



RidgeGate at Lone Tree

Office District Subarea Plan
2001

Revised May 2006

Ridgegate
Lone Tree, Colorado
Office District Subarea Plan
2001

Revised May 2006

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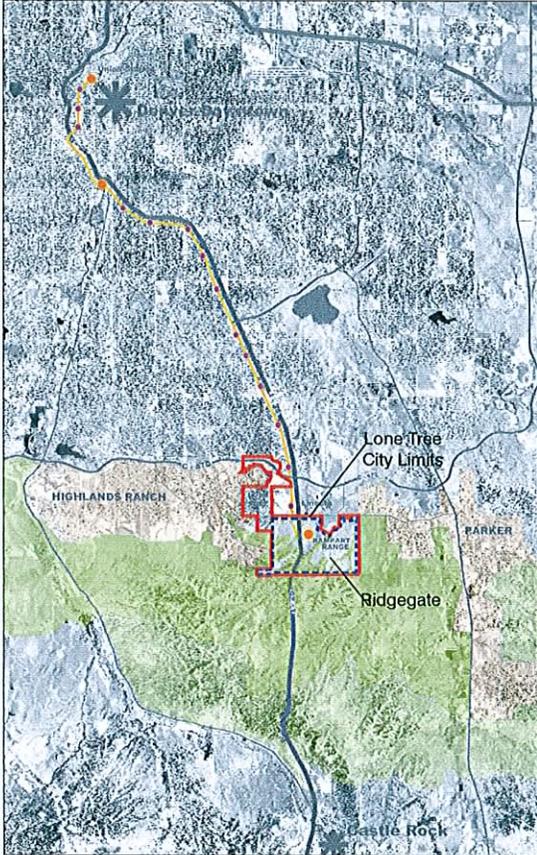
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+01/ Ridgegate Planned Development District (PDD)

1.1 Planning Concept

Exhibit 1.0: PDD Zoning Plan - Overall Project



Airphoto of the south metro Denver region and Ridgegate's strategic location to leverage transportation and open space assets.

Ridgegate consists of approximately 3,500 undeveloped acres and is located in northern Douglas County, Colorado, in the City of Lone Tree. Ridgegate's position at the natural edge of the metro-Denver area and its adjacency to existing surface and planned light-rail transportation systems create a unique set of circumstances and opportunities.

Ridgegate is envisioned as an example for smart growth along Colorado's Front Range. It locates an integrated community of housing, employment and services immediately adjacent to existing urban development. Ridgegate's development framework applies this locational advantage to create a series of planning areas - relatively concentrated, efficient mixed-use neighborhoods that help to reduce the cost of infrastructure, services and transportation.

The Planning Areas have distinct edges defined by an open space system that also serve to protect sensitive watersheds and habitat and provide a framework for an integrated trail system.

Individual Subarea Plans and related Design Guidelines will be developed prior to or concurrent with platting. Subarea Plans may include complete or partial planning areas, or a combination of multiple planning areas. These will serve to further define development standards that allow for flexibility to respond to market conditions, yet provide specificity so that development implements the planning concepts. Any conflict in provision(s) between the PDD and the Subarea Plans and Design Guidelines will be resolved by the most restrictive or higher standard(s) or requirement(s) governing, unless determined otherwise by the City of Lone Tree Director of Community Development (the "Director," as referred to in the City of Lone Tree's Planned Development District Ordinance General Provisions), in consultation with the City Council.

1.2 Planning Framework

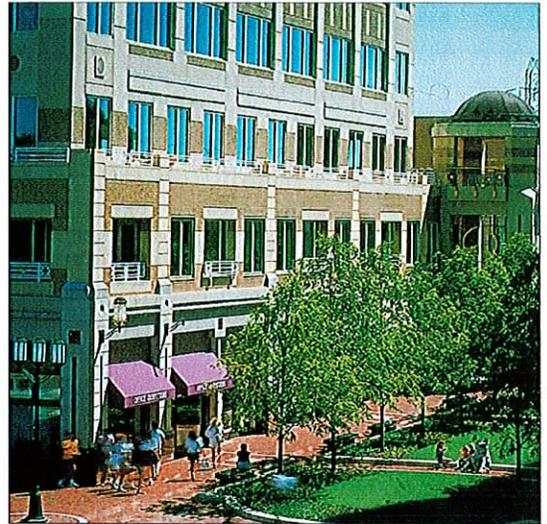
City Center Planning Area

The City Center Planning Area is a compact, pedestrian oriented and mixed-use environment providing a wealth of choice for retail, business, civic, cultural and residential uses as well as an anchor for the community. A multi-modal transportation center that further supports the area as a center of activity will be located in the City Center.

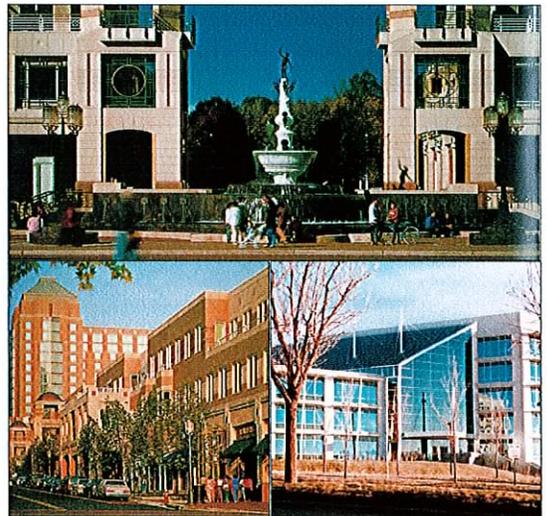
The City Center is sited at a knoll that affords visibility from I-25 and views to the Front Range. It is adjacent to one of the Ridgeway's major open space systems to provide easy access and establish the relationship of "a city in a landscape". The City Center Planning Area is planned to create an urbanized environment with buildings at the edges of sidewalks, on-street and structured parking, an interconnected street network and a commitment to high quality and functional public spaces. It is also envisioned that this area may become the location for the City of Lone Tree's municipal offices, government offices and a range of cultural facilities. Office and other non-residential components will be required to meet a minimum FAR in order to support the character and intensity required to create a vibrant urban environment. Residential components are expected to be of a higher density with only limited single-family detached homes allowed.

Commercial/Mixed-use Planning Area

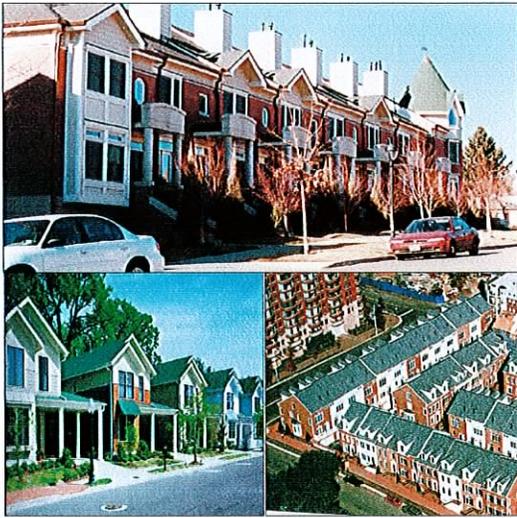
The Commercial/ Mixed-use Planning Areas are mixed-use neighborhoods with an emphasis on commercial uses. They arrange residential, office and retail uses around a central focus such as public spaces and buildings that are generally within walking distance to the edge of the area. The Commercial/ Mixed-use Planning Areas are intended to create a balanced mix of uses in a pedestrian friendly environment with an interconnected street network and multiple connections to open space systems that define edges of the district. Residential components are expected to be of a higher density and will buffer themselves from adjacent non-residential components. It is expected that some residential components may be located on floors above non-residential uses and other creative approaches of housing densities and styles will be encouraged.



City Center development character



Commercial/Mixed-Use development character



Residential/Mixed-Use development character

Residential/Mixed-use Planning Area

The Residential/Mixed-use Planning Areas are mixed-use neighborhoods with an emphasis on residential uses. They arrange residential, office and retail uses around a central focus such as public spaces and buildings that are generally within walking distance to the edge of the area. The Residential/Mixed-use Planning Areas are intended to create a balanced mix of uses in a pedestrian friendly environment with an interconnected street network and multiple connections to open space systems that define edges of the district. Residential components are expected to reflect a range of densities with attached and detached housing styles encouraged through creative planning that minimizes streets with garages or parking areas. Non-residential components will be connected to, but appropriately transitioned from, single-family detached residential components and will not be located on residential level local streets, unless appropriate and as determined by the Director. Multi-family residential (attached residential units in excess of three stories or more in height) will be limited to certain percentages of some Planning Areas, as referenced on Sheet 4 of the PDD.

Rural Residential Planning Area

The Rural Residential Planning Area is a residential neighborhood set back from the edge of the mesas in the southwest portion of the community. Due to their abrupt rise in elevation and the expansive views to the west and northwest, these mesas are a visually dominant landscape feature and regional landform. Therefore, development of these areas is guided by restrictive standards intended to preserve visual character from I-25. Homes located in the Rural Residential Planning Area may range from single family homes on larger lots to homes arranged in a clustered pattern. Other techniques may be utilized, such as restricted building envelopes and fencing restrictions, to provide for continuous areas of natural open space, with the objective of preserving views and maintaining areas for wildlife habitat and movement.

Open Space Planning Area

Open space is an important framework element of the Ridgeway community to create a "city in a landscape". This open space system is concentrated to preserve sensitive riparian and floodplain habitats, create neighborhood edges and connect Planning Areas with trail systems. A majority of the system provides passive recreation opportunities incorporating regional trail connections and buffers with native landscape character. A Central Community Park of approximately 77 acres will be located in close proximity to the greatest concentration of housing and jobs and will provide opportunities for active recreation uses. Local neighborhood park sites identified in the Statement of Commitments will be located by a subsequent detailed Parks Open Space and Trails Master Plan and in the Subarea Plans.



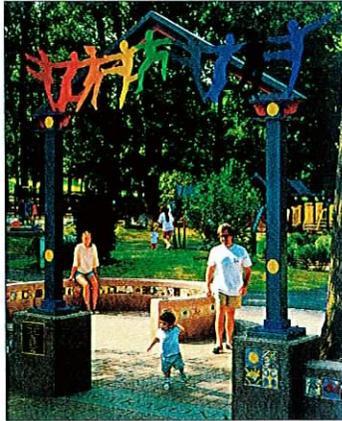
Open Space development character

The concept of equestrian uses as part of the on-site trail network through the Happy Canyon trail system will be explored and may be possible, provided careful consideration is given to public safety issues in light of the urban nature of the project to determine if these issues can be satisfactorily resolved and the owner is not liable or responsible for equestrian crossings at Lincoln Avenue to connect these trails to equestrian communities located to the northeast.



Open Space development character

The subsequent Master Plan for Parks, Trails and Open Space will include provisions for limited access to the bluffs. The provision will include details to carefully locate, construct and maintain trails with lookout/picnic area(s), to be designed in an environmentally and visually sensitive manner, with consideration given to connections with other trail networks and amenities in the plan, as well as a connection between the bluffs on the east and west sides of I-25. Said trails shall be accessible only by non-motorized means. Lookout areas may include limited provisions for emergency and maintenance vehicles, but no public vehicle access or parking would be allowed without the written consent of the Owner.



Open Space development character

+ 02/ Subarea Plan/Design Guideline Intent and Purpose

The long-term nature of the development implies that the provisions of the PDD may not address every subject. Accordingly, Subarea Plans will define a greater level of detail for transportation, land use and open space frameworks and development standards and guidelines. These Subarea Plans may address complete or partial planning areas, or a combination of multiple planning areas and will be prepared at or near the time of anticipated development. The Director shall have the authority to interpret future Subarea Plans and Design Guidelines, and to interpret certain City regulations and standards for consistency with the planning concepts and objectives of the PDD, the Annexation and Development Agreement, the Subarea Plans and the Design Guidelines, provided that such interpretations do not conflict with the City of Lone Tree's Comprehensive Plan, as reasonably determined by the Director in consultation with the City Council.

2.1 Subarea Districts-Phase One Development Area *Exhibit 2.0: Subsequent Subarea Plans*

The Ridgeway Phase 1 Development Area, is generally defined by Section 15 of the property and is bounded by Lincoln Avenue to the north, I-25 to the east and the western property boundary - the Phase 1 Development Area is anticipated to be composed of four Districts, each having a somewhat distinct character. Together, they form the basis for anticipated near-term (2000-2005) development of Ridgeway that balances employment, retail, living and civic use environments. The boundaries of these are outlined on Exhibit 2.0. Subsequent Subarea Plans may alter these boundaries or the related District concepts.

2.2 Office District Subarea - Concept

The Office District is composed of the portions of the property that are well-suited for large-scale single and multiple tenant employment/office users that desire visibility and access proximity to I-25. Because of this highly visible position, it is important that the character of such development, and its related streetscape, presents a positive, high-quality identity for Ridgeway and Lone Tree. The Subarea Plan will outline the framework criteria for infrastructure, site development and architectural character in this area so that users understand the minimum expectations and the quality and character standards of the overall development.

The Office District is further divided into two sub-districts identified on Exhibit 2.0. District O-1 is approximately 42 acres of the Subarea located east of Park Meadows Blvd., west of I-25 and south of the North Loop Road, which is anticipated for development as a major regional hospital facility and integrated medical office campus.



Office District development character

2.3 Commercial and Multi-Family District Subarea - Concept

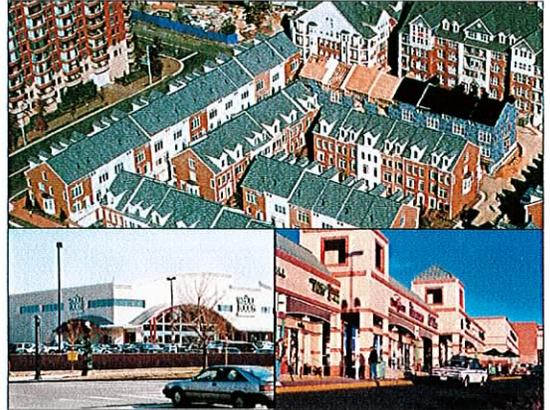
The Commercial and Multi-Family District is composed of the portion of the property located for logical automobile orientation along Lincoln Avenue and the western entrance of Ridgeway Parkway. These areas are well suited for automobile oriented mid-large scale commercial office and retail, as well as multi-family housing uses needed to offer choice and a strong economic basis for near-term development.

Site planning of these areas will endeavor to support a street orientation to reduce the visual impact of the automobile.

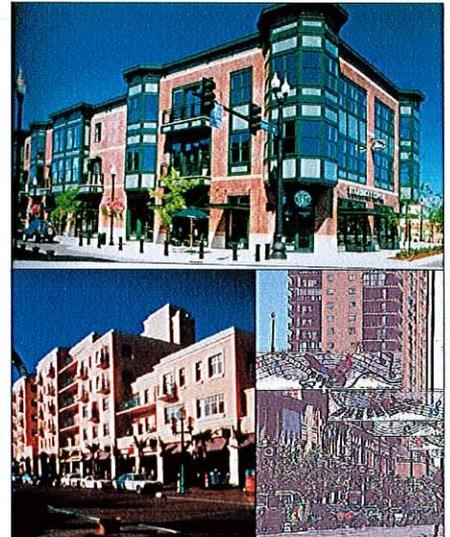
2.4 Main Street/Mixed-Use District Subarea - Concept

The Main Street/Mixed-use District is composed of property located in the center portion of the land west of I-25, which offers access to Lincoln Avenue and the other Subarea Districts. It provides an opportunity to combine residential uses with working, shopping, dining and services in a vertically and horizontally integrated walkable environment. Buildings in the District will be street-oriented with on-street parking to help provide an edge for pedestrians. Parking will be primarily accommodated in parking structures behind or below the buildings.

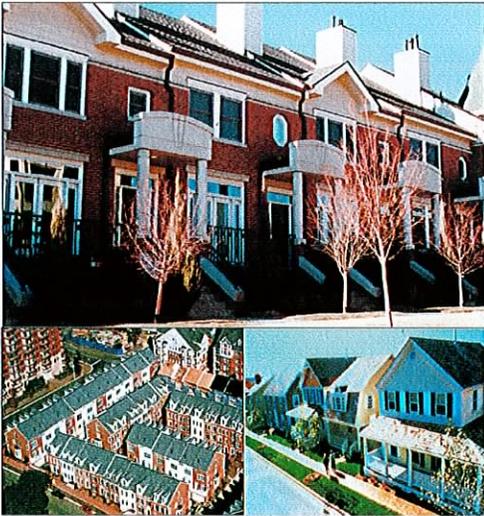
Main Street, between Ridgeway Parkway and the North Loop Road, will be organized with ground level retail/dining/entertainment uses and living and/or working environments above the ground level. Main Street is contemplated to have an intimate character servicing as an important public gathering space. This District includes the two civic uses anticipated for the project's west side, a regional recreation center and an elementary school.



Commercial and Multi-Family District development character



Main Street/Mixed-Use District development character



Residential District development character

2.5 Residential District Subarea - Concept

The Residential District is composed of the portion of the property with orientation and proximity to the project's Open Space Planning Area that is well-suited for housing and locates living environments within walking distance of a wide range of recreation, employment and shopping/dining opportunities.

It is anticipated that living environments in this District will offer a range of market-rate choices of attached and detached housing for rent and sale in a variety of configurations.

2.6 Subarea Plan/ Design Guideline Administration

All structures, whether new or to be renovated, shall be subject to the Subarea Standards and Guidelines contained herein and the separate prior approval of the Ridgeway Design Review Board ("DRB"). The Design Review Board will be composed of land owner, professional and Metropolitan District representatives and is authorized by the property's C C & R's. The City of Lone Tree shall not issue permits for use and construction until all applicable DRB requirements have been met.

Concept

The Subarea Standards and Guidelines contained herein address the quality of the environment, recognizing that it is ultimately formed by numerous individual, private, and public decisions. They are intended to promote a clear, consistent and predictable process for the development of land within Ridgeway. These Subarea Standards and Guidelines are distinct from the DRB's Design Standards and Guidelines.

Relationship between the Design Review Board and the City of Lone Tree

Design Review Board approval, must precede a submission to the City of Lone Tree Community Development Department for the applicable project review of subdivision, site improvement plan, etc., the process for which is separate and independent from the DRB's.

Site Development Resources and References

Ridgeway Design Review Board

Darryl Jones (DRB Coordinator)
Coventry Development Corporation
10475 Park Meadows Drive #600
Lone Tree, CO 80124
720.279.2581 (p)

City of Lone Tree

John Johnson
Community Development Department
9777 S. Yosemite Street, #100
Lone Tree, CO 80124
303.662.8112 (p)

John Cotton
Public Works Director
Teddy Lane
Lone Tree, CO 80124
303.662.8112

Pete Tyree
Building Official
9777 S. Yosemite Street, #100
Lone Tree, CO 80124
303.708.1818

Stephan Hassler
Chief of Police
9777 S. Yosemite Street, #100
Lone Tree, CO 80124
303.708.1818

Rampart Range Metropolitan District

Denise Denslow (District Manager)
6399 S. Fiddler's Green Circle #102
Greenwood Village, CO 80111
303.779.4525

South Metro Fire Rescue

Mike Dell'Orfano
9195 E. Mineral Avenue
Centennial, CO 80112
720.488.7200

Centennial Airport

Properties in the Office Campus District will be required to coordinate with Centennial Airport regarding their Aviation Easement and the filing of FAA Form 7460-1.

Scott Brownlee, Manager of Development

303/790-0598 (p)

303/790-2129 (f)

Public Service Co. (Xcel Energy)

Properties adjacent to or including the PSCO Gas Easement will be required to coordinate with PSCO (Excel Energy) and file an Encroachment Application as appropriate.

Tim Knowlton, Land Rights Agent

303/571-3402.(p)

Colorado Department of Public Health & Environment

Properties will be required to apply for a Colorado Discharge System Permit.

Nathan Moore or Kathy Dolan (Storm Water Permits)

303/692-3500 (p)

303/782-0309 (f)

Note:

The City's review process may involve consultation from additional referral agencies and Homeowner Associations.

+03/ Office District Subarea Plan

3.1 Office District Subarea Plan Boundaries

Exhibit 3.0: Office District Boundaries (ODB)

Exhibit 3.0 delineates conceptual boundaries for the Office District Subarea

3.2 Transportation Framework

Exhibit 3.1: Section 15 Transportation Framework Plan

Exhibit 3.2: Section 15 Public Transportation Framework Plan

Exhibit 3.3: Vehicular Access Points - Office District

Public streets in the Office District area will be designed to deliver higher levels of traffic and also serve as a framework for pedestrian, rail and bus transportation systems to offer options in reducing the reliance on the automobile.

Hierarchy of Street Design Features

Streetscape concepts have been developed to reinforce the hierarchy of streets within Ridgeway. The following streets envisioned to provide access to the Office District do not represent approved or final designs. Final design will be reviewed and approved by the City Departments of Engineering and Community Development, Southgate Water & Sewer District and South Metro Fire Protection District.

The following public streets will be built in the initial phase of development and will serve the Office District.

Park Meadows Blvd. (South of Lincoln Avenue to the East Roundabout)

Exhibit 3.4a: Street Section

Primary Role:

- The major connection between the future Ridgeway interchange and Lincoln Avenue and a major entry into the project

Primary Characteristics:

- Vehicular-oriented street with higher traffic volume on four lanes serving medium density land-use (office and hospital)

Functional Elements:

- R.O.W. Width: 150' (Allows for future expansion to six lanes if Park Meadows north of Lincoln Ave. can be expanded. If not accomplished within 10 years, excess ROW beyond 128' reverts back to landowner)

- Four lanes - 11' lane widths plus 2' gutter pans
- Asphalt paving
- No on-street parking
- Median - 20' width: needed to separate travel lanes, provide landscape amenity, and offer refuge for pedestrian crossing
- Landscaped tree lawn - 31' width: provides a buffer between pedestrians and traffic and includes the sidewalk - detached from the street, on both sides to promote pedestrian activity and connect with the overall pedestrian and trail network (also accommodates potential future addition of two travel lanes)
- ADT Range: 30,000-45,000
- Design Speed: 35mph
- Utility Easements: 10' width at outside edge of R.O.W.

Ridgegate Parkway - North Section (West Roundabout to Lincoln Avenue)

Exhibit 3.4b: Street Section

Primary Role:

- The secondary connection between the future Ridgegate interchange and Lincoln Avenue and a major entry into the project

Primary Characteristics:

- Vehicular-oriented street with high traffic volume (four lanes) and low-medium density land use (office, retail, multi-family housing)

Functional Elements:

- R.O.W. Width: 128'
- Four Lanes - 11' lane widths plus 2' gutter pans
- Asphalt paving
- No on-street parking
- Median - 20' width: needed to separate travel lanes, provide landscape amenity, and offer refuge for pedestrian crossing
- Landscaped tree lawn - 20' width: provides a buffer between pedestrians and traffic and includes the sidewalk - detached from the street, on both sides to promote pedestrian activity between commercial and multi-family land-uses and connect with the overall pedestrian and trail network
- ADT Range: 28,000-35,000
- Design Speed: 35mph
- Utility Easements: 10' width at outside edge of R.O.W.

Ridgegate Parkway - Center Section (between Rotaries)

Exhibit 3.4d: Street Sections

Primary Role:

- The secondary connection between the future Ridgegate interchange and Lincoln Avenue

Primary Characteristics:

Pedestrian-oriented street with moderate traffic volume (four lanes) and medium-high density mixed-use development

Functional Elements:

- R.O.W. Width: 95'
- Four lanes: 12' center/11' outside lane widths plus 2' gutter pans
- Asphalt paving
- On-street parallel parking near the future "Main Street"
- No median
- Landscaped/hardscaped tree lawn -14'-16'- foot width includes the sidewalk - attached to the street near the future "Main Street" to create an active pedestrian zone tied to ground level retail activity and on-street parking
- ADT Range: 20,000-24,000
- Design speed: 30mph
- Utility easements: 10' width at outside edge of R.O.W.

Ridgegate Parkway - East Section (East Rotary to I-25)

Exhibit 3.4e: Street Section

Primary Role:

- The primary connection to the new I-25 interchange and a major entry into the project

Primary Characteristics:

- Vehicular-oriented street with very high traffic volume (six lanes) and low-medium density land use (office, retail)

Functional Elements:

- R.O.W. width: 150'
- Six lanes - 11' lane widths plus 2' gutter pans
- Asphalt paving
- No on-street parking
- Median - 23' width: needed to separate travel lanes, provide landscape amenity and offer refuge for pedestrian crossing
- Landscaped tree lawn - 20' width provides a buffer between pedestrians and traffic and includes the sidewalk - detached from the street, on both sides to promote pedestrian activity

- ADT range: 35,000-50,000
- Design speed: 40mph
- Utility easements: 10' width at outside edge of R.O.W

Roundabout (East and West)

A roundabout is a circular intersection where the traffic entering yields the right-of-way to the circulating traffic. Roundabouts in similar applications have proven to:

- Reduce fatal and injuring accidents by as much as 75% due to slower speeds and a reduced number of conflict points.
- Eliminate costs associated with traffic signals
- Reduce delay in movements through intersections as compared to traditional signals.
- Increase traffic capacity and level of service by up to 30% as compared to traditional signals.
- Reduce environmental impacts from fuel consumption and idling vehicle emissions.
- Enhanced opportunities to create community identity elements in the island at the middle of the rotary.

Functional Elements:

- Outside Diameter: 160-200ft.
- Concrete paving
- ADT range: 30,000-40,000
- Design speed: 20-25mph

New I-25 Interchange

It is anticipated that a new interchange approximately one mile south of the Lincoln Avenue interchange will be built at some time in the future. A 1601 Process with the Colorado Department of Transportation (CDOT) has been initiated and is expected to be complete by mid-2001.

Primary Role:

Access to I-25 to relieve regional congestion on the Lincoln Avenue interchange. An important connection between the eastern and western portions of the Ridgeway/Lone Tree community.

Vehicular Access Points

Exhibit 3.3: Vehicular Access Points - Office District

Vehicular access points from the major roadways have been defined per Exhibit 3.3 and are intended to reinforce a clear hierarchy and organization of vehicular circulation within Ridgeway. Individual developments must accommodate this plan, unless an equivalent or superior solution is provided.

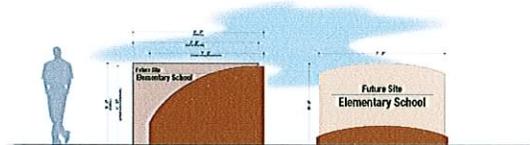


Entry and Project Identity Monumentation/Signage

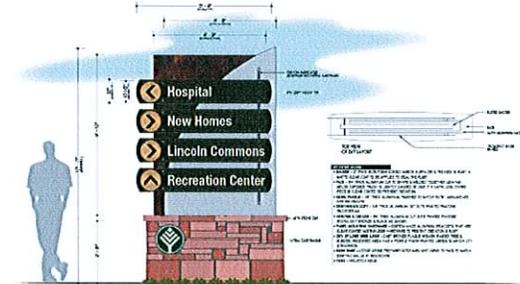
Exhibit 3.5: Project Identity Monumentation/Signage

Entry and project identification signage will be limited in scale and understated, but high quality, in approach. Exhibit 3.5 identifies these locations. A consistent standard for project identification signage will be developed and submitted to the City for approval with street landscaping plans before any installation occurs.

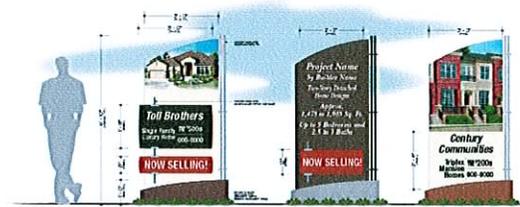
The Wayfinding Signage Program in RidgeGate is a comprehensive signage program that directs and informs people with a hierarchy of sign types. The approved comprehensive Wayfinding Signage Program for the west side of the RidgeGate PD is intended to direct and inform the public with regard to neighborhood destinations with a hierarchy of sign types. This Program involves the following types of signs:



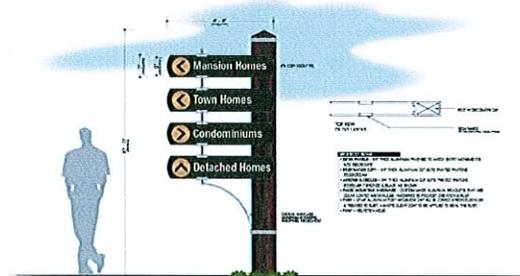
Future amenity signs (temporary)



Option A directional kiosk sign (permanent)



Builder identification signs (temporary)



Street kiosk sign (temporary & moveable)

Entry monument signs

Existing along Lincoln at Park Meadows Blvd., and at RidgeGate Parkway; future signs at new interchange; would be subject to landscaping enhancements per condition of approval.

Directional Kiosks (Large)

Permanent signs to direct people to destination areas within RidgeGate.

Street Kiosks (Small)

Temporary signs that may be relocated; provide more detailed directional information

Future Amenity Sign

Temporary signs to inform people about upcoming amenities and future development.

Builder Identification Sign

Temporary signs proposed for each home builder's sales area.

Pedestrian Routes and Access Points

Exhibit 3.6: Section 15 Pedestrian and Bicycle Access

Pedestrian routes and access points have been defined per Exhibit 3.6 and are intended to reinforce a clear hierarchy and organization of pedestrian circulation within Ridgegate. Individual project development must accommodate this plan, unless an equivalent or superior solution is provided.

Bicycle Routes

Exhibit 3.6: Section 15 Pedestrian and Bicycle Access

Bicycle routes have been defined per Exhibit 3.6 and are intended to reinforce a clear hierarchy and organization of bicycles, and other non-motorized wheeled vehicle circulation within Ridgeway.

Open Space Trail Connections

Exhibit 3.7: Section 15 Open Space Framework Plan

Open Space trail connections to individual development sites have been defined per Exhibit 3.7 and are intended to reinforce a clear hierarchy and organization of multi-purpose circulation within Ridgeway. Individual project developments must accommodate this plan, unless an equivalent or superior solution is provided.

Public Transportation Routes and Stops

Exhibit 3.2: Section 15 Public Transportation Framework Plan

Light Rail

An extension of the Southeast Corridor light rail by the Regional Transportation District (RTD) is anticipated to be located within the Office District Subarea, as defined on Exhibit 3.2, including a basic rail stop (without public parking) near the intersection of Park Meadows Blvd. and the North Loop Road. Development adjacent to the rail line and stop would be encouraged to maximize the benefit offered by this transportation system.

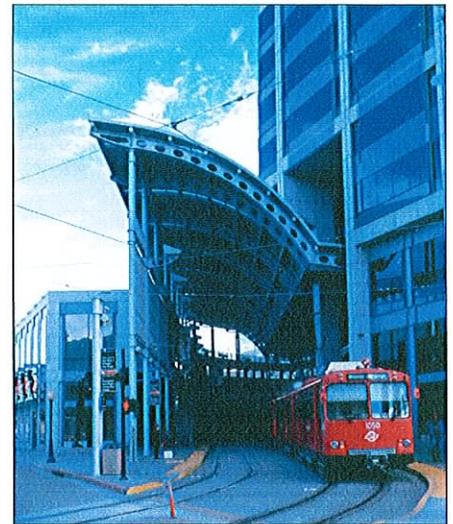
Bus/Rubber-Tire Shuttle

A bus/rubber-tire shuttle system is anticipated to expand and extend alternative transportation choices within Ridgeway. Exhibit 3.2 defines these anticipated routes and stops, to be developed utilizing pull-off lanes and waiting shelters.

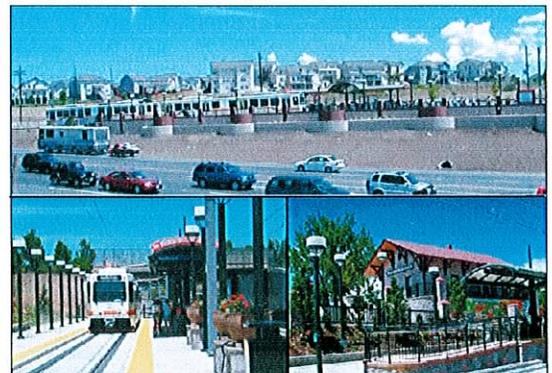
Streetscape Elements

Street furniture, lights and trees are intended to unify the public right-of-way. Street trees are intended to provide shade and comfort for pedestrians, as well as establish a buffer from automobile traffic. They help spatially define and unify the overall streetscape theme.

The following design standards for streetscape elements apply to all public rights-of-way within Ridgeway. Street landscaping plans will be submitted to the City for approval before any installation occurs.



Transit/Development integration example



Light Rail stop example



Bus Stop example



Representative streetscape example



Representative streetscape example

Design Standards

- Street furnishings, lights and trees shall not block a 6' wide (minimum) clear pedestrian walking zone or make maintenance of a clear walking surface (such as snow removal) difficult.
- Street lights and trees shall be placed in an orderly fashion to define the edge of the pedestrian activity zone.

Design Guidelines

- There should be a clear pedestrian walking zone of adequate width on both sides of the street to serve projected pedestrian needs.
- Street furnishings and trees should occupy consistent, well defined zones parallel to the pedestrian walking zone.

Street Trees

Design Standards

- All trees shall be a minimum of three-inch (3") caliper at the time of installation. A consistent standard for street tree species shall be developed and submitted to the City for approval with street landscaping plans before any installation occurs.
- All tree planting or removal within the public rights-of-way shall be approved by the City of Lone Tree.
- Existing healthy trees and their root systems shall be protected during construction through the use of barricades and fencing.
- Plant materials, fencing, or landscaping improvements greater than six inches (6") in height shall not extend into the sight triangle for any intersection of a street or driveway, as defined by the City of Lone Tree.

Lighting

Design Standards

- Lighting for pedestrians and vehicles shall be provided within all public rights-of-way. A consistent standard for lighting shall be developed and submitted to the City for approval with street landscaping plans before any installation occurs.

Seating

Design Standards

- Benches for pedestrians shall be provided within public rights-of-way. A consistent standard for benches shall be developed and submitted to the City for approval with street landscaping plans before any installation occurs.
- Seating shall be placed to serve bus and transit stops, building entry areas and plazas.

- Sidewalk benches shall be oriented parallel to the curb line when utilized at the face of a building so as not to impede pedestrian movement.

Design Guidelines

- Benches should be of quality materials and have backs.
- Movable chairs and seating for sidewalk cafes are encouraged in public open space and sidewalk areas, so long as they are stored after hours or in inclement weather.

Sidewalk Paving

Design Standards

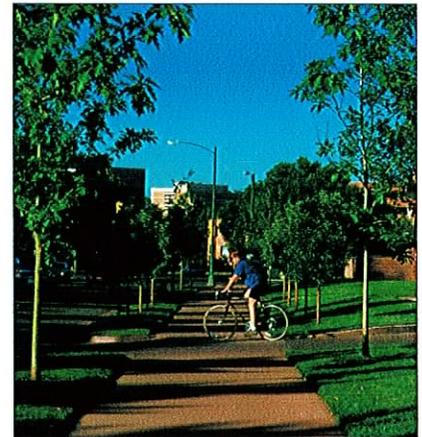
- Paving materials shall be of quality and durable materials such as, but not limited to, concrete, stone, concrete unit pavers or brick.
- All sidewalk paving shall meet City and ADA standards.

Design Guidelines

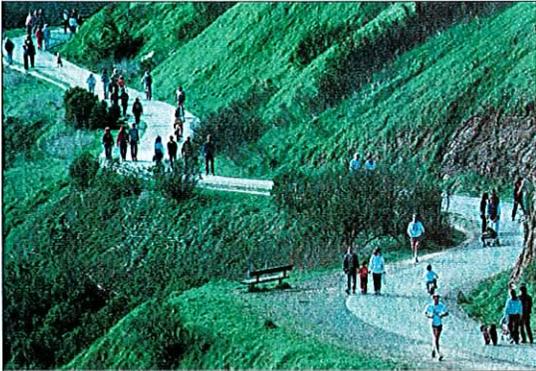
- Special paving materials should be used to highlight the pedestrian areas associated with street crossings, and special use areas such as plazas, building entries or café seating areas.



Representative streetscape example



Representative streetscape example



Open Space character example



Open Space character example



Open Space character example

3.3 Open Space Framework

Exhibit 3.7: Section 15 - Open Space Framework Plan

The Open Space Framework defined on Exhibit 3.7 is intended to provide a landscape context and contrast for the concentrated, walkable development pattern envisioned. This "city-in-a-landscape" provides an environment with great amenities and a chance to connect people with nature.

The four principal components of the framework are the two drainage corridors (Willow Creek to the west and Cottonwood Creek to the east), the underground east-west gas easement (expanded to a 100' width) connecting these two corridors, and the large bluff's open space. This framework organizes landscape character to transition from native and rustic at higher elevations to more manmade and programmed for active use at lower elevations closer to development concentrations, along the two drainage corridors. A regional recreation center to be constructed by South Suburban Recreation District, is expected to be built along the underground gas easement. The landscape character of the east-west gas easement is expected to be more structured and manmade in constitution. Improvements, other than buildings, are acceptable within this easement, so long as they do not preclude the maintenance and operation of the gas line.

The two drainage corridors will, in addition to allowing wildlife habitat and pedestrian use areas, accommodate stormwater management for the project. In general, a series of detention basins and grass-lined and/or planted conveyance channels will serve to filter water and enhance habitat values. Trails within this open space framework will serve to connect destinations, link to adjacent development sites and offer an additional non-motorized transportation choice. Where trails cross major vehicular routes, grade-separated crossing opportunities may be considered.

Ultimately, this framework will be developed into a Master Facilities and Management Plan for Parks, Trails and Open Space, which will be submitted concurrent with residential development, per the PDD.

3.4 Utilities Framework

Exhibit 3.8: Utility Framework Plan

Water

Water service will be provided by the Southgate Water District. Two pressure zones exist that will serve the site, Zone 9E and Zone 10E.

Water main extensions within proposed roadway corridors will be required to provide site accessible water and fire protection. Zone 10E main line extensions and looping will extend from an existing water main within Yosemite Street via Ridgeway Boulevard and South Loop Road. Zone 9E main line extensions will extend from an existing water main located within Lincoln Avenue via proposed Park Meadows Drive, Main Street, Ridgeway Boulevard, and Willow Creek Lane roadway corridors.

Sanitary Sewer

Sanitary sewer service will be provided by the Southgate Sanitation District in conjunction with the City of Englewood. Southgate Sanitation District contracts wastewater treatment with the City of Englewood. Southgate sewer main facilities will require extension into Ridgeway via the Willow Creek. The point of main line connection is located on the north side of Lincoln Avenue at the proposed Willow Creek Lane. Main line extension required to serve this office district will extend south along the proposed Willow Creek Lane and east within the proposed North Loop Road and Ridgeway Parkway. Due to topographic relief, sanitary sewer laterals will be required to serve each respective site.

Dry Utility Corridors/Easements (Electrical Power, Natural Gas, Telephone, Cable and Fiber Optic)

Dry utilities will generally follow proposed roadway alignments within utility easements on either side of the public roadways. Location and width of utility easements shall be determined at time of platting and dedicated as part of the final plat. Separate utility easement agreements may be required for unplatted parcels to allow extension of dry utilities to proposed platted sites.

3.5 Grading/Stormwater Management Framework

Exhibit 3.9: Stormwater Management Concept Plan

Regional Stormwater Management Strategy

The objectives of the Stormwater Management Plan includes the application of the "Best Management Practices" (BMP's) as defined in the Urban Drainage and Flood Control District "Drainage Criteria Manual, Volume 3." The purpose of the BMP's is to provide for a more integrated, sustainable, and localized system for stormwater management and flood control than conventional requirements. Key elements of the BMP's include the maximum use of grass lined and/or planted swales to encourage infiltration of stormwater into the soil; the use of water quality enhancement basins and facilities to minimize the concentration of sediments and debris; and an emphasis on dispersal of detention areas to minimize the rapid

concentration of area-wide flooding. When applied to Ridgeway, the BMP's will provide water quality management, minimize flood risk, and allow for re-establishment of natural riparian and wetland habitats for wildlife and recreation.

Parcel Requirements

- **Location of Detention or Retention**

Stormwater detention or retention located between buildings and the street is discouraged when other alternatives are possible.

The use of multiple detention areas is encouraged. Large and/or deep detention areas that dominate the landscape are discouraged.

- **Landscaping**

Each individual detention or retention area that is located on landscaped areas shall be improved and maintained with a landscape that includes ground covers or turf, shrubs, and trees compatible with the overall landscape design of the Lot. Low-flow channels and drainage control structures shall be designed for attractive appearance, including landscaping, grading and location to minimize their visual impact. Low water areas that may remain moist for extended periods shall be designed with surrounding mow-strips of concrete, planted, and maintained in a condition with a minimum of muds, silts, or standing water.

- **Basin Design**

The shape, grading, side-slopes, and location of detention or retention areas within the landscape shall be designed to integrate into the overall landscape design of the Lot of the proposed development. Curvilinear shapes with variation are preferred over rigid, engineered shapes.

Overlot Grading and Pre-Development Landscape Treatment

It is anticipated that community infrastructure development and parcel development will require overlot mass grading. Overlot grading will be required to transition to existing grades where parcels are adjacent to preserved open space. Any overlot grading anticipated to occur in advance of individual site development construction for more than nine (9) months will be required to apply native grass seeding to minimize erosion and enhance aesthetic appearance.

3.6 Public Land Dedication(s)

None anticipated for this Subarea other than road ROW's

3.7 Land Uses

Exhibit 3.10: Permitted Land Uses

Exhibit 3.10 defines land uses which are permitted by right, allowed with Special Review and disallowed in each of the Planning Areas appropriate for this Subarea, per the PDD. To the extent that this component of the PDD is amended, then this exhibit shall be deemed amended in the same manner.

3.8 Building Heights

Exhibit 3.11: Permitted Building Heights

Exhibit 3.11 defines maximum building heights permitted in this Subarea, per the PDD. To the extent that this component of the PDD is amended, then this exhibit shall be deemed amended in the same manner.

3.9 Floor Area and Density

Exhibit 3.12: Allowable Floor Area and Density Ratios

Exhibit 3.12 defines minimum and maximum development yields allowable in this Subarea, per the PDD. To the extent that this component of the PDD is amended, then this exhibit shall be deemed amended in the same manner.

+ 04/ Office District Subarea Standards and Guidelines

These Subarea Standards and Guidelines, as authorized by the PDD approved by the City in June, 2000, define a level of quality and provide for consistency among individual lots and public areas within the Office District Subarea and are supplementary to the City's Design Guidelines.

Design Review Board approval is mandatory for all projects in the Ridgeway Office District prior to submission for the City of Lone Tree project review.

Definition of Terms

Design review evaluation criteria are organized by three headings (Concept, Standards, Guidelines) for each category.

Concept:

Concept statements define goals for which the standards and guidelines are created to achieve.

Standards:

Standards are objective criteria that provide specific direction based on the stated concept statement and are used to denote issues that are considered critical to achieving the concept. Standards use the term "shall" to indicate that compliance is required unless it can be demonstrated that an acceptable alternative meets one or more of the following conditions:

- The alternative equally or better achieves the stated intent;
- The intent, which the standard was created to address, will not be achieved by application of the standard in this particular circumstance.
- The application of other standards and guidelines to achieve stated intents will be improved by not applying this standard.
- Unique site factors make the standard impractical or cost prohibitive.

Guidelines:

Guidelines provide further considerations to promote the goals defined by the concept statement. Guidelines use the term "should" or "may" to denote they are considered relevant to achieving the stated concept, and will be pertinent to the review process but will not be required for approval. Guidelines will, however, be strongly considered in the event of a waiver to a related standard.

4.1 Site Planning

4.1.1 Access and Circulation

Concept:

- Minimize vehicular access (curb cuts) on primary roadways to reinforce a clear hierarchy and organization of circulation within Ridgeway
- Maximize uninterrupted public sidewalks

4.1.1.1 Standards

- Development shall limit curb cuts to those areas where curb cuts are allowed as illustrated in Exhibit 3.1: Transportation Framework.
- Reasonable pedestrian connections between buildings and transit stops.
- Internal vehicular and pedestrian circulation within a development of multiple buildings or lots must interconnect in an obvious and consistent manner.
- Each project must connect to systems (walks, trails, open space) from adjacent parcels.

4.1.2 Building Locations Concept:

Exhibit 4.0: Build-To/Setbacks Diagram

Concept:

- Define and contain the street space. Concentrate and reinforce pedestrian activity.
- Create a sense of the street as a “place”.

4.1.2.1 Standards

- The location of setbacks that affect development within Ridgeway are illustrated in Exhibit 4.0: Build-To-Setbacks Diagram.
- Where so designated by a build-to line, the front building facade shall be located on or near the designated property line as explained below. The frontage shall be measured along the designated property line, and for corner lots shall mean the length of the combined frontage of front property lines.
 - (i) A portion of the building facade shall be located on the designated build-to line for at least thirty-three percent (33%) of the property frontage; and
 - (ii) An additional portion of the building facade shall be located on or within eight feet (8') of the designated build-to line for a distance of at least thirty-three percent (33%) of the property frontage.



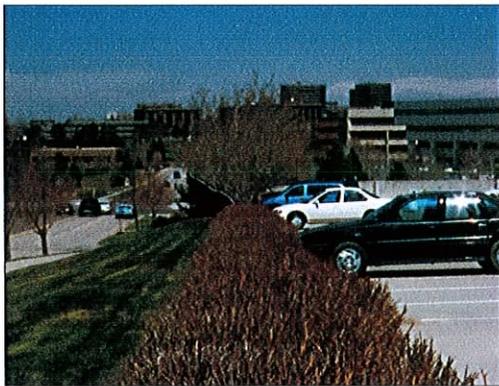
Setback building example



Build-to line/zone building example



Parking garage character example



Parking lot example

Note: A building facade that is substantially built to the required line or zone, allowing for typical architectural variation in the wall plane, shall be considered to meet the build-to requirement.

- Where so designated by a build-to zone, the building façade shall be located anywhere within the zone. Parking is allowed within the setback area but shall be located a minimum of 20' from the property line.
- Where so designated by a setback, the building façade shall be located no closer to the property line than the setback allows.

4.1.3 Grading

Concept:

- To the extent practical and economically feasible, respect the natural variation in topography.
- Minimize the impact of walls to any public streets.

4.1.3.1 Standards

- 3:1 Maximum slope - retaining walls, terraced parking lots and stepped building pads shall be considered to avoid slopes steeper than 3:1

4.1.4 Parking

Concept:

- Minimize the visual impact of the automobile on public spaces within Ridgeway.

4.1.4.1 Standards

- Parking structures facing pedestrian-oriented streets must incorporate elements at all levels to conceal views of parked cars and are encouraged to incorporate active uses at street level edges.
- Parking structures shall conform to building Standards and Guidelines pertaining to massing, scale and materials.
- Parking edges must be defined and enhanced with low walls or berming and landscaping.
- Bicycle parking to be provided within buildings or parking areas as appropriate to the use.
- Pavement grades shall not exceed 5% longitudinal slope within parking areas and an 8% longitudinal slope in drive aisles that do not have parking stalls along the aisles.

4.1.4.2 Guidelines

- “Bioswale” landscaped parking islands to filter and reduce stormwater runoff in parking areas are encouraged.

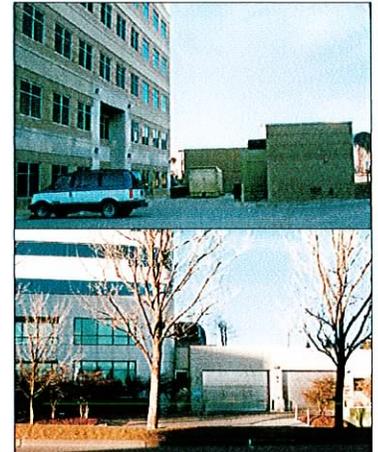
4.1.5 Service Areas

Concept:

- Reduce the visual and functional impact of service areas on public spaces within Ridgeway.

4.1.5.1 Standards

- Service and delivery activities shall be separated from the primary public access and screened from public view by means such as:
 - locating underground
 - locating internal to structures
 - providing masonry walls, fences and/or landscaping of sufficient height and density.
- Such areas shall not be visible from streets, other public areas, or adjacent properties. Roofs shall be required if residential uses look down on the area.
- Adequate loading and maneuvering space shall be provided for trucks and other service vehicles.



Service area/screening examples

4.2 Architecture

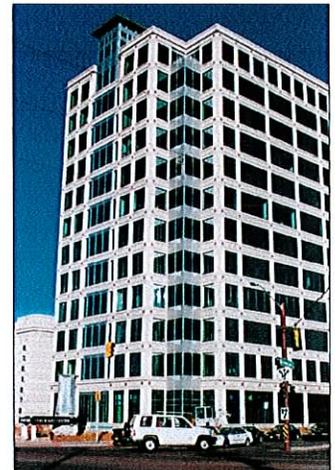
4.2.1 Massing and Scale

Concept:

- Provide an organized variety of building massing, architectural form and detail with human-scaled building components at ground level.

4.2.1.1 Standards

- Each building facade oriented to a public street or open space shall incorporate architectural scaling patterns that include three or more of the following elements:
 - Expressions of building structural elements such as floors (banding, belt courses, etc. not less than 1' deep and 4" wide), columns (pilasters, piers, quoins, etc. not less than 4" deep and 1' wide), and foundations (waterables, rustication);
 - Patterns of window and door openings that are emphasized through change of plane (not less than 4" deep), and/or the use of sills, lintels, mullions, muntins, and other scale providing elements;



Building massing/scale example



Building massing/scale example



Building massing/scale example

- Changes in material;
- Changes in color;
- Changes in texture;
- Changes in material module or pattern;
- Patterns of architectural ornament integral to the building materials.
- Each change of material shall involve at least 1" variation in wall plane. Reveals shall be not less than 1" deep and 1" wide.
- The Ridgeway PDD identifies view corridors and heights limitations that must be preserved and respected, as appropriate.

4.2.1.2 Guidelines

- Visible joint patterns and projected sills are encouraged.



Building fenestration example

4.2.2 Four Sided Fenestration

Concept:

- Provide a high degree of transparency at the lower levels of building façades.
- Insure the visibility of active pedestrian uses.
- Establish a pattern of individual windows at upper floors that provides a greater variety of scale through fenestration patterns, material variation, detail, and surface relief.

4.2.2.1 Standards

- No highly reflective glazing shall be permitted on the building facade (maximum reflectance factor of .20).
- Primary entrances into buildings shall be easily identified through design elements and detailing.

4.2.3 Roofs

Concept:

- Buildings must integrate all building systems within a complete architectural form.
- Respect the character of and views from the surrounding context.

4.2.3.1 Standards

- All rooftop mechanical systems shall be incorporated into the building form in a manner integral to the building architecture in terms of form and material. All mechanical, electrical and telecommunications systems shall be screened from view of surrounding streets and structures.



Building entry example

- Parapet roofs must be articulated to avoid a continuous warehouse-like form.

4.2.3.2 Guidelines

- Three-dimensional rooftops are encouraged.
- Darker colors are encouraged for flat roofs.

4.2.4 Materials and Color

Concept:

- Encourage human scaled buildings through the use of smaller material modules.
- Consistent use of quality materials appropriate to the urban environment.

4.2.4.1 Standards

- All façades facing public streets or open space shall be constructed on high quality materials including , without limitation,the following.
 - Masonry, including stone, brick, terra cotta, architectural precast concrete, cast stone and pre-fabricated brick panels.
 - Architectural metals, including metal panel systems, metal sheets with expressed seams, metal framing systems, or cut, stamped or cast, ornamental metal panels.
 - Glass and glass block.
- Glazing systems shall typically utilize framing and mullion systems that stand out from the surface of the glass to provide scale and surface relief.
- In general, subdued colors typical of the muted native grasses, rocks and soil of the high Colorado plains and the natural setting are to be used as the primary colors.
- Intense colors are limited to accents only.
- The following materials are disallowed unless approved by variance:
 - Reflective glass curtain wall systems
 - Unfinished CMU Block

4.2.4.2 Guidelines

- Building materials should be selected with the objectives of quality and durability.



Roof treatment examples

4.2.5 Signage

Concept:

- Create an organized and interrelated system of quality signs, sign structures, lighting and graphics.
- Create signs and graphic elements that are related to and/or respect the architecture of the building which they serve.
- Encourage a variety of signs and graphic elements that are compatible with the character of the surrounding district.

4.2.5.1 Standards

- Each building project must prepare a comprehensive signage plan defining locations, sizes, and types of information, identity and directional signage.
- Building mounted signs must be limited to the individual letters and logo of a single company name and shall be internally lighted. Rooftop signs are prohibited.
- Individual development project monument signs must not exceed 30 square feet in area.
- Pole mounted signs are prohibited.
- Strobe, flashing, blinking and portable signs are not allowed.
- Use of neon lighted signs is allowable in certain, limited applications.
- Maximum height of any letter or symbol shall not exceed four feet (4').
- Individual development project identification signage must recognize the overall project name (Ridgegate).

4.2.5.2 Guidelines

- Signs should fit within the architectural features of the façade and compliment the building's architecture. Signs should not overlap and conceal architectural elements.

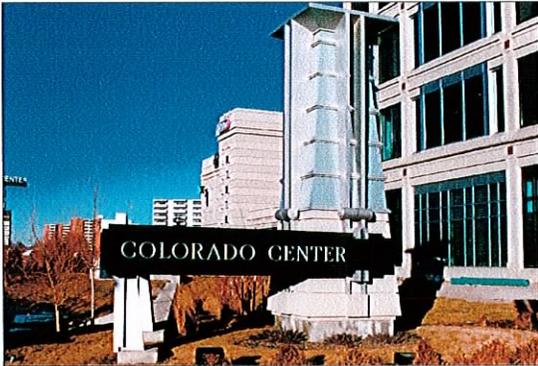
4.2.6 Lighting

Concept:

- Provide a functional attractive unified lighting system throughout the lot that contributes to a high quality design and a feeling of security for users.

4.2.6.1 Standards

- Each building project must prepare a comprehensive lighting plan defining levels, locations and types of illumination proposed for the following:



Signage example



Signage example

- Road and Parking areas
- Pedestrian areas
- Special Features/Accents
- Lighting shall not exceed eight (8') foot-candles at any locations, when measured on the ground, with certain, limited, exceptions such as under service station canopies or at building entrances.
- The maximum heights of light fixtures shall be thirty feet (30') above the ground.
- Metal halide or other "white" lighting sources are required.
- Fixtures must complement those selected for the overall project streetscapes.



Landscaping example



Landscaping example



Landscaping examples

4.3 Landscape

4.3.1 Landscape Character Zones

Concept:

- Establish distinct character and use zones that create identity, add value and limit high water use plant materials to high activity areas.

4.3.1.1 Standards

- A majority of the plant materials shall be of an indigenous or low water use plant palette.
- Small scale, high activity human use areas may utilize more water intensive plant palettes
- Minimum plant sizes are as follows:
 - Trees: Minimum 3" caliper (evergreens: 9' height - limited to 25% of total number of trees).
 - Shrubs: Minimum 1 gallon - limited to 25% of total number of shrubs.

4.3.1.2 Guidelines

- The DRB will adopt recommended plant lists.

4.3.2 Irrigation

Concept:

- Support the conservation of water resources

4.3.2.1 Design Standards

- A majority of landscaped areas must utilize drip or other low-volume application systems controlled by automatic computerized clocks.

4.3.3 Fences and Walls

Concept:

- Minimize fences and walls in front and side yard areas or where visible from public streets and open spaces.

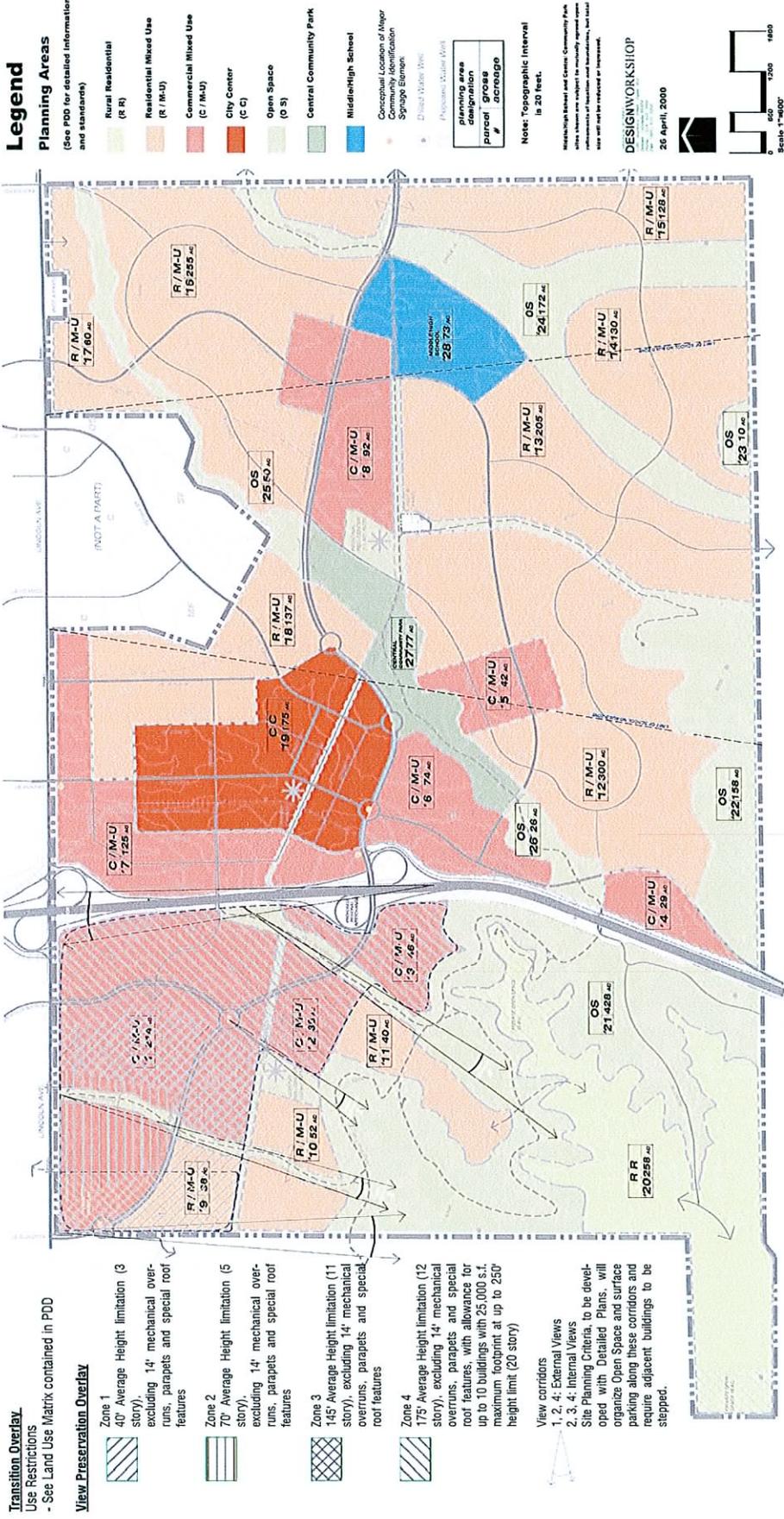
4.3.3.1 Design Standards

- No free-standing walls or fences in front yards over three feet (3') in height are allowed.
- Wall materials must directly relate to building materials.
- No permanent chain link fences are allowed.

RAMPART RANGE

Sections 13, 14, 15, 22, 23, 24 and a portion of Section 21
Township 8 South, Range 67 West of the 6th Prime Meridian,
City of Lone Tree, State of Colorado (3,514 Acres)

Planned Development District



Legend

Planning Areas
(See PDD for detailed information and standards)

- Rural Residential (R R)
- Residential Mixed Use (R/M-U)
- Commercial Mixed Use (C/M-U)
- City Center (C C)
- Open Space (O S)
- Central Community Park
- Middle-High School
- Conceptual Location of Major Sprague Element
- Direct Water Way
- Proposed Water Way

Planning area designation parcel acreage

Note: Topographic Interval is 20 feet.

Middle-High School and Central Community Park sites shown are subject to mutually agreed upon determinations of location and construction, but shall not be used for the purpose of development.

DESIGNWORKSHOP

26 April 2000



NOTE: MAJOR CIRCULATION ELEMENTS SHOWN ARE CONCEPTUAL IN NATURE AND SUBJECT TO MODIFICATION AND REFINEMENT.

***To the extent that the PDD is amended, then this exhibit shall be deemed automatically amended in the same manner.

Transition Overlay Use Restrictions

- See Land Use Matrix contained in PDD

View Preservation Overlay

- Zone 1
40' Average Height limitation (3 story), excluding 14' mechanical overruns, parapets and special roof features
- Zone 2
70' Average Height limitation (5 story), excluding 14' mechanical overruns, parapets and special roof features
- Zone 3
145' Average Height limitation (11 story), excluding 14' mechanical overruns, parapets and special roof features
- Zone 4
175' Average Height limitation (12 story), excluding 14' mechanical overruns, parapets and special roof features, with allowance for up to 10 buildings with 25,000 s.f. maximum footprint at up to 250' height limit (20 story)

View corridors

- 1, 2, 4: External Views
 - 2, 3, 4: Internal Views
- Site Planning Criteria, to be developed with Detailed Plans, will organize Open Space and surface parking along these corridors and require adjacent buildings to be stepped.

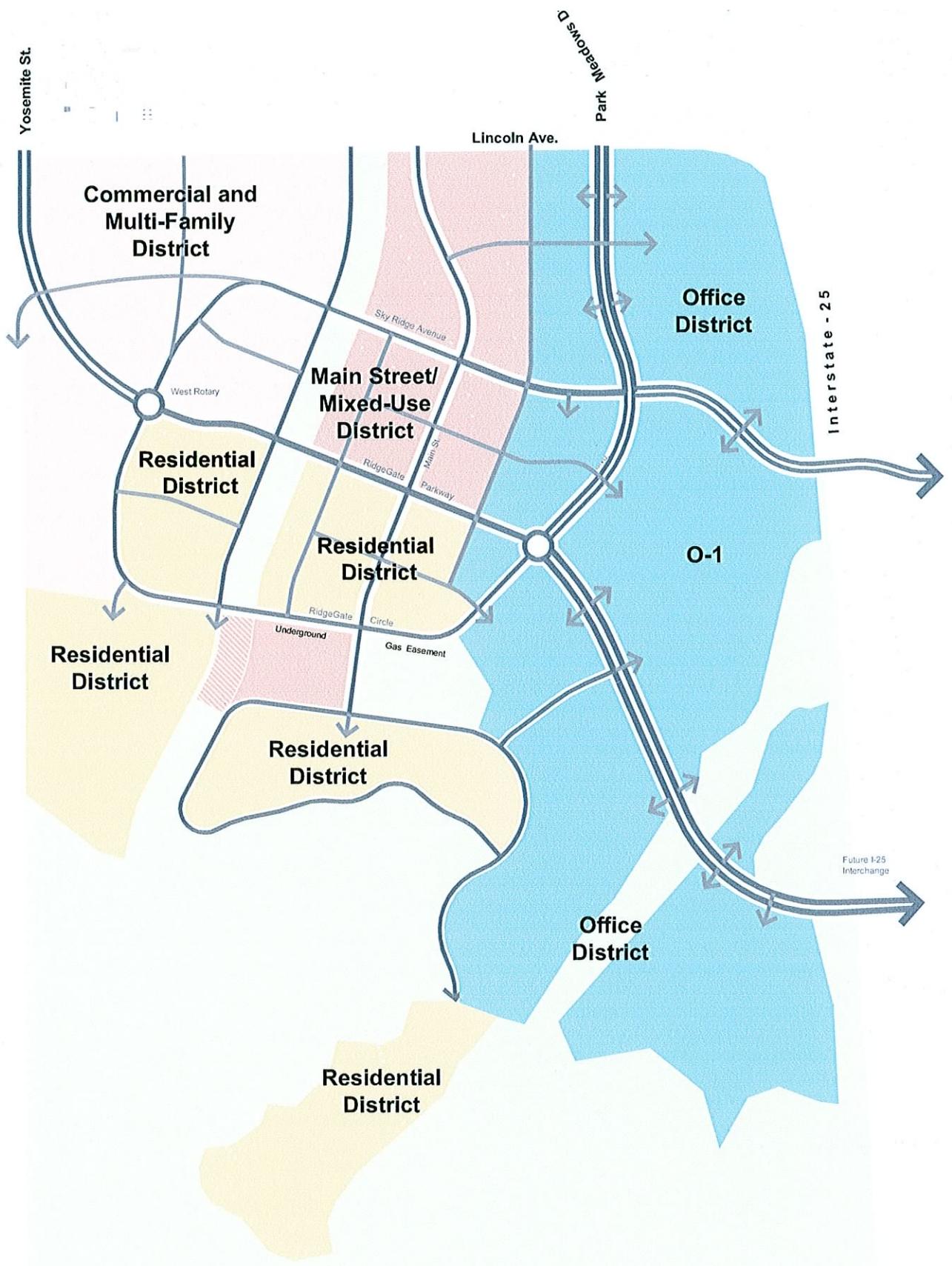


Exhibit 2.0: *Section 15 Subarea Districts*

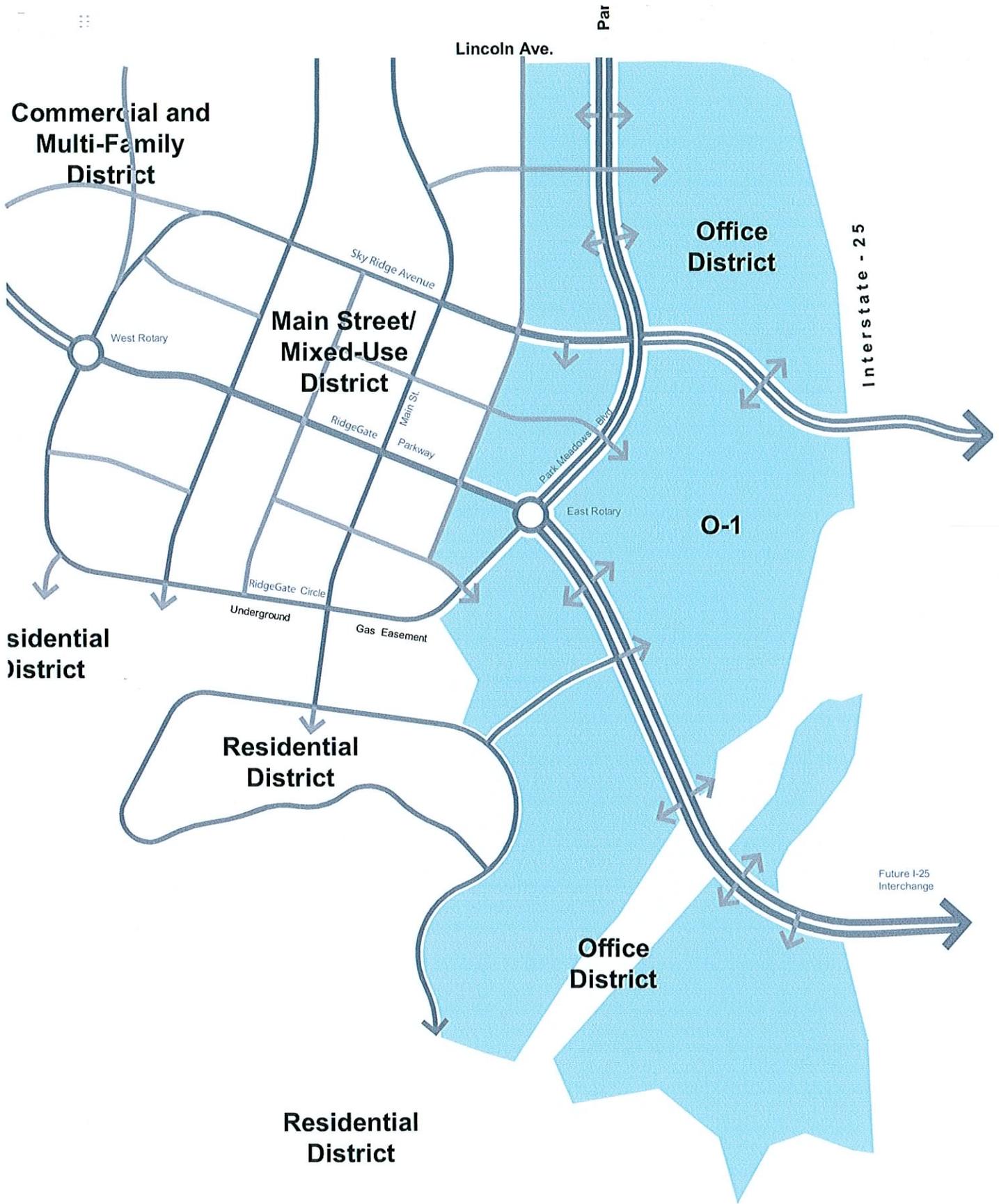


Exhibit 3.0: Office District Subarea Plan Boundary

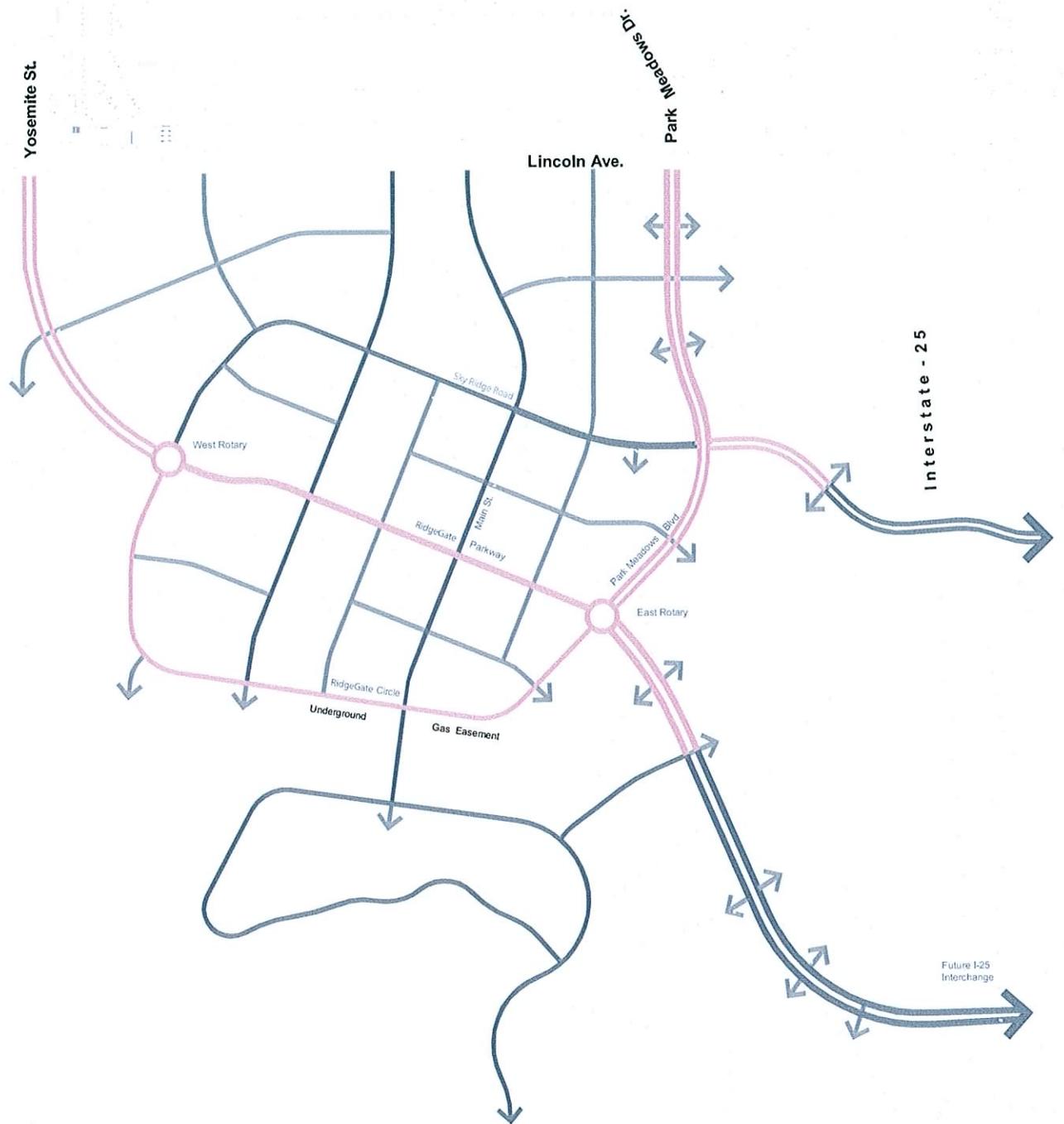


Exhibit 3.1: Section 15 Transportation Framework Plan

Phase One Roads ———

Note: The Transportation Framework is conceptual. Final road alignments will be subject to refinement with Final Design Plans and Site Improvement Plans. Some Roads may be built in phased increments based on capacity needs.

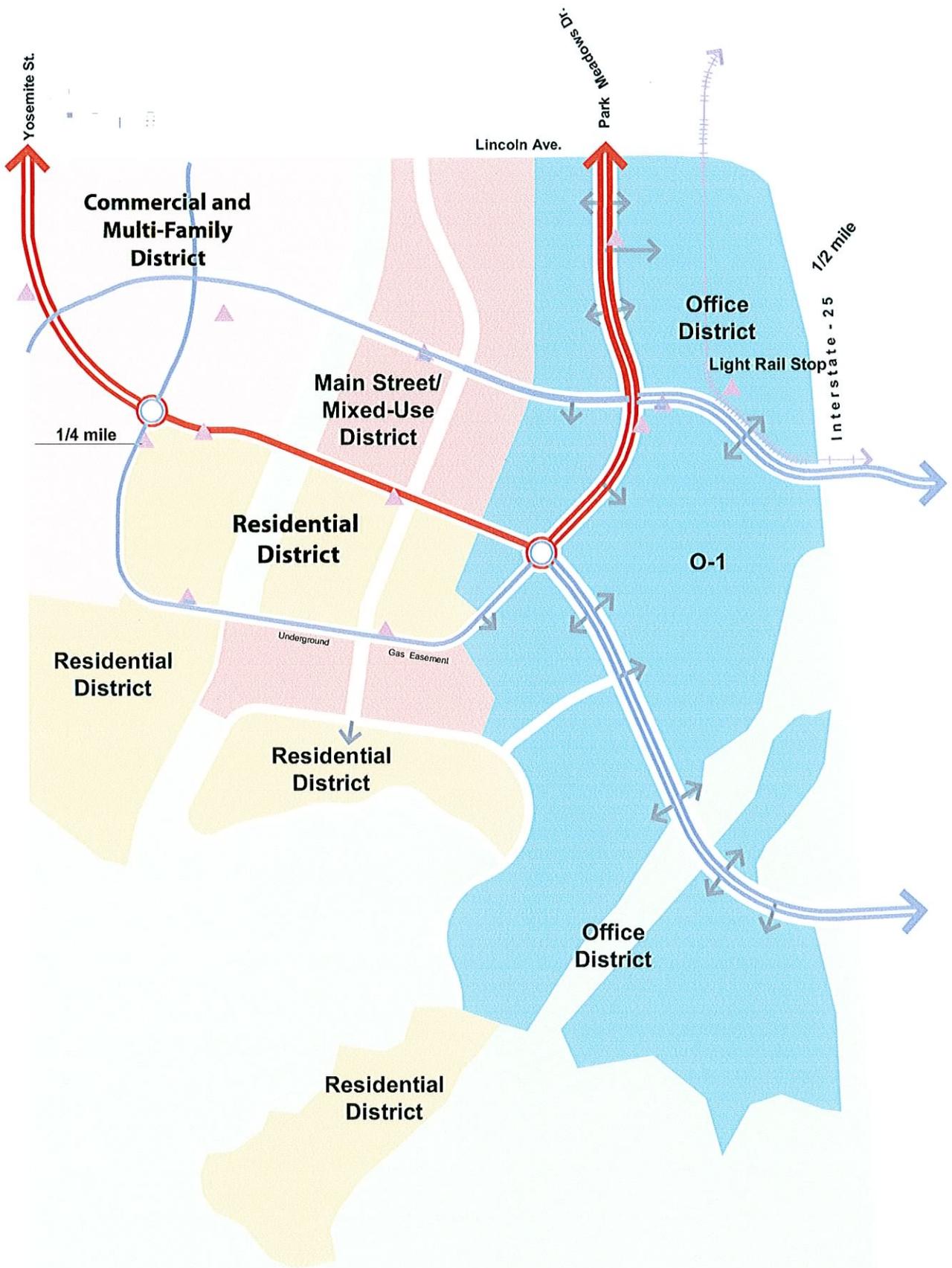


Exhibit 3.2: Section 15 Public Transportation Framework Plan



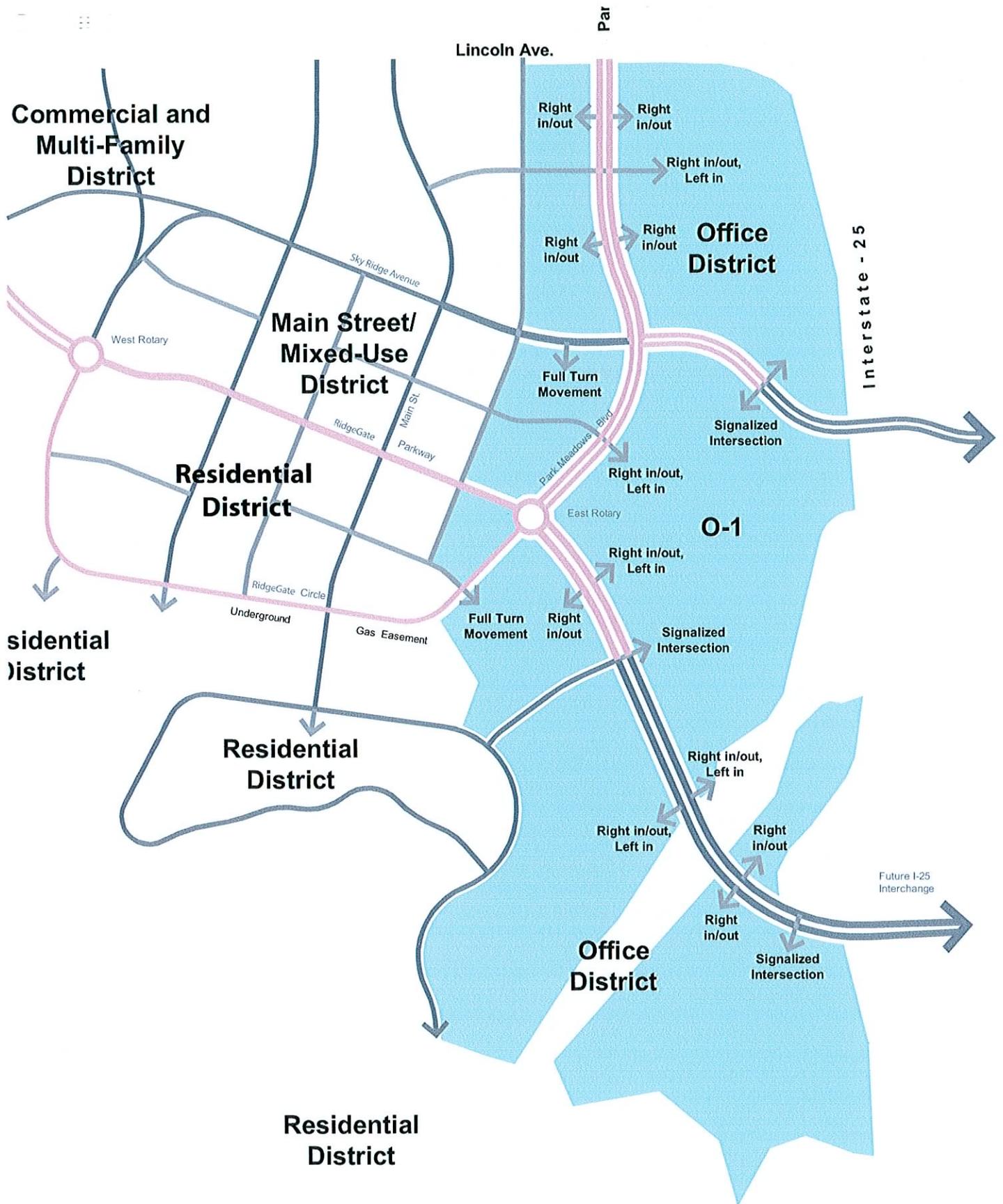
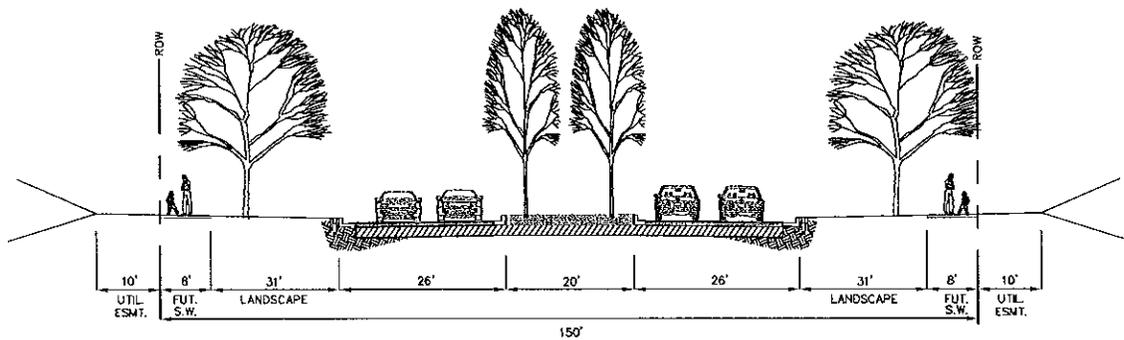
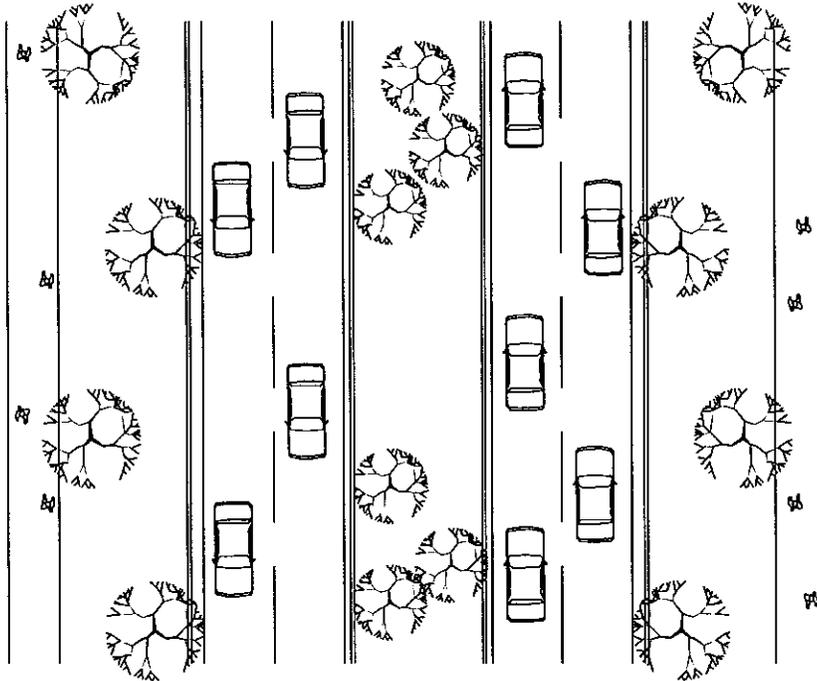


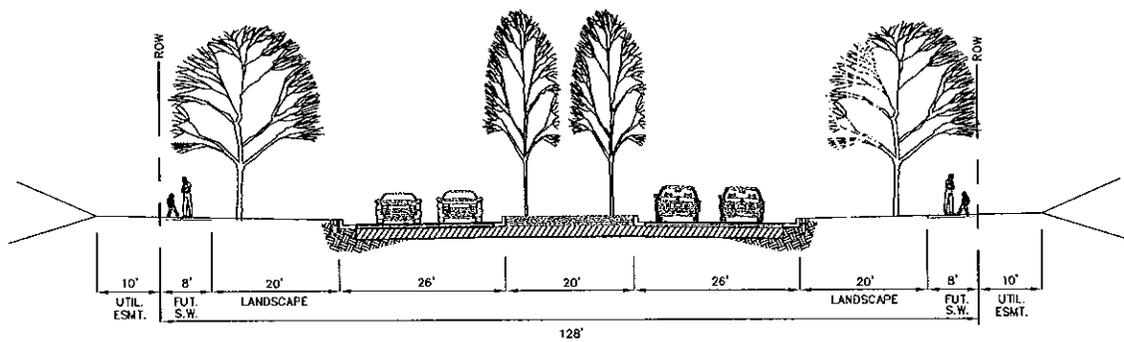
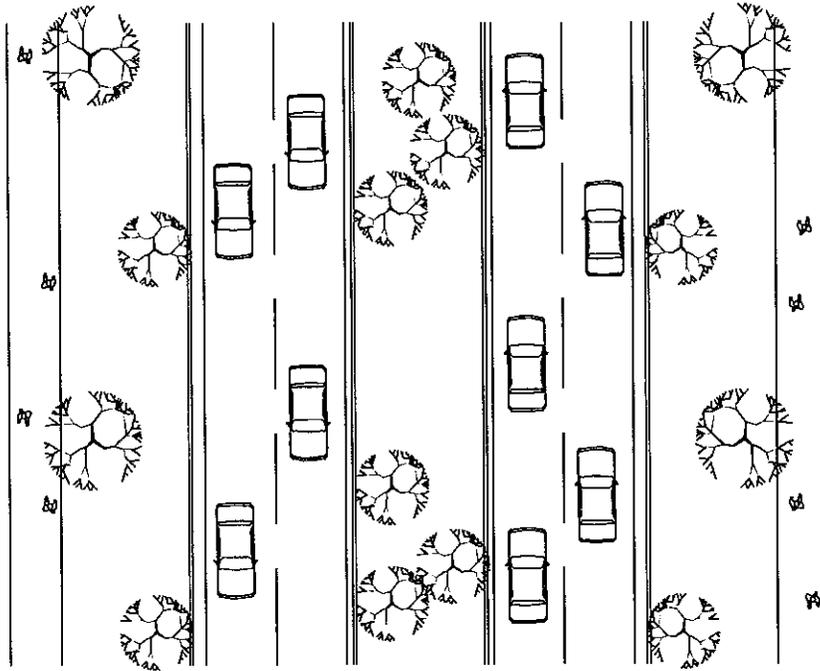
Exhibit 3.3: Vehicular Access Points - Office District

Phase One Roads ———



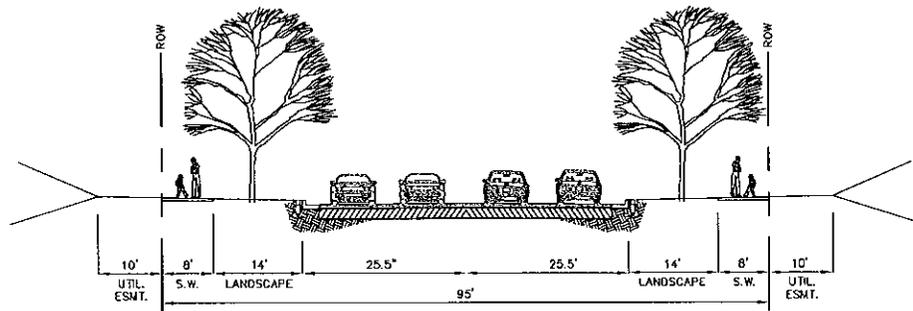
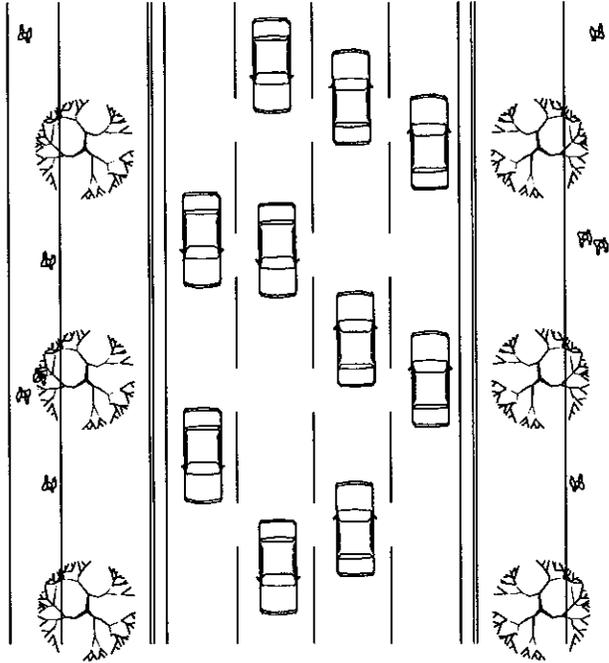
PARK MEADOWS BLVD.
 N.T.S.
 (STD. 4 LANE SECTION)

Exhibit 3.4A: Street Sections - Park Meadows Blvd. (4 Lane with Median)



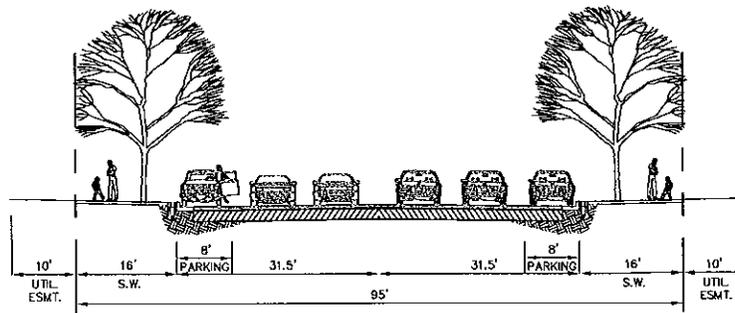
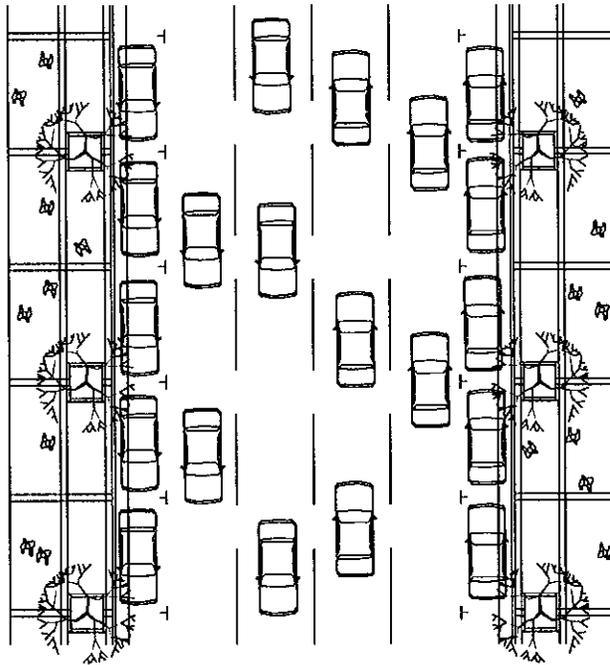
RIDGEGATE PARKWAY
 N.T.S.
 (STD. 4 LANE WITH MEDIAN SECTION)

Exhibit 3.4B: Street Sections - Ridgeway Parkway (4 Lane with Median)



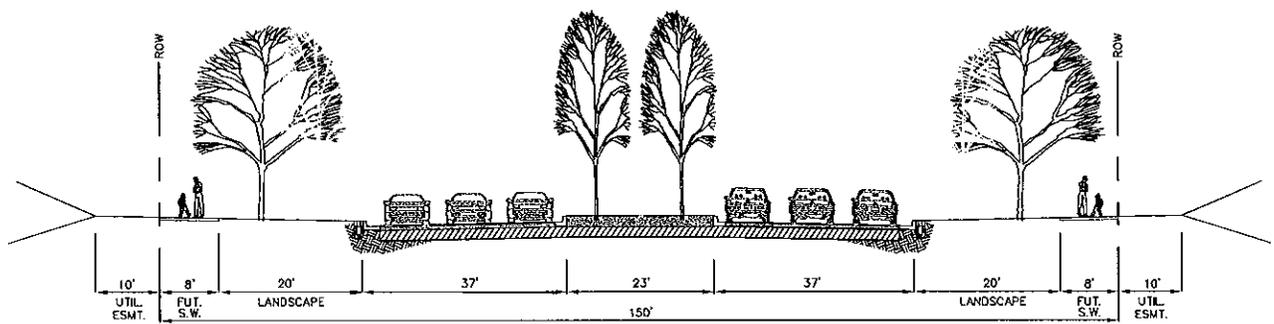
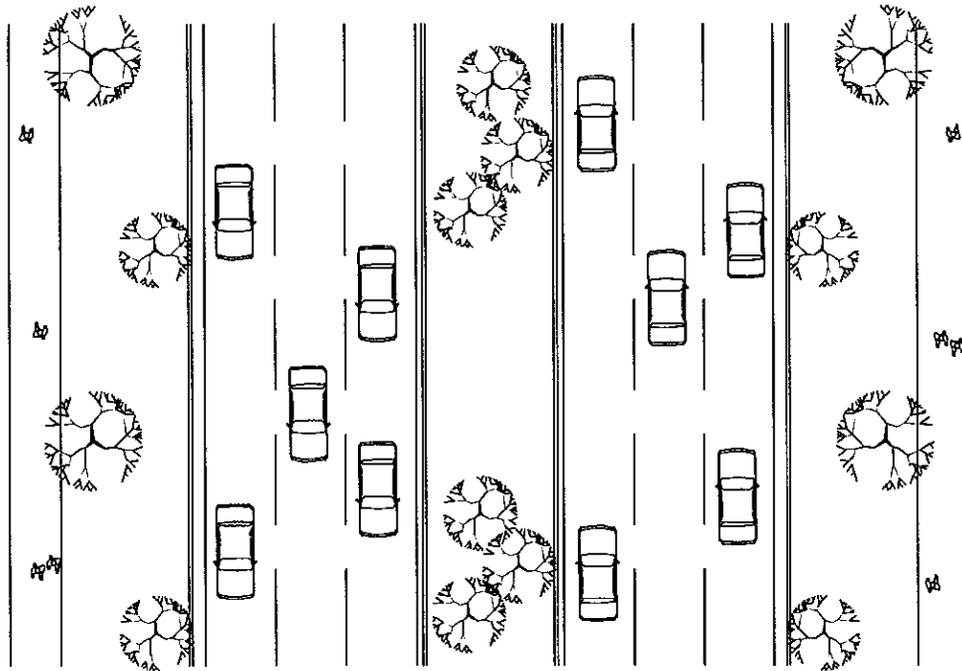
RIDGEGATE PARKWAY
 N.T.S.
 (STD. 4 LANE SECTION)

Exhibit 3.4C: Street Sections - Ridgeway Parkway (4 Lane)



RIDGEGATE PARKWAY
 N.T.S.
 (STD. 4 LANE SECTION WITH PARKING)

Exhibit 3.4D: Street Sections - Ridgeway Parkway (4 Lane with Parking)



RIDGEGATE PARKWAY
 N.T.S.
 (STD. 6 LANE SECTION)

Exhibit 3.4E: Street Sections - Ridgagate Parkway (6 Lane Median)

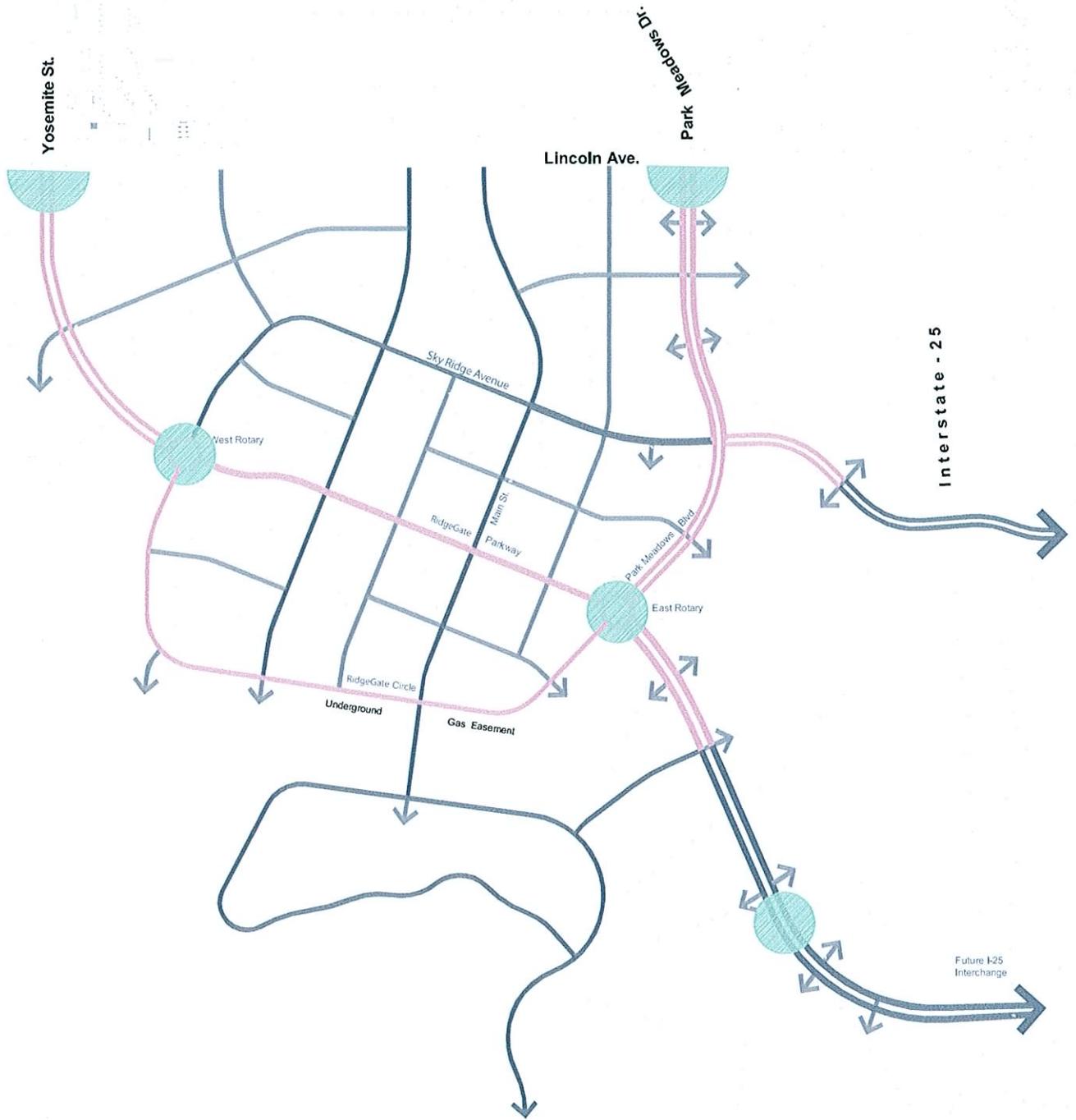
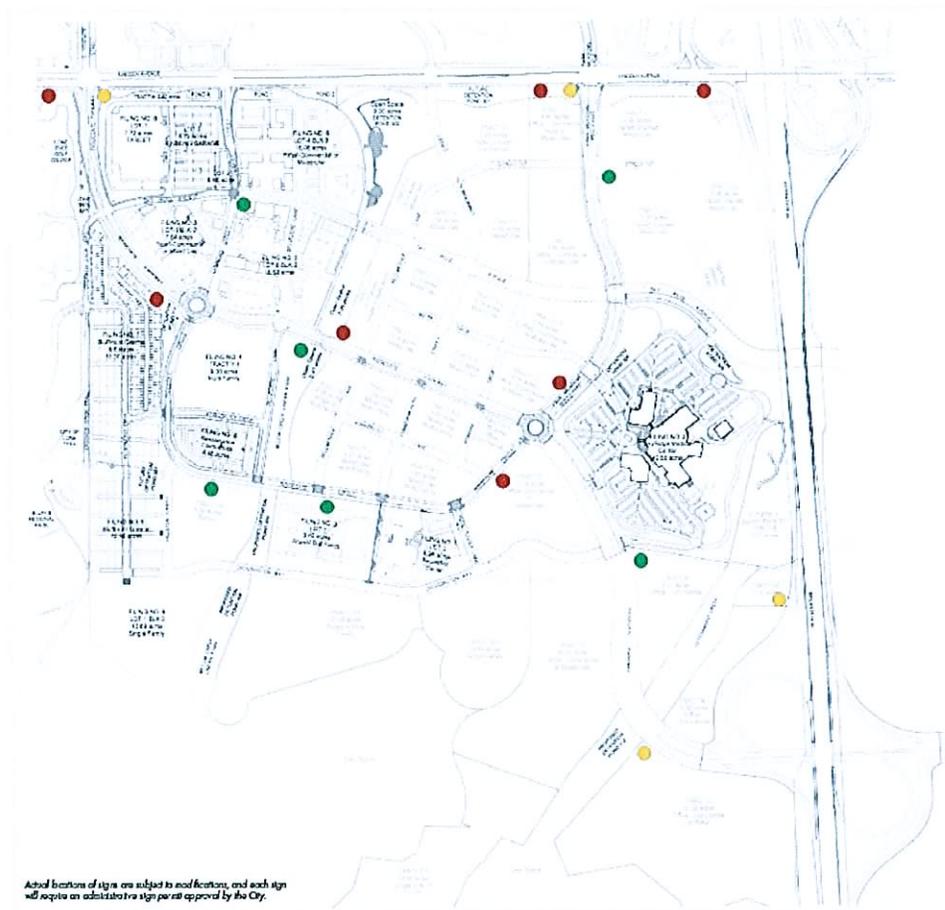


Exhibit 3.5: Project Identity - Entry Features/Signage

Entry Feature/Signage 



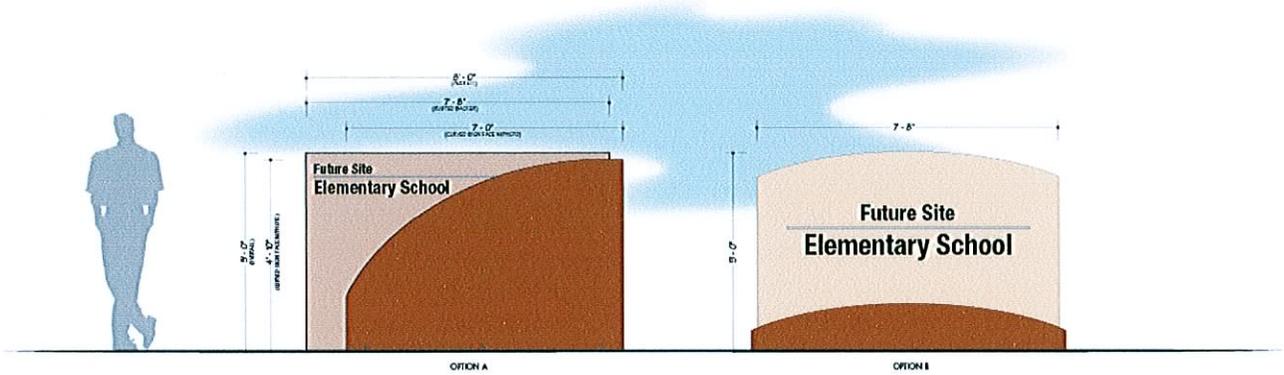
| RIDGEGATE SIGN LEGEND | |
|-----------------------|---|
| | Entry Monuments |
| | Directional Kiosks |
| | Future Amenity Sites |
| | Street Kiosk Signs <i>Temporary moveable signs, locations TBD</i> |
| | Builder Identification Signs <i>Temporary, 1 per project, locations TBD.</i> |

Actual locations of signs are subject to road locations, and each sign will require an administrative sign permit approval by the City.

10/1/2018

Exhibit 3.5A: Wayfinding Signage Program

FUTURE AMENITY SIGNS (TEMPORARY)



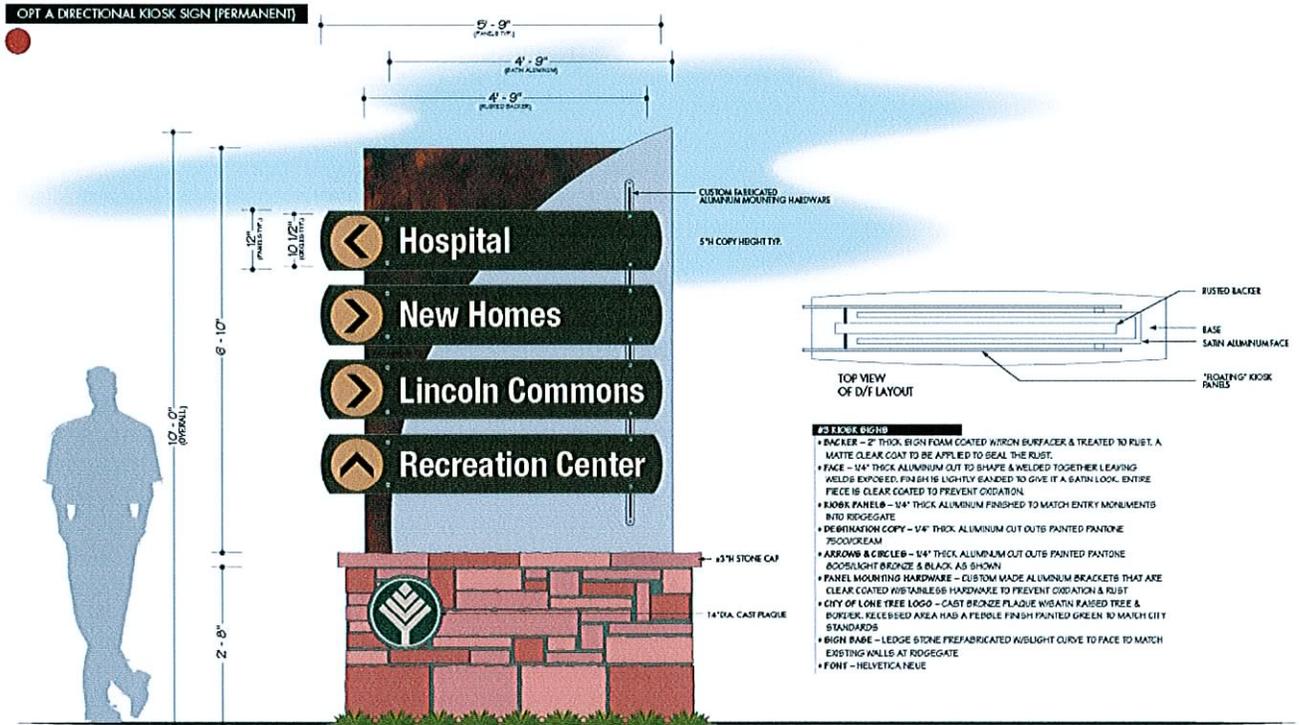
2.2 FUTURE SITE SIGNS

CONCEPTUAL ONLY - ACTUAL COLORS, MATERIALS AND METHODS OF FABRICATION TO BE DETERMINED

| | | | | |
|-----------|--|----------------|--------------|-------------------------|
| RIDGEGATE | RIDGEGATE 2.FUT SITE_A_B - GD-001 - MATT SHEREDY | 1/2" = 1' - 0" | 05 - 14 - 06 | Mathallonal Systems Inc |
|-----------|--|----------------|--------------|-------------------------|

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Exhibit 3.5B: Wayfinding Signage Program - Future Amenity Signs (Temporary)

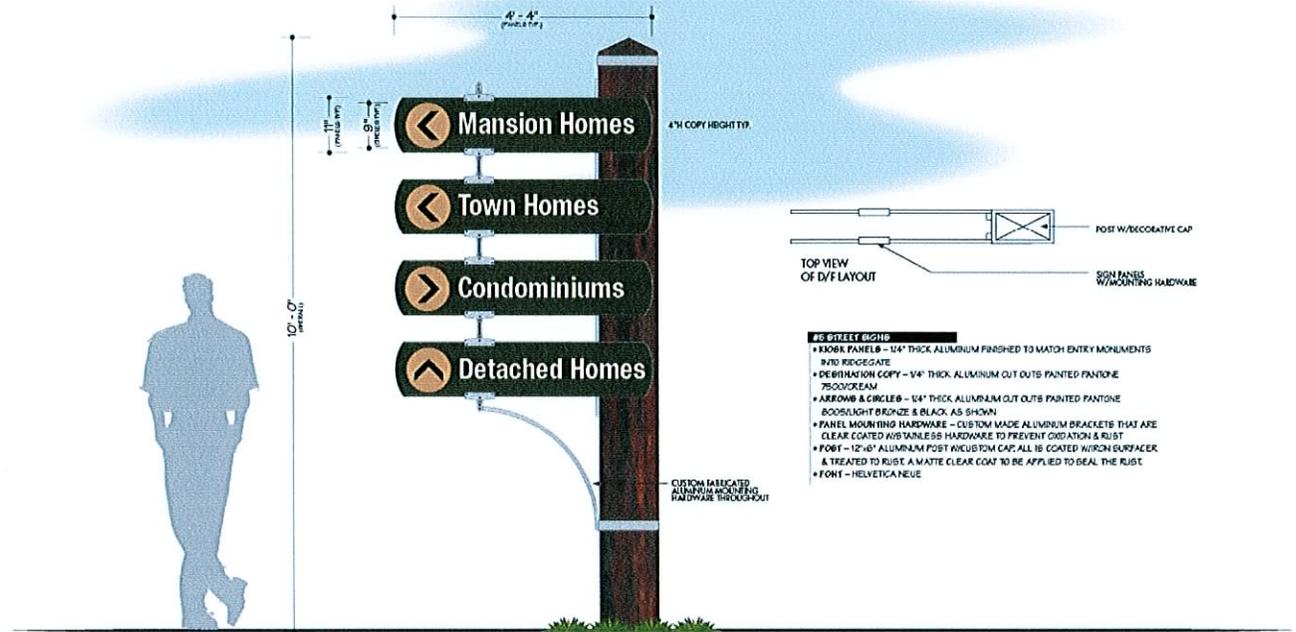


| | | | | |
|-----------|---|----------------|--------------|-------------------------|
| RIDGEGATE | RIDGEGATE 3 KIOSK OPT A TAB - GD-001 - MATT SHEREDY | 3/4" = 1' - 0" | 03 - 14 - 06 | Mathational Systems Inc |
|-----------|---|----------------|--------------|-------------------------|

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Exhibit 3.5C: Wayfinding Signage Program - Option A Directional Kiosk Sign (Permanent)

STREET KIOSK SIGN (TEMPORARY & MOVEABLE)



| | | | | |
|-----------|--|----------------|--------------|--------------------------|
| RIDGEGATE | RIDGEGATE 5 STREET TAB - GP-001 - MATT SHEREDY | 3/4" = 1' - 0" | 01 - 12 - 06 | Motivational Systems Inc |
|-----------|--|----------------|--------------|--------------------------|

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Exhibit 3.5E: Wayfinding Signage Program - Street Kiosk Sign (Temporary & Moveable)

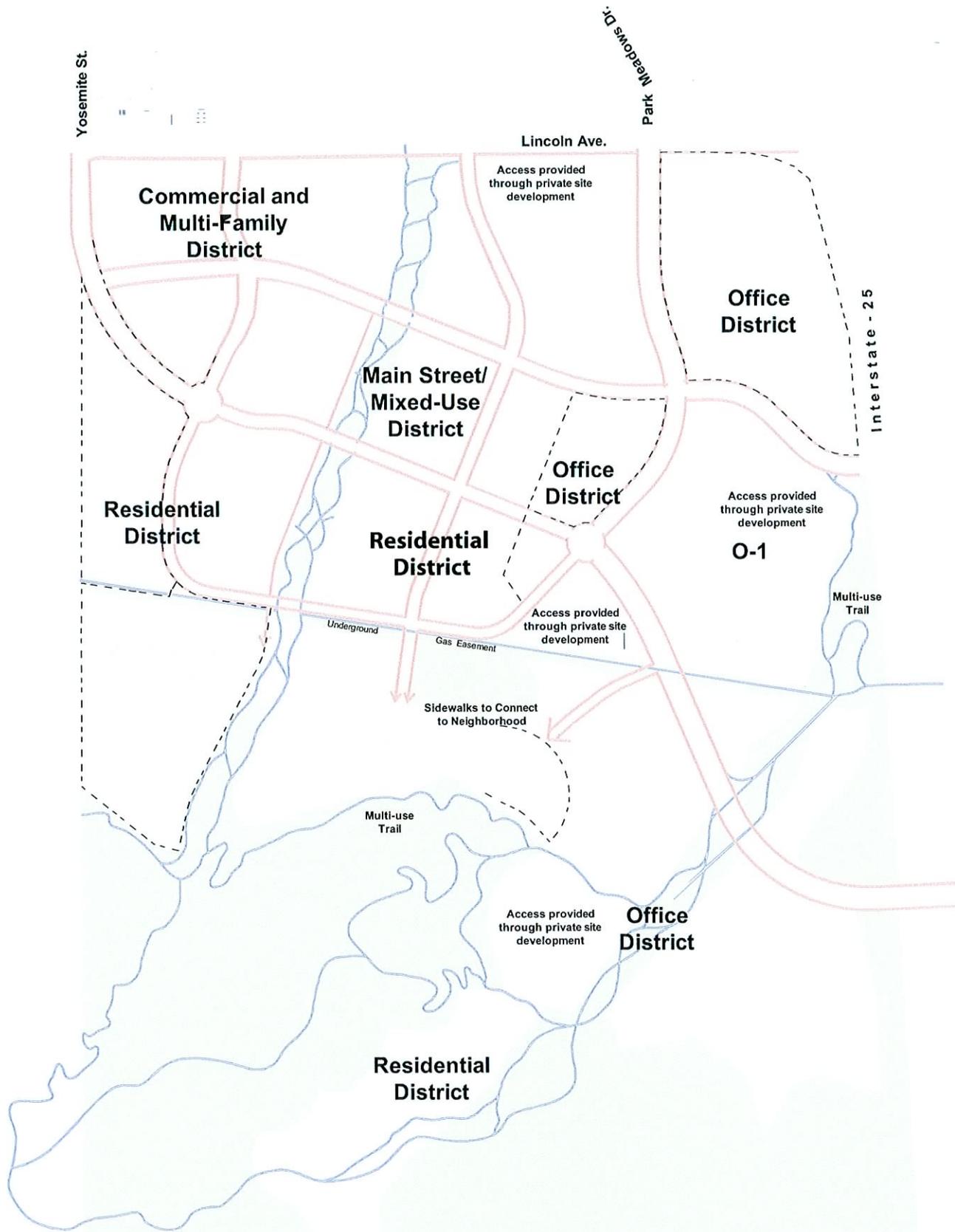


Exhibit 3.6: Section 15 Pedestrian and Bike Access Framework Plan

Multi-use Trail ———
Sidewalk ———

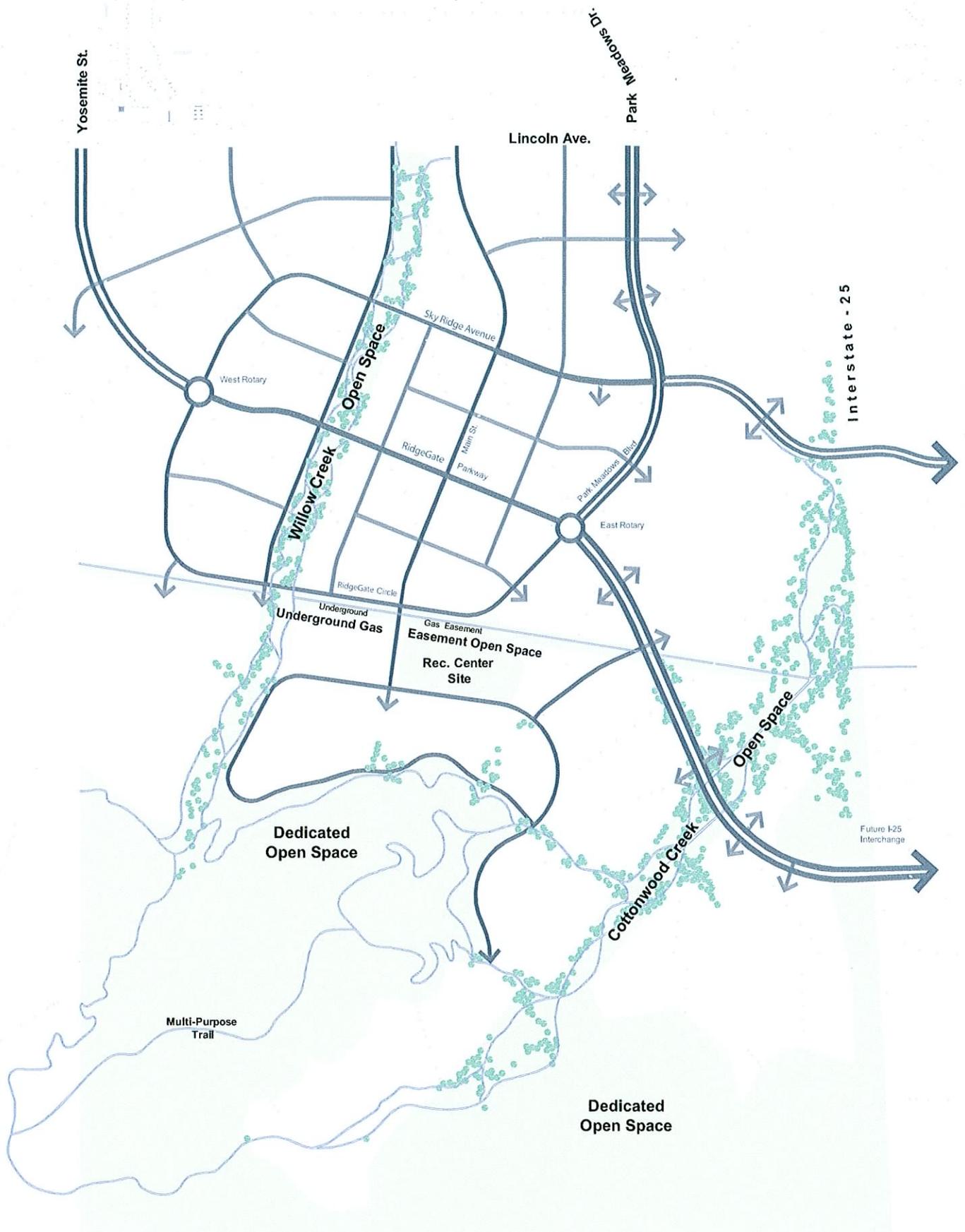


Exhibit 3.7: Section 15 Open Space Framework Plan

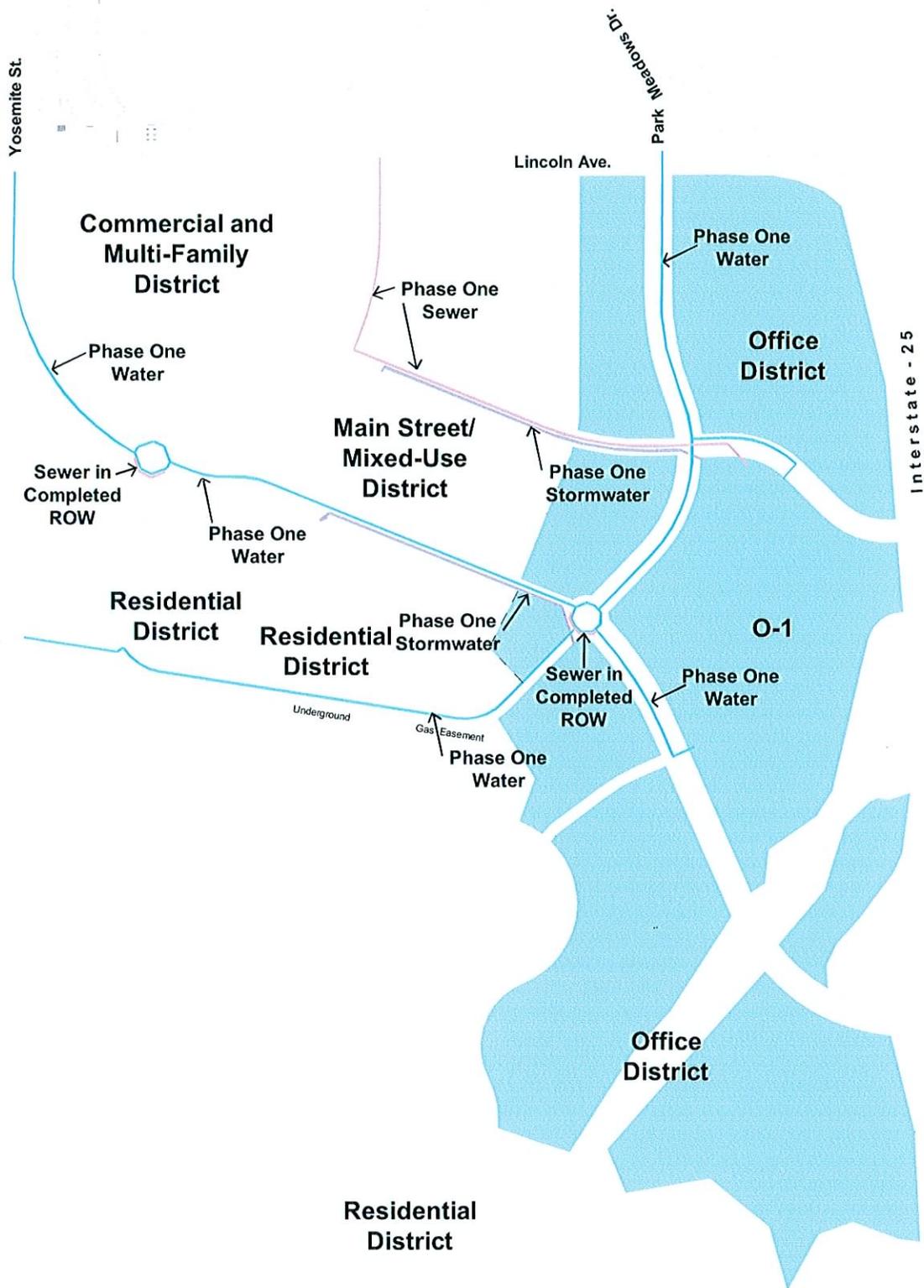


Exhibit 3.8: Utilities Plan - Phase One



Exhibit 3.9: Stormwater Management Concept Plan

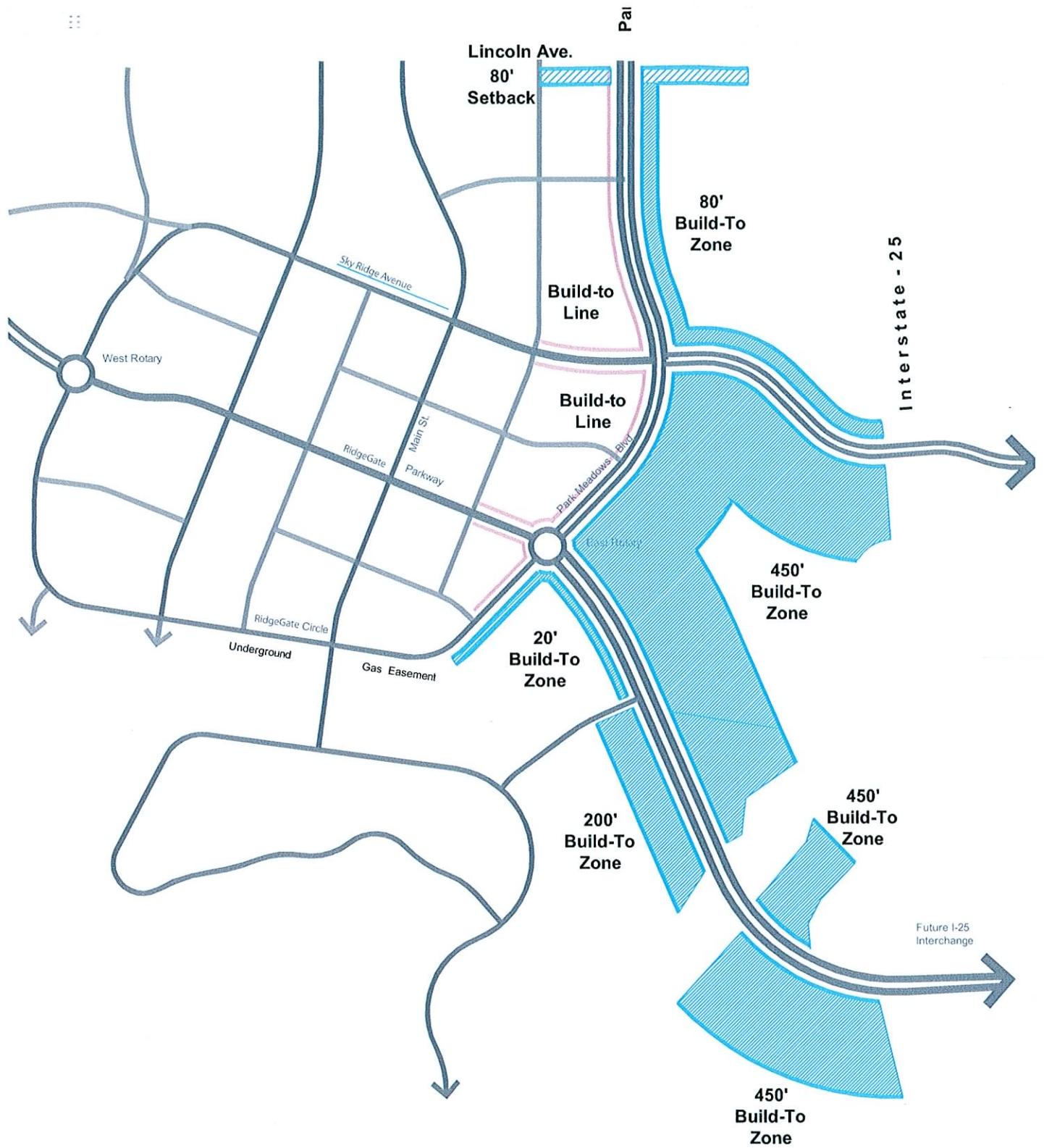


Exhibit 4.0: Build-to and Setback Location Plan

Build-to Line
 Build-To Zone
 Setback

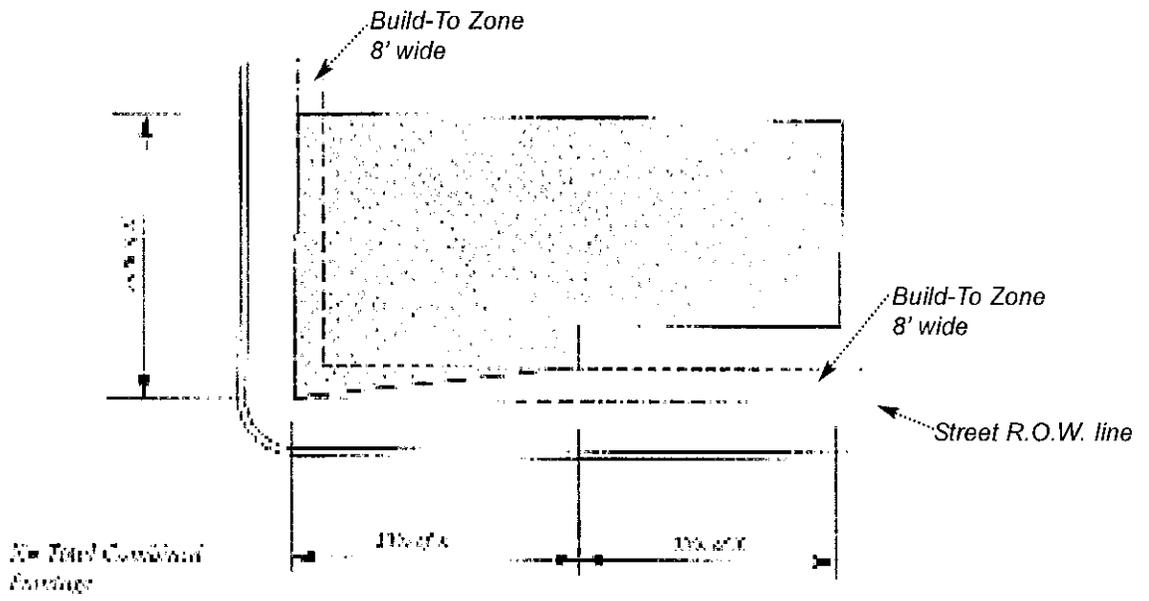


Exhibit 4.1: Build-To Line - Corner Example

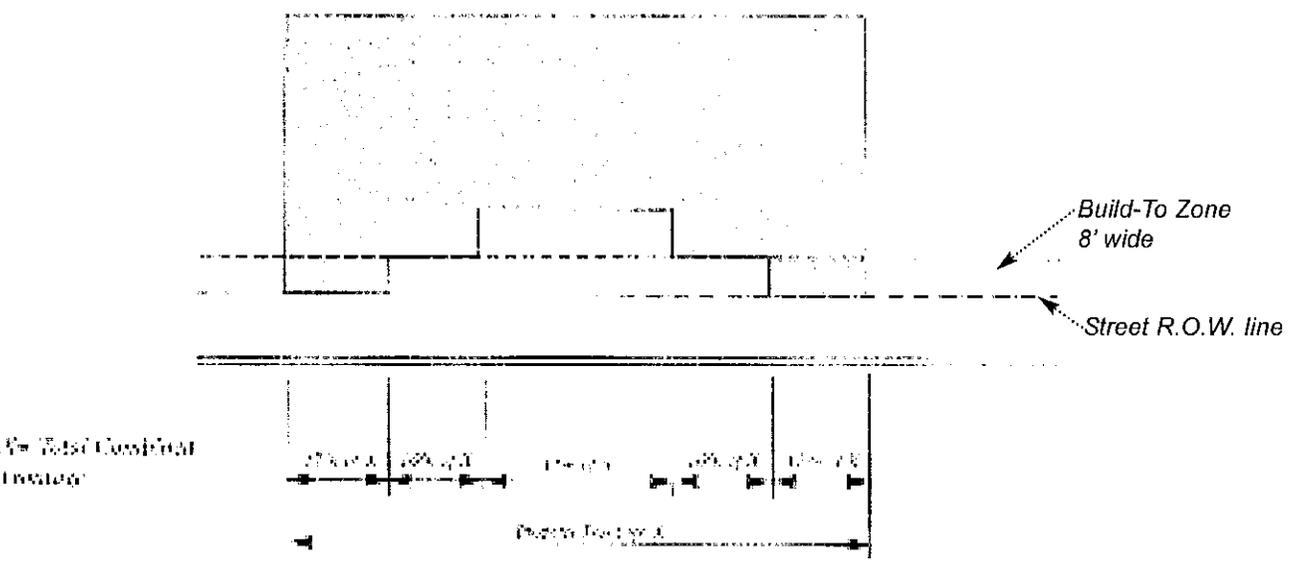
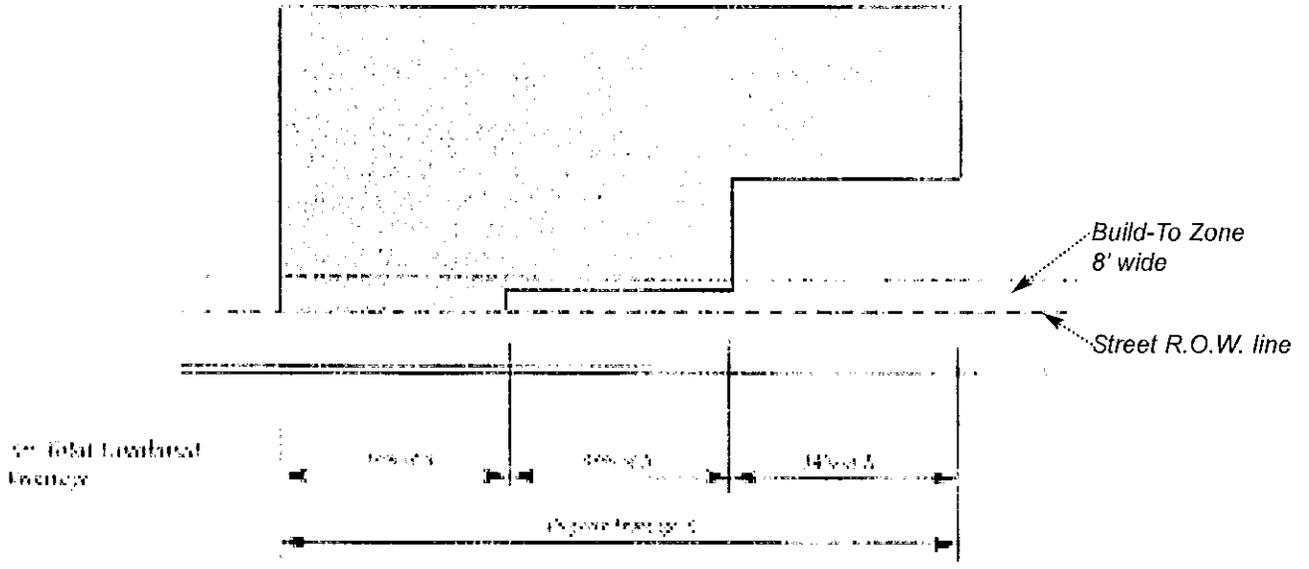


Exhibit 4.2: Build-To Line - Midblock Example