORS)

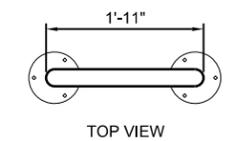


SECTION A-A



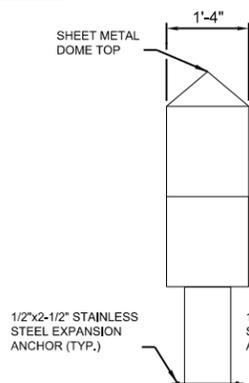
FRONT ELEVATION

SIDE ELEVATION

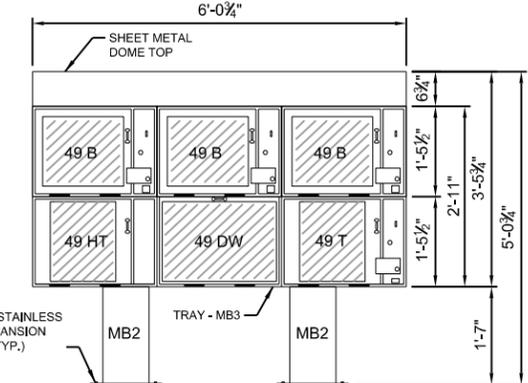


TOP VIEW

DETAIL 2 - BICYCLE RACK



SIDE ELEVATION



FRONT ELEVATION

NEWSPAPER "RAK" SYSTEMS

NOTES:

1. STANDARD COLOR SHALL BE RAL 6009 FIR GREEN
2. ALL FABRICATED COMPONENTS ARE STEEL SHOT-BLASTED, ETCHED, PHOSPHATIZED AND ELECTROSTATICALLY POWDER-COATED WITH TGIC POLYESTER POWDER COATING.
3. ALL SPECIFICATIONS ARE SUBJECT TO CHANGE. PLEASE CONTACT FACTORY FOR DETAILS.
4. ALL DIMENSIONS ARE IN INCHES.
5. AVAILABLE WITH LOCK OR LATCH
6. "RAK" SYSTEM MODEL NOS. AND DESCRIPTIONS AVAILABLE FROM:
 RAK SYSTEMS, INC.
 5500 PLANTATION RD.
 THEODORE, AL 36582 U.S.A.
 TOLL FREE: (800) 467-1725
 LOCAL: (251) 653-4080
 FAX: (251) 653-1014

RTD FasTracks
Southeast Rail Extension
 1560 BROADWAY, SUITE 700
 DENVER, COLORADO 80202
 (303) 299-6990

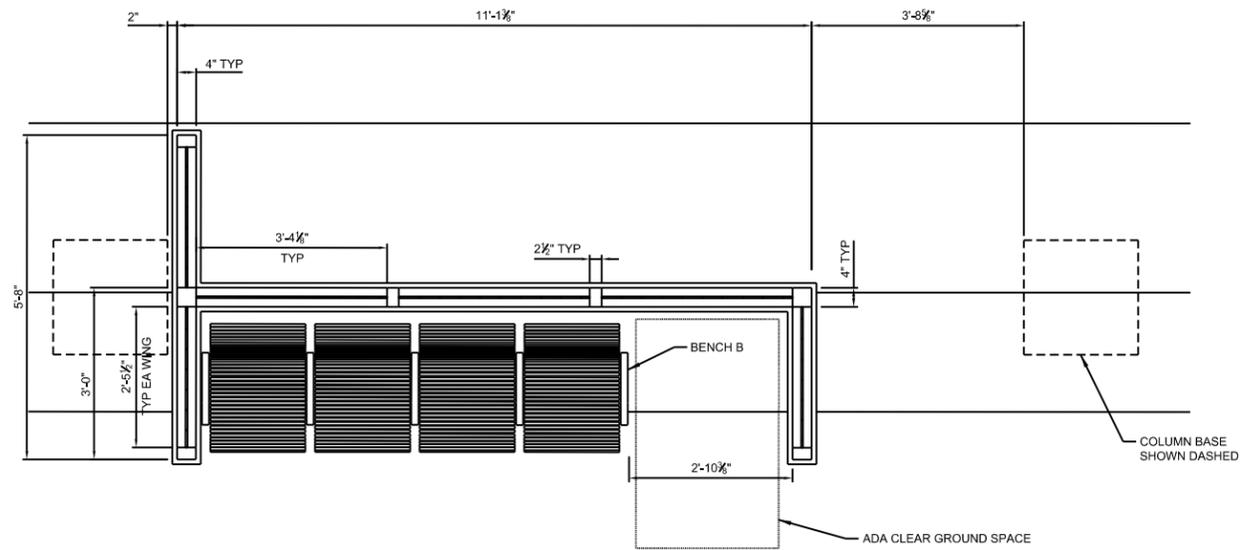
Balfour Beatty
 555 17th STREET, SUITE 400
 DENVER, CO 80202

WSP | PARSONS BRINCKERHOFF
 555 17th STREET, SUITE 500
 DENVER, CO 80202
 P: 303.832.9091

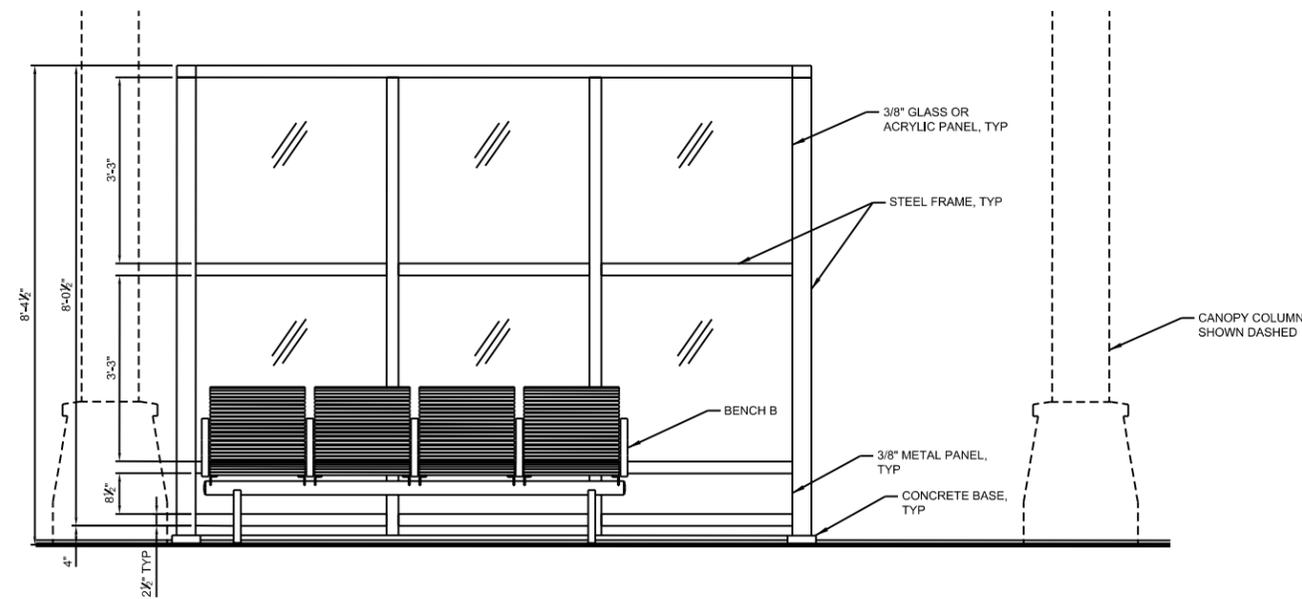
SITE FURNISHINGS

SHEET 4 OF 12

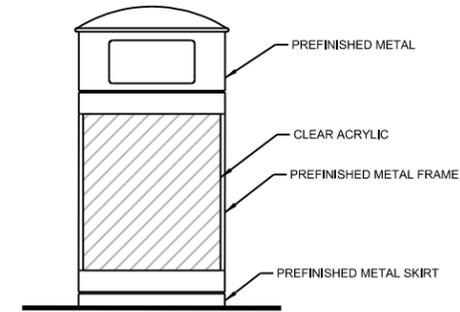
RIDGEGATE EAST FILING 1
 RIDGEGATE PDD, 4" AMENDMENT, PLANNING AREA C/M-U #4
 12.000 ACRES
 SP16-11R
 (RIDGEGATE STATION)



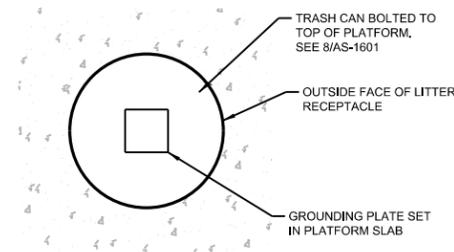
WINDSCREEN TYPE 2 PLAN



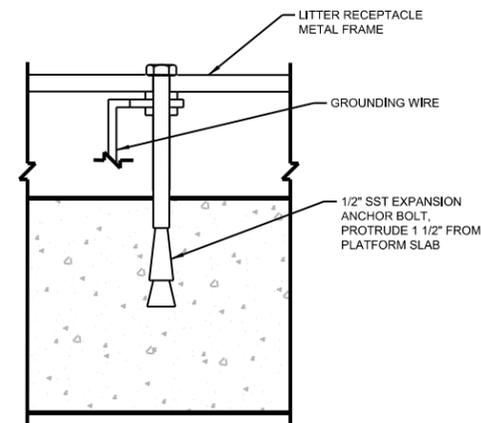
WINDSCREEN TYPE 2 FRONT ELEVATION



LITTER RECEPTACLE ELEVATION



LITTER RECEPTACLE PLAN



LITTER RECEPTACLE GROUNDING CONNECTION

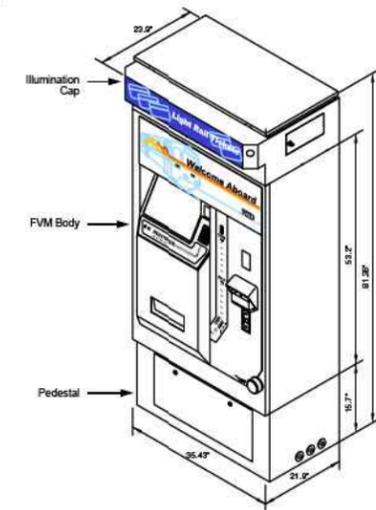


Figure 11-2. FVM Dimensions

FVM	FVM Unpacked	FVM Packed
Width	35.4"	Approximately 44"
Height	63.2"	Approximately 63"
Depth	21.0"	Approximately 31"
Weight	Approximately 551 lbs.	Approximately 591 lbs.

Table 11-1 FVM Dimensions and Weight

TICKET VENDING MACHINE (TVM)

RTD FasTracks
Southeast Rail Extension
 1560 BROADWAY, SUITE 700
 DENVER, COLORADO 80202
 (303) 299-6990

Balfour Beatty
 555 17TH STREET, SUITE 400
 DENVER, CO 80202

WSP | PARSONS BRINCKERHOFF
 555 17TH STREET, SUITE 500
 DENVER, CO 80202
 P: 303.832.9091

SITE FURNISHINGS

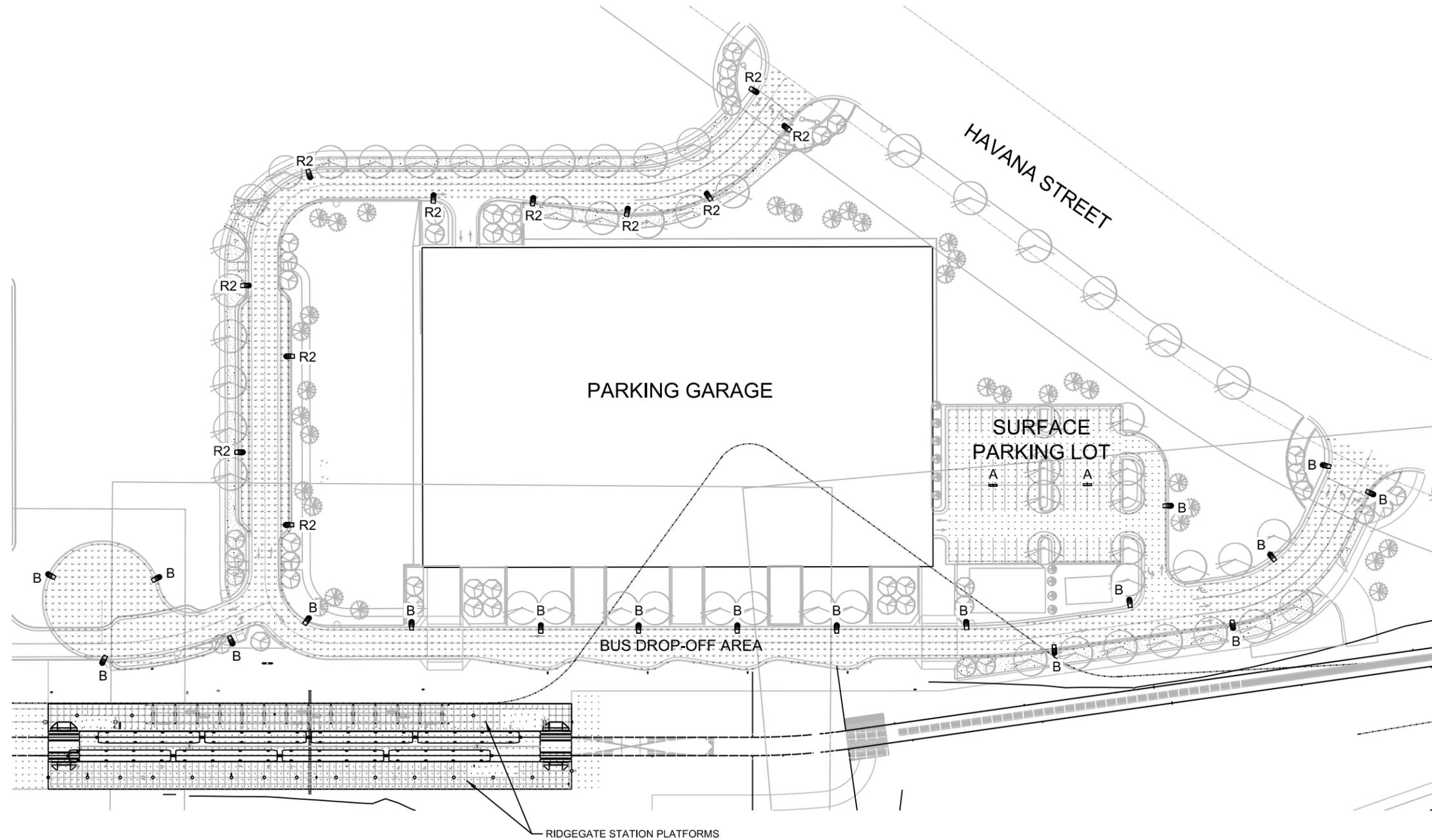
SHEET 5 OF 12



Know what's below.
Call before you dig.

RIDGEGATE EAST FILING 1

RIDGEGATE PDD, 4" AMENDMENT, PLANNING AREA C/M-U #4
12.000 ACRES
SP16-11R
(RIDGEGATE STATION)



RTD FasTracks
Southeast Rail Extension
 1560 BROADWAY, SUITE 700
 DENVER, COLORADO 80202
 (303) 299-6990

Balfour Beatty
 555 17TH STREET, SUITE 400
 DENVER, CO 80202

WSP | PARSONS BRINCKERHOFF
 555 17TH STREET, SUITE 500
 DENVER, CO 80202
 P: 303.832.9091

PHOTOMETRICS PLAN

SHEET 6 OF 11

RIDGEGATE EAST FILING 1
 RIDGEGATE PDD, 4" AMENDMENT, PLANNING AREA C/M-U #4
 12.000 ACRES
 SP16-11R
 (RIDGEGATE STATION)

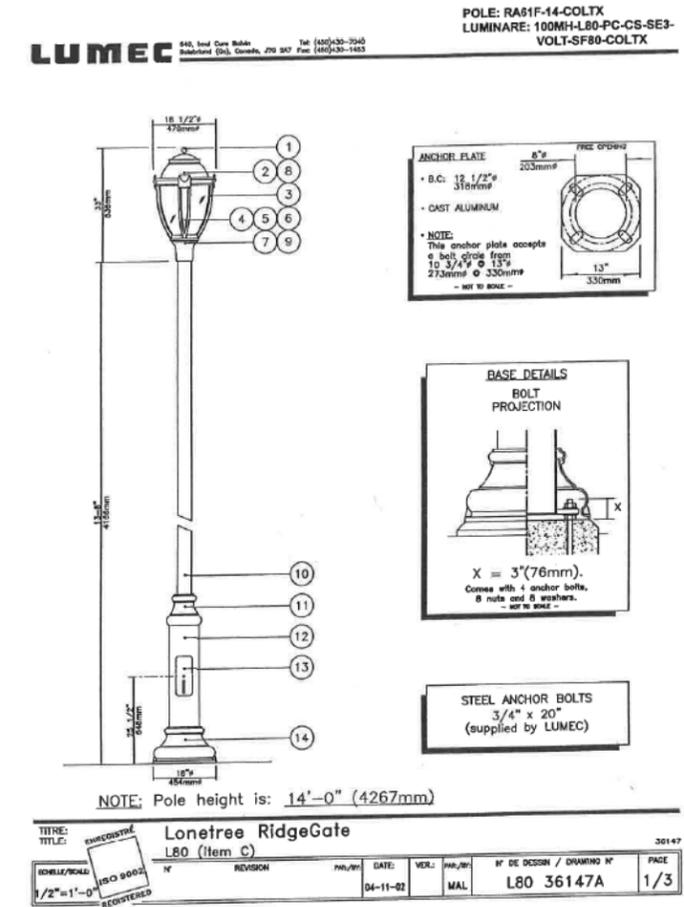
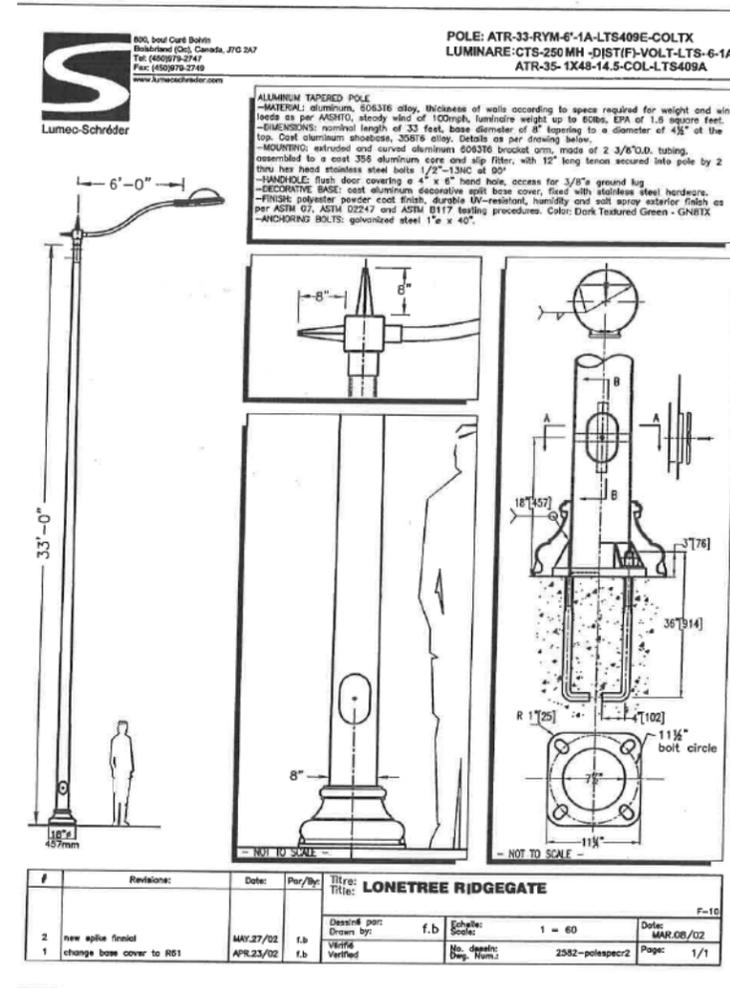


Figure 16: Pedestrian Light Specification

RTD FasTracks
Southeast Rail Extension
 1560 BROADWAY, SUITE 700
 DENVER, COLORADO 80202
 P: 303-299-6990

Balfour Beatty
 555 17th STREET, SUITE 400
 DENVER, CO 80202

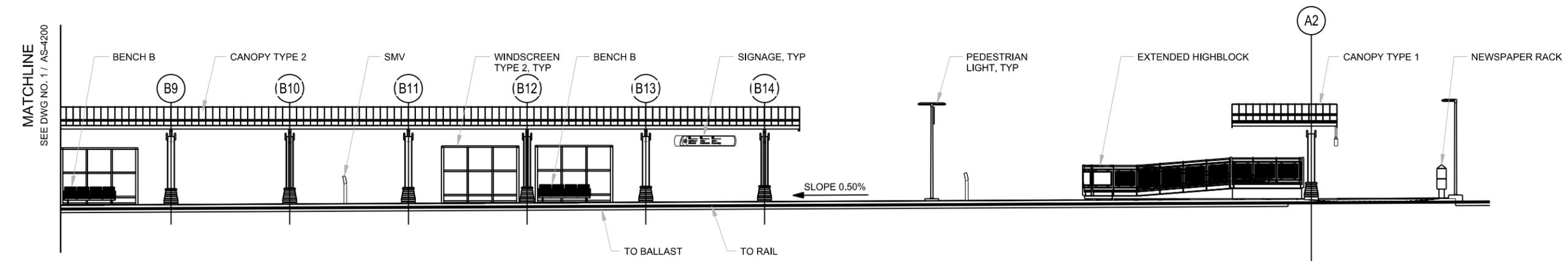
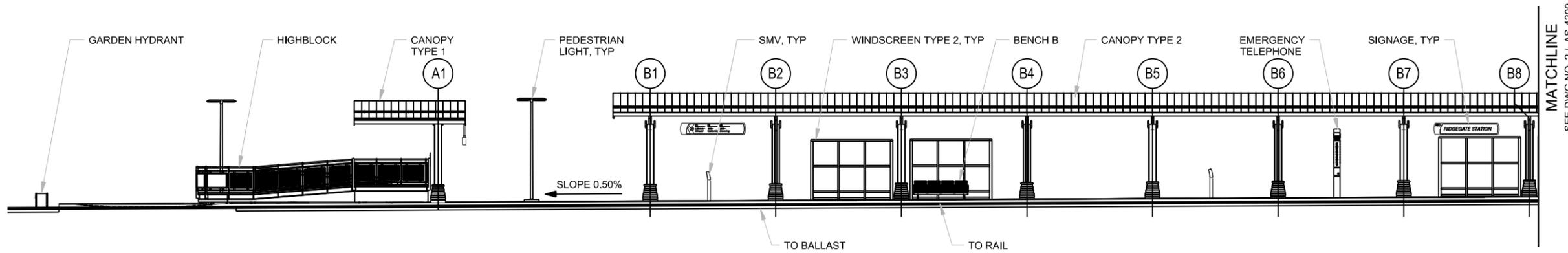
WSP | PARSONS BRINCKERHOFF
 555 17TH STREET, SUITE 500
 DENVER, CO 80202
 P: 303.832.9091

LIGHTING FEATURES

SHEET 7 OF 11

RIDGEGATE EAST FILING 1

RIDGEGATE PDD, 4" AMENDMENT, PLANNING AREA C/M-U #4
 12.000 ACRES
 SP16-11R
 (RIDGEGATE STATION)



RTD FasTracks
Southeast Rail Extension
 1560 BROADWAY, SUITE 700
 DENVER, COLORADO 80202
 (303) 299-6990

Balfour Beatty
 555 17TH STREET, SUITE 400
 DENVER, CO 80202

WSP | PARSONS BRINCKERHOFF
 555 17TH STREET, SUITE 500
 DENVER, CO 80202
 P: 303.832.9091

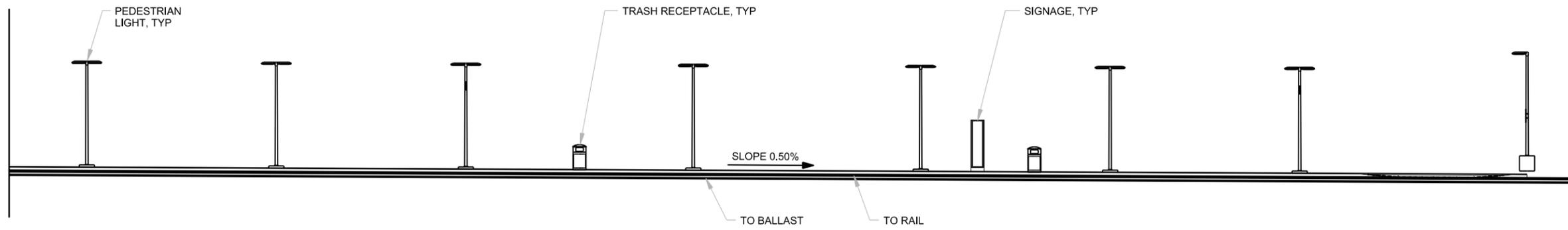
EAST PLATFORM ELEVATION

SHEET 8 OF 11

RIDGEGATE EAST FILING 1

RIDGEGATE PDD, 4" AMENDMENT, PLANNING AREA C/M-U #4
 12.000 ACRES
 SP16-11R
 (RIDGEGATE STATION)

MATCHLINE
SEE DWG NO. 2 / AS-4201



GARDEN HYDRANT

SIGN POLE

PEDESTRIAN LIGHT, TYP

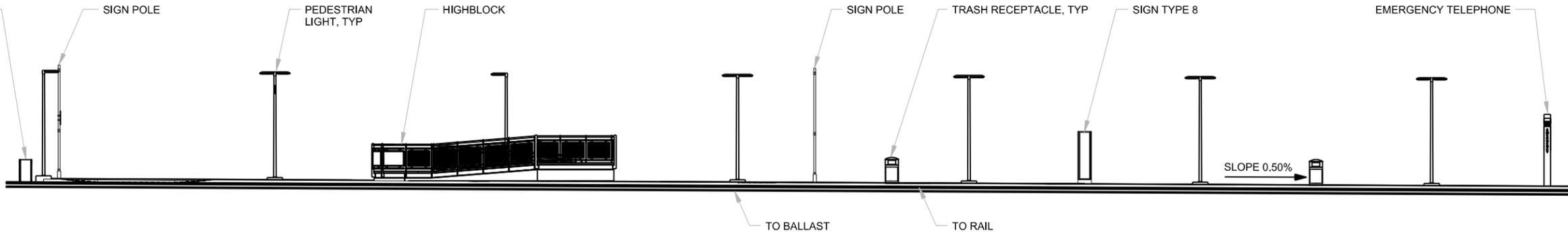
HIGHBLOCK

SIGN POLE

TRASH RECEPTACLE, TYP

SIGN TYPE 8

EMERGENCY TELEPHONE



MATCHLINE
SEE DWG NO. 1 / AS-4201

RTD FasTracks
Southeast Rail Extension
 1560 BROADWAY, SUITE 700
 DENVER, COLORADO 80202
 (303) 299-6990

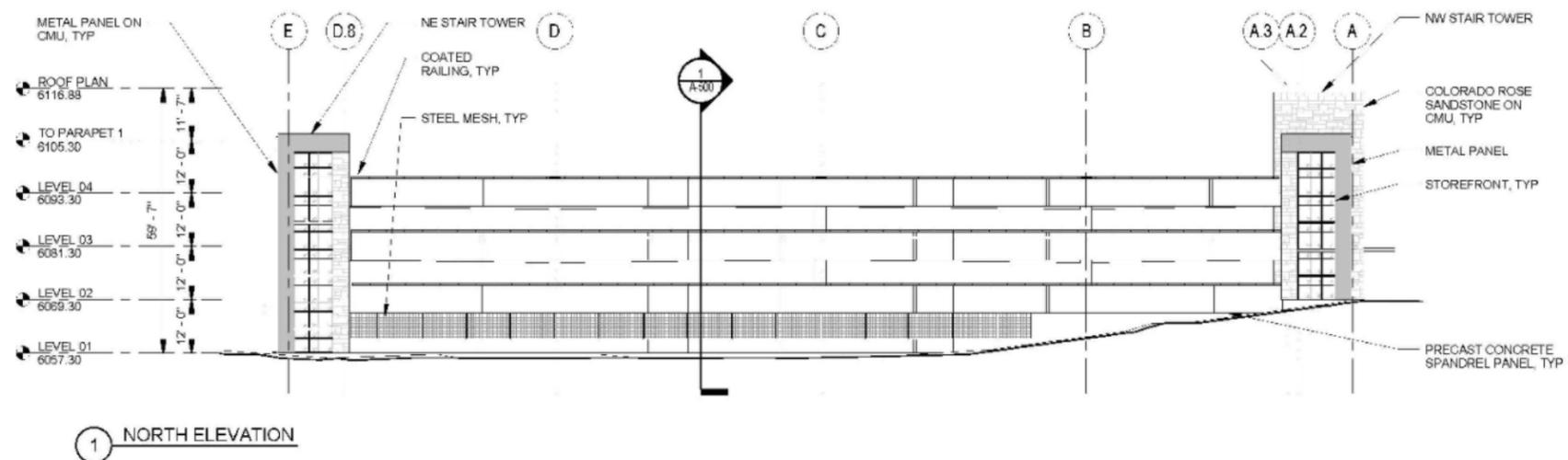
Balfour Beatty
 555 17TH STREET, SUITE 400
 DENVER, CO 80202

WSP | PARSONS BRINCKERHOFF
 555 17TH STREET, SUITE 500
 DENVER, CO 80202
 P: 303.832.9091

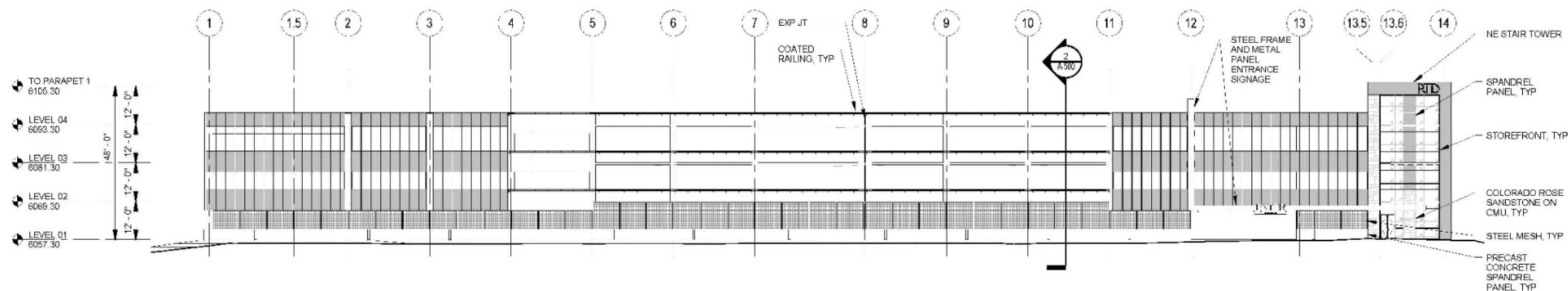
WEST PLATFORM ELEVATION

SHEET 9 OF 11

RIDGEGATE EAST FILING 1
 RIDGEGATE PDD, 4" AMENDMENT, PLANNING AREA C/M-U #4
 12.000 ACRES
 SP16-11R
 (RIDGEGATE STATION)



1 NORTH ELEVATION



2 EAST ELEVATION

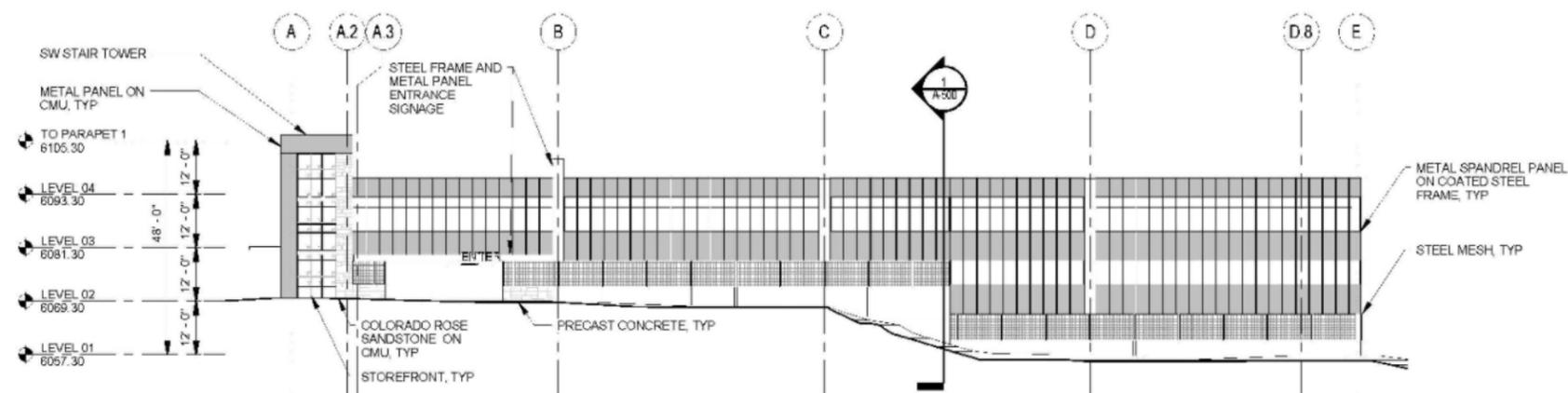
RTD FasTracks
Southeast Rail Extension
 1560 BROADWAY, SUITE 700
 DENVER, COLORADO 80202
 (303) 299-6990

Balfour Beatty
 555 17TH STREET, SUITE 400
 DENVER, CO 80202

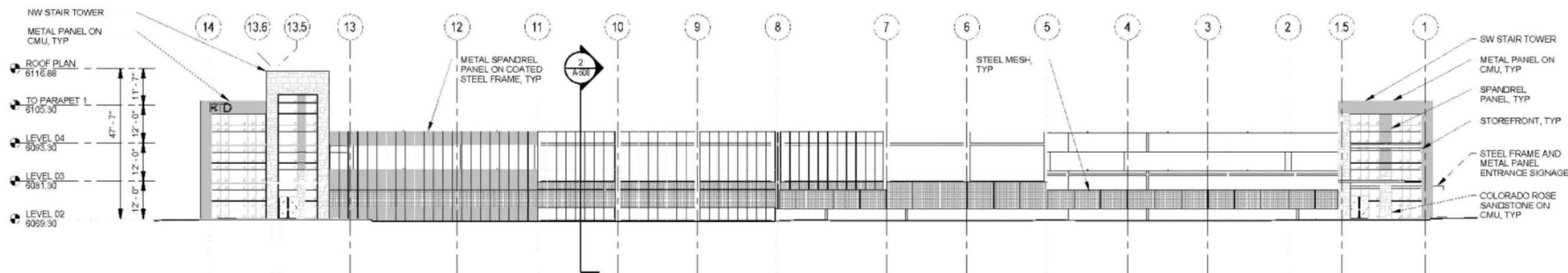
WSP | PARSONS BRINCKERHOFF
 555 17TH STREET, SUITE 500
 DENVER, CO 80202
 P: 303.832.9091

PARKING GARAGE NORTH/EAST
 ELEVATIONS

RIDGEGATE EAST FILING 1
 RIDGEGATE PDD, 4" AMENDMENT, PLANNING AREA C/M-U #4
 12.000 ACRES
 SP16-11R
 (RIDGEGATE STATION)



① SOUTH ELEVATION



② WEST ELEVATION

RTD FasTracks
Southeast Rail Extension
 1560 BROADWAY, SUITE 700
 DENVER, COLORADO 80202
 (303) 299-6990

Balfour Beatty
 555 17TH STREET, SUITE 400
 DENVER, CO 80202

WSP | PARSONS BRINCKERHOFF
 555 17TH STREET, SUITE 500
 DENVER, CO 80202
 P: 303.832.9091

PARKING GARAGE SOUTHWEST
 ELEVATION



RTD FasTracks
Southeast Rail Extension
 1560 BROADWAY, SUITE 700
 DENVER, COLORADO 80202
 (303) 299-6990

Balfour Beatty
 555 17TH STREET, SUITE 400
 DENVER, CO 80202

WSP | PARSONS BRINCKERHOFF
 555 17TH STREET, SUITE 500
 DENVER, CO 80202
 P: 303.832.9091

IRON HORSE ARCHITECTS
 RESPONSIBLE DESIGN
 475 17TH STREET, SUITE 720
 DENVER, CO 80202

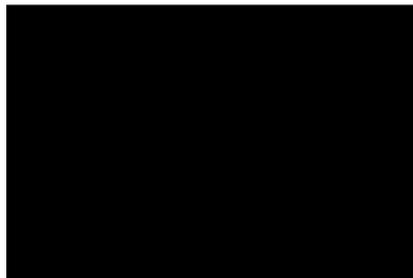


1615 CALIFORNIA STREET, SUITE 411
 DENVER, CO 80202
 P: 303.456.2887



**Pre-Finished Standing Seam
Berridge Shasta White**

Canopy Roof



**Painted/Pre-Finished Metal
Black RAL 9005**

Windscreen Structure and Panels,
Railings, Benches, Light Poles,
Trash Receptacles

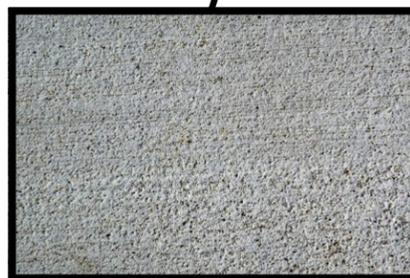


**Painted/Pre-Finished Metal
Tnemec White 00WH**

Canopy Structure
VMS
Canopy Deck



**Tactile Warning Strip
Federal Standard #33538**



**Concrete
Medium Broom Finish**
Platform, Plaza, Sidewalks



**Stone Veneer
Colorado Rose Sandstone, Buff Finish**
Canopy Column Bases



**Stone Veneer
Colorado Sandstone, Buff Finish**
Canopy Column Bases

RTD FasTracks
**Southeast Rail
Extension**
1560 BROADWAY, SUITE 700
DENVER, COLORADO 80202
P: 303.299.6890

Balfour Beatty
555 17TH STREET, SUITE 400
DENVER, CO 80202

**WSP | PARSONS
BRINCKERHOFF**
555 17TH STREET, SUITE 500
DENVER, CO 80202
P: 303.832.9091

IRON HORSE
ARCHITECTS
RESPONSIBLE DESIGN
475 17TH STREET, SUITE 720
DENVER, CO 80202



BRITINA
design group
1615 CALIFORNIA STREET, SUITE 411
DENVER, CO 80202
P: 303.456.2887



**Pre-Finished Standing Seam
Berridge Shasta White**

Roof



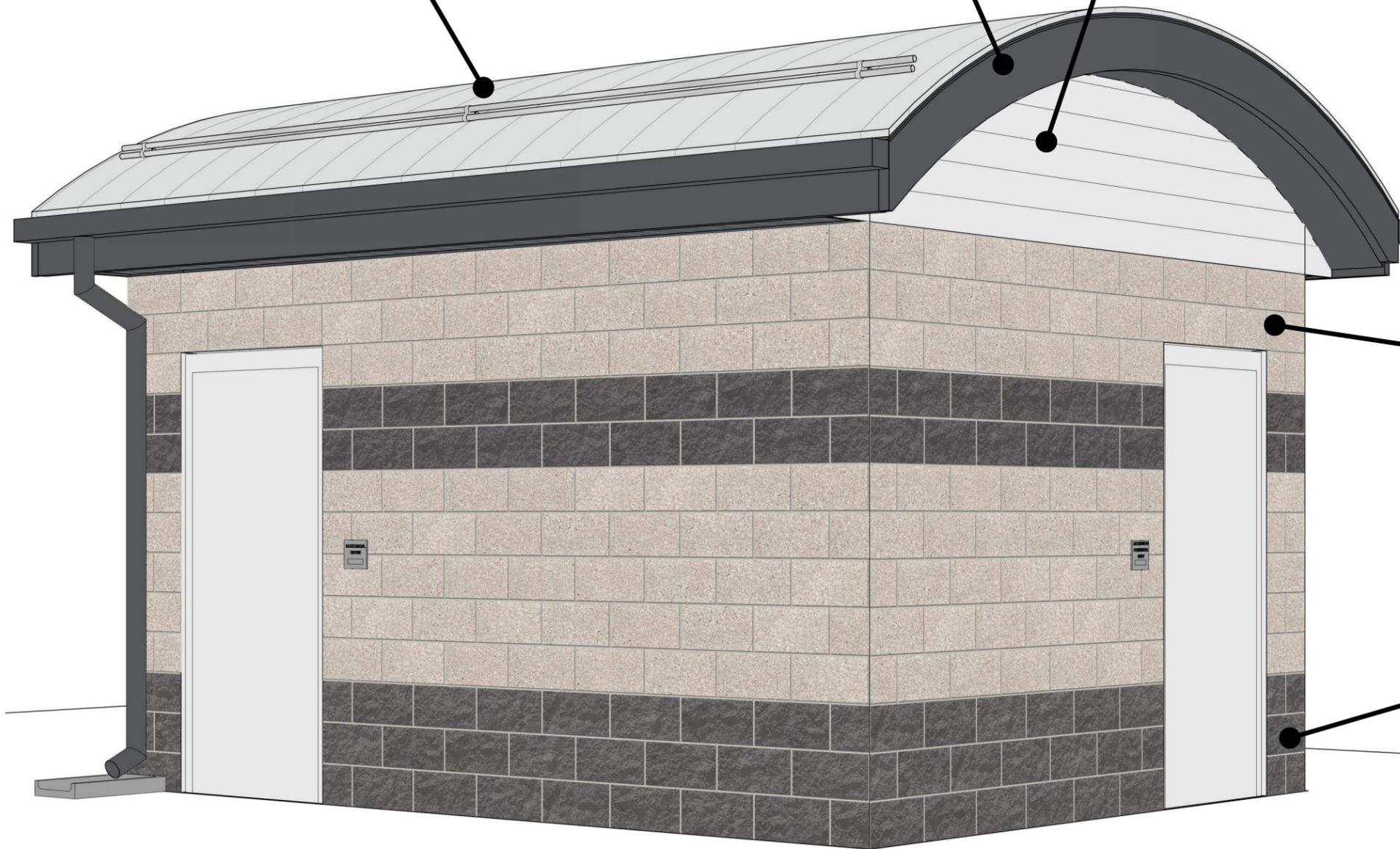
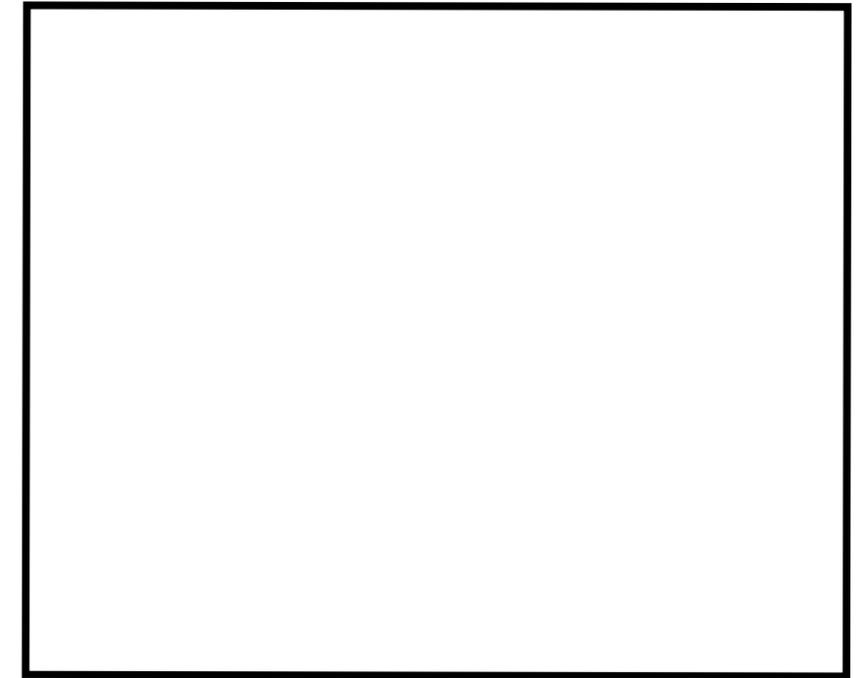
**Painted/Pre-Finished Metal
Tnemec No. 2 Pencil**

Gutter, Downspout, Soffit, Fascia



**Painted/Pre-Finished Metal
Tnemec Winter Mist**

Doors, Trim, Metal Panel



**Glass
Oldcastle Clear**

Windscreens



**CMU Base Course
Basalite 720 WR Ground Face**

Exterior walls



**CMU Accent Course
Basalite 807 WR Split Face**

Exterior walls

RTD FasTracks
**Southeast Rail
Extension**
1560 BROADWAY, SUITE 700
DENVER, COLORADO 80202
P: 303.299.6890

Balfour Beatty
555 17TH STREET, SUITE 400
DENVER, CO 80202

**WSP | PARSONS
BRINCKERHOFF**
555 17TH STREET, SUITE 500
DENVER, CO 80202
P: 303.832.9091

IRON HORSE
ARCHITECTS
RESPONSIBLE DESIGN
475 17TH STREET, SUITE 720
DENVER, CO 80202



BRITINA
design group
1615 CALIFORNIA STREET, SUITE 411
DENVER, CO 80202
P: 303.456.2887



Precast Spandrel Panel
Buff / Tan, Sandblasted



Storefront Glazing
Clear, Pilkington Energy Advantage Low E
Stair Towers



Painted/Pre-Finished Metal
FLEXOSPAN 380 Charcoal
Metal Panel
Pedestrian Canopies
Storefront Mullions
Metal Frame
Vehicular Entry Signage



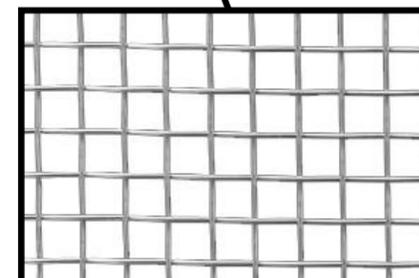
Stone Veneer
Colorado Rose Sandstone,
Coarse Ashlar
Stair Tower Bases
Columns



Painted/Pre-Finished Metal
FLEXOSPAN 320 Parchment,
Ribbed Panel
Metal Panels



Painted Concrete
Hazelnut, TNEMEC Paint
Upturned Beams



4" Square Wire Mesh
Stainless Steel
Ground Levels



RTD FasTracks
Southeast Rail Extension
1560 BROADWAY, SUITE 700
DENVER, COLORADO 80202
(303) 299-6890

Balfour Beatty
555 17TH STREET, SUITE 400
DENVER, CO 80202

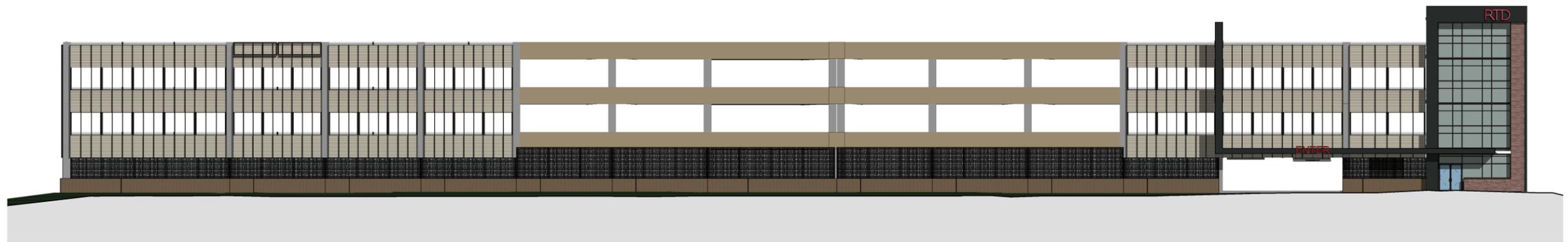
WSP | PARSONS BRINCKERHOFF
555 17TH STREET, SUITE 500
DENVER, CO 80202
P: 303.832.9091

IRON HORSE ARCHITECTS
RESPONSIBLE DESIGN
475 17TH STREET, SUITE 720
DENVER, CO 80202



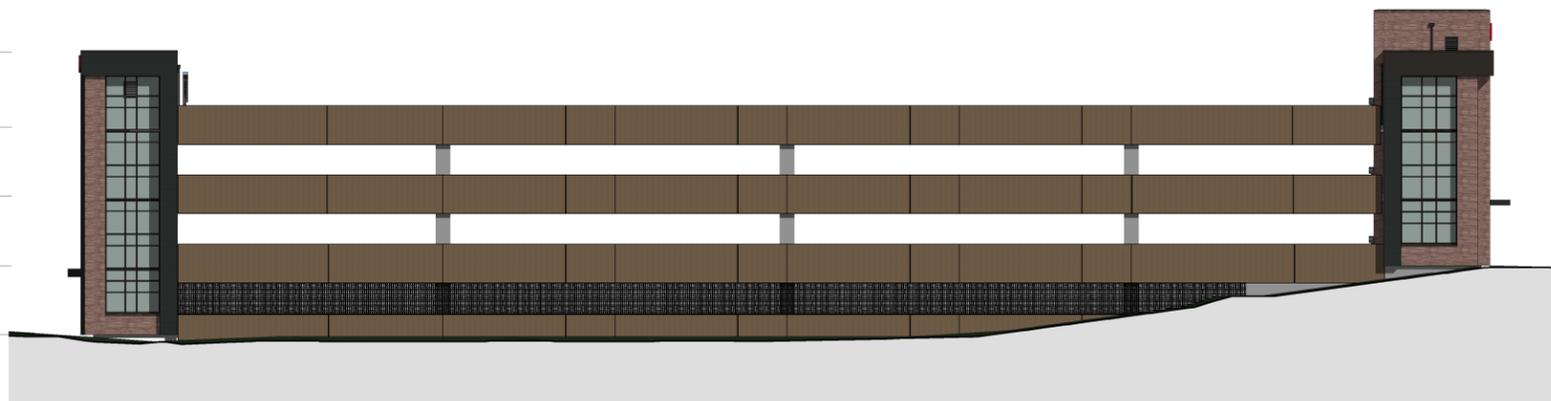
BRITINA
design group
1615 CALIFORNIA STREET, SUITE 411
DENVER, CO 80202
P: 303.456.2887

- TO PARAPET 1
149.00
- LEVEL 04
136.00
- LEVEL 03
124.00
- LEVEL 02
112.00
- LEVEL 01
100.00



East Elevation

- TO PARAPET 1
149.00
- LEVEL 04
136.00
- LEVEL 03
124.00
- LEVEL 02
112.00
- LEVEL 01
100.00



North Elevation

RTD FasTracks
Southeast Rail
Extension
1560 BROADWAY, SUITE 700
DENVER, COLORADO 80202
(303) 299-6990

Balfour Beatty
555 17TH STREET, SUITE 400
DENVER, CO 80202

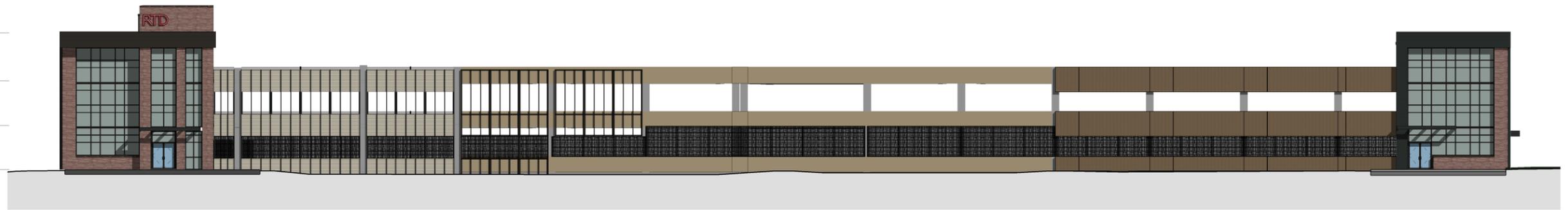
**WSP | PARSONS
BRINCKERHOFF**
555 17TH STREET, SUITE 500
DENVER, CO 80202
P: 303.832.9091

IRON HORSE
ARCHITECTS
RESPONSIBLE DESIGN
475 17TH STREET, SUITE 720
DENVER, CO 80202



BRITINA
design group
1615 CALIFORNIA STREET, SUITE 411
DENVER, CO 80202
P: 303.456.2887

- ROOF PLAN
159.58
- TO PARAPET 1
149.00
- LEVEL 04
136.00
- LEVEL 03
124.00
- LEVEL 02
112.00



West Elevation

RTD FasTracks
Southeast Rail Extension
 1560 BROADWAY, SUITE 700
 DENVER, COLORADO 80202
 (303) 299-6990

Balfour Beatty
 555 17TH STREET, SUITE 400
 DENVER, CO 80202

WSP | PARSONS BRINCKERHOFF
 555 17TH STREET, SUITE 500
 DENVER, CO 80202
 P: 303.832.9091

IRON HORSE ARCHITECTS
 RESPONSIBLE DESIGN
 475 17TH STREET, SUITE 720
 DENVER, CO 80202



BRITINA
 design group
 1615 CALIFORNIA STREET, SUITE 411
 DENVER, CO 80202
 P: 303.456.2887

- TO PARAPET 1
149.00
- LEVEL 04
136.00
- LEVEL 03
124.00
- LEVEL 02
112.00
- LEVEL 01
100.00



South Elevation