



RIDGE  GATE
at Lone Tree

Sky Ridge Station Transit Oriented Development
Subarea Plan
January 2015

RidgeGate
Lone Tree, Colorado
Sky Ridge Station Transit Oriented Development
Subarea Plan
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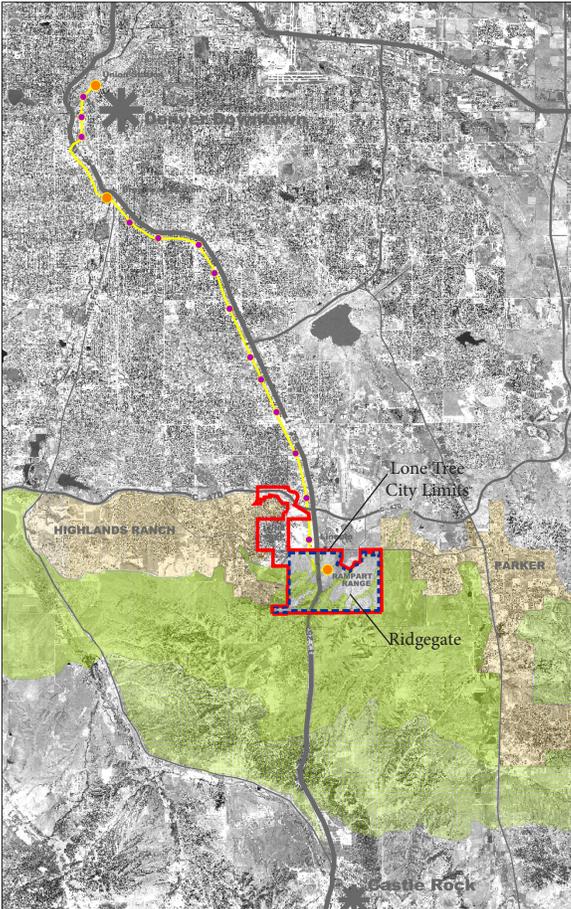
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+01/ RidgeGate Planned Development District (PDD)

1.1 Planning Concept

Exhibit 1.0: PDD Zoning Plan - Overall Project



Airphoto of the south metro Denver region and RidgeGate's strategic location to leverage transportation and open space assets.

RidgeGate consists of approximately 3,500 acres and is located in northern Douglas County, Colorado, in the City of Lone Tree. RidgeGate's position at the natural edge of the metro-Denver area and its adjacency to existing surface and planned light-rail transportation systems create a unique set of circumstances and opportunities.

RidgeGate is envisioned as an example for smart growth along Colorado's Front Range. It locates an integrated community of housing, employment and services immediately adjacent to existing urban development. RidgeGate's development framework applies this locational advantage to create a series of planning areas - relatively concentrated, efficient mixed-use neighborhoods that help to reduce the cost of infrastructure, services and transportation.

The Planning Areas have distinct edges defined by an open space system that also serve to protect sensitive watersheds and habitat and provide a framework for an integrated trail system.

Individual Subarea Plans and related Design Guidelines will be developed prior to or concurrent with platting. Subarea Plans may include complete or partial planning areas, or a combination of multiple planning areas. These will serve to further define development standards that allow for flexibility to respond to market conditions, yet provide specificity so that development implements the planning concepts. Any conflict in provision(s) between the PDD and the Subarea Plans and Design Guidelines will be resolved by the most restrictive or higher standard(s) or requirement(s) governing, unless determined otherwise by the City of Lone Tree Director of Community Development (the "Director," as referred to in the City of Lone Tree's Planned Development District Ordinance General Provisions), in consultation with the City Council.

1.2 Planning Framework

City Center Planning Area

The City Center Planning Area is a compact, pedestrian oriented and mixed-use environment providing a wealth of choice for retail, business, civic, cultural and residential uses as well as an anchor for the community. A multi-modal transportation center that further supports the area as a center of activity will be located in the City Center.

The City Center is sited at a knoll that affords visibility from I-25 and views to the Front Range. It is adjacent to one of the RidgeGate's major open space systems to provide easy access and establish the relationship of "a city in a landscape". The City Center Planning Area is planned to create an urbanized environment with buildings at the edges of sidewalks, on-street and structured parking, an interconnected street network and a commitment to high quality and functional public spaces. It is also envisioned that this area may become the location for the City of Lone Tree's municipal offices, government offices and a range of cultural facilities. Office and other non-residential components will be required to meet a minimum FAR in order to support the character and intensity required to create a vibrant urban environment. Residential components are expected to be of a higher density with only limited single-family detached homes allowed.

Commercial/Mixed-use Planning Area

The Commercial/ Mixed-use Planning Areas are mixed-use neighborhoods with an emphasis on commercial uses. They arrange residential, office and retail uses around a central focus such as public spaces and buildings that are generally within walking distance to the edge of the area. The Commercial/ Mixed-use Planning Areas are intended to create a balanced mix of uses in a pedestrian friendly environment with an interconnected street network and multiple connections to open space systems that define edges of the district. Residential components are expected to be of a higher density and will buffer themselves from adjacent non-residential components. It is expected that some residential components may be located on floors above non-residential uses and other creative approaches of housing densities and styles will be encouraged.



City Center development character



Commercial/Mixed-Use development character



Residential/Mixed-Use development character



Residential/Mixed-Use development character



Residential/Mixed-Use development character

Residential/Mixed-use Planning Area

The Residential/Mixed-use Planning Areas are mixed-use neighborhoods with an emphasis on residential uses. They arrange residential, office and retail uses around a central focus such as public spaces and buildings that are generally within walking distance to the edge of the area. The Residential/Mixed-use Planning Areas are intended to create a balanced mix of uses in a pedestrian friendly environment with an interconnected street network and multiple connections to open space systems that define edges of the district. Residential components are expected to reflect a range of densities with attached and detached housing styles encouraged through creative planning that minimizes streets with garages or parking areas. Non-residential components will be connected to, but appropriately transitioned from, single-family detached residential components and will not be located on residential level local streets, unless appropriate and as determined by the Director. Multi-family residential (attached residential units in excess of three stories or more in height) will be limited to certain percentages of some Planning Areas, as referenced on Sheet 4 of the PDD.

Rural Residential Planning Area

The Rural Residential Planning Area is a residential neighborhood set back from the edge of the mesas in the southwest portion of the community. Due to their abrupt rise in elevation and the expansive views to the west and northwest, these mesas are a visually dominant landscape feature and regional landform. Therefore, development of these areas is guided by restrictive standards intended to preserve visual character from I-25. Homes located in the Rural Residential Planning Area may range from single family homes on larger lots to homes arranged in a clustered pattern. Other techniques may be utilized, such as restricted building envelopes and fencing restrictions, to provide for continuous areas of natural open space, with the objective of preserving views and maintaining areas for wildlife habitat and movement.

Open Space Planning Area

Open space is an important framework element of the RidgeGate community to create a "city in a landscape". This open space system is concentrated to preserve sensitive riparian and floodplain habitats, create neighborhood edges and connect Planning Areas with trail systems. A majority of the system provides passive recreation opportunities incorporating regional trail connections and buffers with native landscape character. A Central Community Park of approximately 77 acres will be located in close proximity to the greatest concentration of housing and jobs and will provide opportunities for active recreation uses. Local neighborhood park sites identified in the Statement of Commitments will be located by a subsequent detailed Parks Open Space and Trails Master Plan and in the Subarea Plans.

The concept of equestrian uses as part of the on-site trail network through the East-West trail system will be explored and may be possible, provided careful consideration is given to public safety issues in light of the urban nature of the project to determine if these issues can be satisfactorily resolved and the owner is not liable or responsible for equestrian crossings at Lincoln Avenue to connect these trails to equestrian communities located to the northeast.



Open Space development character



Open Space development character



Open Space development character

+02/ Subarea Plan/Design Guideline Intent and Purpose



Sky Ridge Station TOD development character

The long-term nature of the development implies that the provisions of the PDD may not address every subject. Accordingly, Subarea Plans will define a greater level of detail for transportation, land use and open space frameworks and development standards and guidelines. These Subarea Plans may address complete or partial planning areas, or a combination of multiple planning areas and will be prepared at or near the time of anticipated development. The Director shall have the authority to interpret future Subarea Plans and Design Guidelines, and to interpret certain City regulations and standards for consistency with the planning concepts and objectives of the PDD, the Annexation and Development Agreement, the Subarea Plans and the Design Guidelines, provided that such interpretations do not conflict with the City of Lone Tree's Comprehensive Plan, as reasonably determined by the Director in consultation with the City Council.



Sky Ridge Station TOD development character

2.1 Sky Ridge Station TOD Subarea - Concept

The Sky Ridge Station TOD is composed of the portions of the property that are within close walking distance of the Sky Ridge Station of the Southeast Light Rail line and are well suited for a balanced mix of uses. Because of this special location, it is important that the character of such development, and its related streetscape, presents a positive, high-quality identity for RidgeGate and Lone Tree. The Subarea Plan will outline the framework criteria for infrastructure, site development and architectural character in this area so that users understand the minimum expectations and the quality and character standards of the overall development.

The Sky Ridge Station TOD is identified on Exhibit 2.0 and is approximately 30 acres. The Subarea is located east of Park Meadows Blvd., west of I-25, south of Lincoln Avenue, and north of Sky Ridge Avenue.

2.2 Subarea Plan/ Design Guideline Administration

All structures, whether new or to be renovated, shall be subject to the Subarea Standards and Guidelines contained herein and the separate prior approval of the RidgeGate Design Review Committee ("DRC"). The Design Review Committee will be composed of land owner, professional and Metropolitan District representatives and is authorized by the property's C C & R's. The City of Lone Tree shall not issue permits for use and construction until all applicable DRC requirements have been met.



Sky Ridge Station TOD development character

Concept

The Subarea Standards and Guidelines contained herein address the quality of the environment, recognizing that it is ultimately formed by numerous individual, private, and public decisions. They are intended to promote a clear, consistent and predictable process for the development of land within RidgeGate. These Subarea Standards and Guidelines are distinct from the DRC's Design Standards and Guidelines.

Relationship between the Design Review Committee and the City of Lone Tree

Design Review Committee approval, must precede a submission to the City of Lone Tree Community Development Department for the applicable project review of subdivision, site improvement plan, etc., the process for which is separate and independent from the DRC's.

Site Development Resources and References

RidgeGate Design Review Committee

720/279-2581 (t)

720/279-2582 (f)

City of Lone Tree

Community Development Department

303/708.1818 (t)

303/225-4949 (f)

Engineering Department

303/792.0557 (t)

303/792-9489 (f)

Building Department

303/708.1818 (t)

303/225-4949 (f)

Lone Tree Police Department

303/339.8150 (t)

303/339-8179 (f)

Rampart Range Metropolitan District

303/779.5710 (t)

303/773-2050 (f)

South Metro Fire Rescue

720/488.7200 (t)

Southgate Water and Sanitation District

303/779-0261 (t)

303/779-0220 (f)

Xcel Energy

Builders Call Line

800/628-2121 (t)

Centennial Airport

Properties in the Sky Ridge Station TOD Subarea Plan will be required to coordinate with Centennial Airport regarding their Aviation Easement and the filing of FAA Form 7460-1.

Mike Fonapfel, Director of Planning & Development

303/218-2903 (p)

Colorado Department of Public Health & Environment

Properties will be required to apply for a Colorado Discharge System Permit.

303/692-3500 (p)

303/782-0309 (f)

Note:

The City's review process may involve consultation from additional referral agencies and Homeowner Associations.

+03/ Sky Ridge Station TOD Subarea Plan

3.1 Sky Ridge Station TOD Subarea Plan Boundaries

Exhibit 3.0: Sky Ridge Station TOD Boundary

Exhibit 3.0 delineates conceptual boundaries for the Sky Ridge Station TOD Subarea

3.2 Transportation Framework

See RidgeGate Roadway Standards & Guidelines

Public streets in the Sky Ridge Station TOD area are designed to accommodate automobile traffic volumes appropriate for this TOD area and also serve as a framework for pedestrians, cyclists, rail and bus transportation systems to offer options in reducing the reliance on the automobile and promote access to transit ridership.

Hierarchy of Street Design Features

Streetscape concepts have been developed to reinforce the hierarchy of streets within RidgeGate. The following streets envisioned to provide access to the Sky Ridge Station TOD do not represent approved or final designs. Final design will be reviewed and approved by the City Departments of Engineering and Community Development, Southgate Water & Sewer District and South Metro Fire Protection District.

The following public streets have been built in the initial phase of development and will serve the Sky Ridge Station TOD.

Park Meadows Blvd. (South of Lincoln Avenue to Sky Ridge Avenue)

Primary Role:

- The major connection between the RidgeGate Parkway interchange and Lincoln Avenue and a major entry into the subarea with signalized intersections at Chatham Drive and Sky Ridge Avenue.

Primary Characteristics:

- Vehicular-oriented street with higher traffic volume on four lanes serving medium density land-use (office and hospital)
- Orient uses towards the street.

Sky Ridge Avenue (East of Park Meadows Blvd. to I-25 Right of Way)

Primary Role:

- The major roadway connection between the western area of the West Village and the Sky Ridge Station TOD and the major entry into the subarea. A future signalized intersection is planned at Sky Ridge Avenue and Park Meadows Boulevard, with a signalized intersection in operation at Park Meadows Boulevard and Chatham Drive and access to light rail station.

Primary Characteristics:

- Vehicular oriented street with higher traffic volume on four lanes of traffic (may have on-street parking) and serving high density land uses (commercial, residential and transit)

Trainstation Circle (East of Park Meadows Blvd. to I-25 Right of Way and south to Sky Ridge Avenue)

Primary Role:

- An internal loop road providing access to all sites within the TOD area. The southeast segment, with access at Sky Ridge Avenue, will also provide an at-grade light rail crossing.

Primary Characteristics:

- A medium-speed connector between mixed-use, office, and residential land uses. Uses should be oriented towards the street with a direct sidewalk connection to front entrances. Mixed-use areas are encouraged to have build-to lines at the sidewalk.

Chatham Drive (East of Park Meadows Blvd. to Trainstation Circle)

Primary Role:

- Entry access road to the area and Trainstation Circle with a signalized intersection at Park Meadows Blvd. and traffic signs at the Trainstation Circle intersection.

Primary Characteristics:

- A medium-speed connector between mixed-use, office, and residential land uses. Uses should be oriented towards the avenue with a direct sidewalk connection to front entrances. Mixed-use areas are encouraged to have build-to lines at the sidewalk.



Transit/Development integration example

Pedestrian Routes and Access Points

Exhibit 7.0: Section 15 Pedestrian and Bicycle Access

Pedestrian routes and access points have been defined per Exhibit 7.0 and are intended to reinforce a clear hierarchy and organization of pedestrian circulation within RidgeGate. Individual project development must accommodate this plan, unless an equivalent or superior solution is provided.

Bicycle Routes - to be consistent with Walk & Wheel Study

Exhibit 7.0: Section 15 Pedestrian and Bicycle Access

Bicycle routes have been defined per Exhibit 7.0 and are intended to reinforce a clear hierarchy and organization of bicycles, and other non-motorized wheeled vehicle circulation within RidgeGate.

Sidewalk Connections to Open Space

Sidewalk connections to individual development sites are intended to reinforce a clear hierarchy and organization of multi-purpose circulation within RidgeGate. Open Space trail connections support this hierarchy. Individual project developments must incorporate this plan, unless an equivalent or superior solution is provided.

Public Transportation Routes and Stops

Exhibit 6.0: Section 15 Public Transportation Framework Plan

Light Rail

An extension of the Southeast Corridor light rail by the Regional Transportation District (RTD) is anticipated to be located within the Sky Ridge Station TOD Subarea, as defined on Exhibit 6.0, including a basic rail stop (without public parking) near the intersection of Park Meadows Blvd. and Trainstation Circle. Development adjacent to the rail line and within one-half mile of the stop would be encouraged to maximize the benefit offered by this transportation system.

Bus/Rubber-Tire Shuttle

A bus/rubber-tire shuttle system is anticipated to expand and extend alternative transportation choices within RidgeGate. Exhibit 6.0 defines these anticipated routes and stops, to be developed utilizing waiting shelters.



Light Rail stop example



Light Rail stop example

Streetscape Elements

Street furniture, lights and trees are intended to unify the public right-of-way. Street trees are intended to provide shade and comfort for pedestrians, as well as establish a buffer from automobile traffic. They help spatially define and unify the overall streetscape theme.

The following design standards for streetscape elements apply to all public rights-of-way within RidgeGate. Street landscaping plans will be submitted to the City for approval before any installation occurs.

Design Standards

- Street furnishings, lights and trees shall not block a 6' wide (minimum) clear pedestrian walking zone or make maintenance of a clear walking surface (such as snow removal) difficult.
- Street lights and trees shall be placed in an orderly fashion to define the edge of the pedestrian activity zone.

Design Guidelines

- There should be a clear pedestrian walking zone of adequate width on both sides of the street to serve projected pedestrian needs.
- Street furnishings, including benches and trash receptacles, and trees should occupy consistent, well defined zones parallel to the pedestrian walking zone.

Street Trees

Design Standards

- All trees shall be a minimum of three-inch (3") caliper at the time of installation. A consistent standard for street tree species shall be developed and submitted to the City for approval with street landscaping plans before any installation occurs.
- All tree planting or removal within the public rights-of-way shall be approved by the City of Lone Tree.
- Existing healthy trees and their root systems shall be protected during construction through the use of barricades and fencing.
- Plant materials, fencing, or landscaping improvements greater than twenty-four inches (24") in height shall not extend into the sight triangle for any intersection of a street or driveway, as defined by the City of Lone Tree.



Shuttle bus example



Representative streetscape example



Representative streetscape example



Representative streetscape example

Lighting
Design Standards

- Lighting for pedestrians and vehicles, consistent with what has been approved elsewhere in the West Village, shall be provided within all public rights-of-way. A consistent standard for lighting shall be developed and submitted to the City for approval with street landscaping plans before any installation occurs.

Seating
Design Standards

- Benches for pedestrians shall be provided within public rights-of-way. A consistent standard for benches shall be developed and submitted to the City for approval with street landscaping plans before any installation occurs.
- Seating shall be placed to serve bus and transit stops, building entry areas and plazas.
- Sidewalk benches shall be oriented parallel to the curb line when utilized at the face of a building so as not to impede pedestrian movement.

Design Guidelines

- Benches should be of quality materials and have backs.
- Movable chairs and seating for sidewalk cafes are encouraged in public open space and sidewalk areas, so long as they are stored after hours or in inclement weather. Seating shall not impede a 6' clear pedestrian walking zone.

Sidewalk Paving

Design Standards

- Paving materials shall be of quality and durable materials such as, but not limited to, concrete, stone, concrete unit pavers or brick.
- All sidewalk paving shall meet City and ADA standards.

Design Guidelines

- Special paving materials should be used to highlight the pedestrian areas associated with street crossings, and special use areas such as plazas, building entries or café seating areas.



Representative seating example

3.3 Utilities Framework

Water

Water service will be provided by the Southgate Water District. Two pressure zones exist that will serve the site, Zone 9E and Zone 10E. Water main extensions within proposed roadway corridors will be required to provide site accessible water and fire protection. Zone 10E main line extensions and looping will extend from an existing water main within Yosemite Street via Ridgegate Parkway and Ridgegate Circle. Zone 9E main line extensions will extend from an existing water main located within Lincoln Avenue via proposed Park Meadows Drive, Commonwealth Street, Ridgegate Parkway, and Belvedere Lane roadway corridors.

Sanitary Sewer

Sanitary sewer service is provided by the Southgate Sanitation District in conjunction with the City of Englewood. Southgate Sanitation District contracts wastewater treatment with the City of Englewood. Southgate sewer main facilities have been extended into RidgeGate via Willow Creek. The Rampart Range Metropolitan District has constructed a main line connection from the north side of Lincoln Avenue west of Heritage Estates Circle across Lincoln Avenue. From a point on the south side of Lincoln Avenue, one main extension serves the west side of Willow Creek and one main extension serves the east side of Willow Creek.

Dry Utility Corridors/Easements (Electrical Power, Natural Gas, Telephone, Cable and Fiber Optic)

Dry utilities will generally follow proposed roadway alignments within utility easements on either side of the public roadways. Location and width of utility easements shall be determined at time of platting and dedicated as part of the final plat. Separate utility easement agreements may be required for unplatted parcels to allow extension of dry utilities to proposed platted sites.

3.4 Grading/Stormwater Management Framework

Regional Stormwater Management Strategy

The objectives of the Stormwater Management Plan includes the application of the "Best Management Practices" (BMP's) as defined in the Urban Drainage and Flood Control District "Drainage Criteria Manual, Volume 3." The purpose of the BMP's is to provide for a more integrated, sustainable, and localized system for stormwater management and flood control than conventional requirements. Key elements of the BMP's include the maximum use of grass lined and/or planted swales to encourage infiltration of

stormwater into the soil; the use of water quality enhancement basins and facilities to minimize the concentration of sediments and debris; and an emphasis on dispersal of detention areas to minimize the rapid concentration of area-wide flooding. When applied to RidgeGate, the BMP's will provide water quality management, minimize flood risk, and allow for re-establishment of natural riparian and wetland habitats for wildlife and recreation.

Parcel Requirements

- **Location of Detention or Retention**

Stormwater detention or retention located between buildings and the street is discouraged when other alternatives are possible.

The use of multiple detention areas is encouraged. Large and/or deep detention areas that dominate the landscape are discouraged.
- **Landscaping**

Each individual detention or retention area that is located on landscaped areas shall be improved and maintained with a landscape that includes ground covers or turf, shrubs, and trees compatible with the overall landscape design of the Lot. Low-flow channels and drainage control structures shall be designed for attractive appearance, including landscaping, grading and location to minimize their visual impact. Low water areas that may remain moist for extended periods shall be designed with surrounding mow-strips of concrete, planted, and maintained in a condition with a minimum of muds, silts, or standing water.
- **Basin Design**

The shape, grading, side-slopes, and location of detention or retention areas within the landscape shall be designed to integrate into the overall landscape design of the Lot of the proposed development. Curvilinear shapes with variation are preferred over rigid, engineered shapes.

Overlot Grading and Pre-Development Landscape Treatment

It is anticipated that community infrastructure development and parcel development will require overlot mass grading. Overlot grading will be required to transition to existing grades where parcels are adjacent to preserved open space. Any overlot grading anticipated to occur in advance of individual site development construction for more than thirty (30) days will be required to apply native grass seeding to minimize erosion and enhance aesthetic appearance.

3.5 Land Uses

The PDD defines land uses which are permitted by right, allowed with Special Review or disallowed in each of the Planning Areas appropriate for this Subarea.

3.6 Building Heights

The PDD defines maximum building heights permitted in this Subarea.

3.7 Floor Area and Density

Minimum and maximum development yields allowable in this Subarea are defined in PDD.

+04/

Sky Ridge Station TOD Subarea Standards and Guidelines

These Subarea Standards and Guidelines, as authorized by the PDD approved by the City in June, 2000, define a level of quality and provide for consistency among individual lots and public areas within the Sky Ridge Station TOD Subarea and are supplementary to the City's Design Guidelines.

Design Review Committee approval is mandatory for all projects in the Sky Ridge Station TOD prior to submission for the City of Lone Tree project review.

Definition of Terms

Design review evaluation criteria are organized by three headings (Concept, Standards, Guidelines) for each category.

Concept:

Concept statements define goals for which the standards and guidelines are created to achieve.

Standards:

Standards are objective criteria that provide specific direction based on the stated concept statement and are used to denote issues that are considered critical to achieving the concept. Standards use the term "shall" to indicate that compliance is required unless it can be demonstrated that an acceptable alternative meets one or more of the following conditions:

- The alternative equally or better achieves the stated intent;
- The intent, which the standard was created to address, will not be achieved by application of the standard in this particular circumstance.
- The application of other standards and guidelines to achieve stated intents will be improved by not applying this standard.
- Unique site factors make the standard impractical or cost prohibitive.

Guidelines:

Guidelines provide further considerations to promote the goals defined by the concept statement. Guidelines use the term "should" or "may" to denote they are considered relevant to achieving the stated concept, and will be pertinent to the review process but will not be required for approval. Guidelines will, however, be strongly considered in the event of a waiver to a related standard.

4.1 Site Planning

4.1.1 Transit Oriented Development

Concept:

- The area is a compact, pedestrian oriented mixed-use environment providing a wealth of choices for retail, business, cultural and residential uses as well as the multi-modal transportation hub for the west village of the community and further supports the area as a center of activity.
- In-line mixed-use/neighborhood station with shops, restaurants, office space, and park/plaza with pedestrian connections to the adjacent medical center, Transtation Circle and high density residential.
- The station is walking distance to adjacent neighborhoods and features a kiss-n-ride loop with RTD bus connections and a RidgeGate loop bus service.

4.1.1.1 Standards

- Development of the area may allow reduced parking standards for non-residential development, intensified structured parking, and phased development. It will include components of a high density mixed-use Transit Oriented Development including:
 - Hotel
 - Residential
 - Plazas/parks
 - Commercial

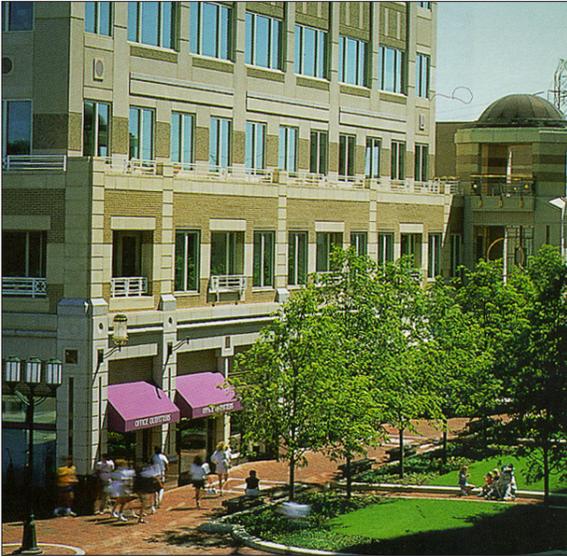
4.1.1.2 Commercial/Mixed Use

- Commercial / Mixed Use Areas place an emphasis on commercial uses. They may arrange residential, office and retail uses around a central focus like public spaces and buildings that are generally within walking distance to the edge of the area. The Commercial / Mixed Use planning areas are intended to create a balanced mix of uses in a pedestrian friendly environment with an interconnected street network.

4.1.1.3 SkyRidge TOD Core Area Objectives

Park/Plaza:

- The Sky Ridge TOD park/plaza (see Exhibit 3.0) will serve as a gathering space for workers, residents, and visitors.



- Provide shaded places for people to sit in the summer and sunny spaces for people to sit in the winter. Consider moveable tables and seats to allow for flexibility in a structured setting.
- Include landscaping such as flowers, shrubs and shade trees.
- Provide such amenities as a water feature and public art. Water features should be designed with water conservation in mind. Public art should be inspired, invite interaction by children and adults and show sensitivity to the surrounding context.
- Include pedestrian lighting features consistent with the RidgeGate West Village standard.
- Include trash receptacles.
- Provide pedestrian connections around the plaza. Outdoor restaurant patios or other uses should not encroach onto such sidewalks.
- No surface parking should front the plaza/park.

Core Commercial:

- Have ground floor uses such as retail, restaurants, services, daycare and/or other such activated uses front the park/plaza and station stop. Any structured parking facing the plaza/park should be wrapped with activated uses.

Architecture:

- Buildings adjacent to the Sky Ridge Light Rail Station should orient their front entry to the outdoor plaza/park. The pedestrian level should include translucent entrances and windows.
- Buildings should be constructed to the sidewalks in the TOD Core Area.
- Consider building step backs for structures higher than three stories for uses fronting the plaza/park to minimize shadow impacts and reduce the canyon effect.

Pedestrian Considerations:

- Provide for pedestrian safety, comfort, and accessibility for all ages and levels of mobility.
- Parking facilities and driveways should not impair pedestrian access between the station and other development or the street network.
- Provide weather protection to make getting to and from transit stops more comfortable. This may include building projections, colonnades, awnings and use of landscaping.



Setback building example

- Adequate lighting should be located along all pedestrian paths and sidewalks.
- Focal points or key nodes within the station's area should be used to guide transit users to the station. Examples of focal points may include public art, special streetscape treatments such as lighting, banners and pedestrian amenities.

4.1.2 Access and Circulation

Concept:

- Minimize vehicular access (curb cuts) on primary roadways to reinforce a clear hierarchy and organization of circulation within RidgeGate
- Maximize uninterrupted public sidewalks

4.1.2.1 Standards

- Development shall limit curb cuts to those areas where curb cuts are allowed as illustrated in Exhibit 5.0 Transportation Framework.
- Reasonable pedestrian connections between buildings and transit stops.
- Internal vehicular and pedestrian circulation within a development of multiple buildings or lots must interconnect in an obvious and consistent manner.
- Each project must connect to systems (walks, trails, open space) from adjacent parcels.
- Minimize curb cuts through shared access where possible.

4.1.3 Building Locations Concept:

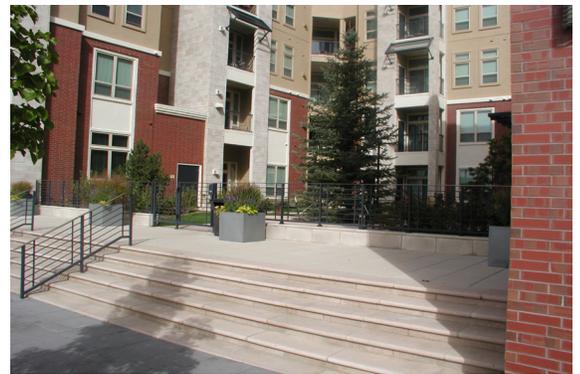
Exhibit 8.0: Build-To/Setbacks Diagram

Concept:

- Define and contain the street space. Concentrate and reinforce pedestrian activity.
- Create a sense of the street as a “place”.

4.1.3.1 Standards

- The location of setbacks that affect development within RidgeGate are illustrated in Exhibit 8.0: Build-To/Setbacks Diagram.
- Where so designated by a build-to line, the front building facade shall be located on or near the designated property line as explained below, unless an exception is granted by the DRC and City. The frontage shall be measured along the designated property line, and for corner lots shall mean the



Example of build to line and setback

length of the combined frontage of front property lines.

- (i) A portion of the building facade shall be located on the designated build-to line for at least thirty-three percent (33%) of the property frontage; and
- (ii) An additional portion of the building facade shall be located on or within eight feet (8') of the designated build-to line for a distance of at least thirty-three percent (33%) of the property frontage.

Note: A building facade that is substantially built to the required line or zone, allowing for typical architectural variation in the wall plane, shall be considered to meet the build-to requirement.

- Where so designated by a build-to zone, the building façade shall be located anywhere within the zone. Parking is allowed within the setback area but shall be located a minimum of 20' from the property line.
- Where so designated by a setback, the building façade shall be located no closer to the property line than the setback allows.

4.1.4 Grading

Concept:

- To the extent practical and economically feasible, respect the natural variation in topography.
- Minimize the impact of walls to any public streets.

4.1.4.1 Standards

- 3:1 Maximum slope - retaining walls, terraced parking lots and stepped building pads shall be considered to avoid slopes steeper than 3:1

4.1.5 Parking

Concept:

- Minimize the visual impact of the automobile on public spaces within RidgeGate.

4.1.5.1 Standards

- Parking structures facing pedestrian-oriented streets must incorporate elements at all levels to conceal views of parked cars and are encouraged to

incorporate active uses, such as commercial uses, at street level edges.

- Parking structures shall conform to building Standards and Guidelines pertaining to massing, scale and materials.
- Parking edges must be defined and enhanced with low walls or berming and landscaping.
- Bicycle parking to be provided within buildings or parking areas as appropriate to the use.
- Pavement grades shall not exceed 5% longitudinal slope within parking areas and an 8% longitudinal slope in drive aisles that do not have parking stalls along the aisles.
- Within private development, provide preferential parking spaces for high efficiency vehicles, car pools, car sharing and ride sharing to reduce vehicle parking demand in office developments.

4.1.5.2 Guidelines

- “Bioswale” landscaped parking islands to filter and reduce stormwater runoff in parking areas are encouraged.

4.1.6 Service Areas

Concept:

- Reduce the visual and functional impact of service areas on public spaces within RidgeGate.

4.1.6.1 Standards

- Service and delivery activities shall be separated from the primary public access and screened from public view by means such as:
 - locating underground
 - locating internal to structures
 - providing masonry walls, fences and/or landscaping of sufficient height and density.
- Such areas shall not be visible from streets, other public areas, or adjacent properties. Roofs shall be required if residential uses look down on the area.
- Adequate loading and maneuvering space shall be provided for trucks and other service vehicles.



Parking garage character example



Parking lot example



Service area/screening examples



Building massing/scale example



Building massing/scale example



Building massing/scale example

4.2 Architecture

4.2.1 Massing and Scale

Concept:

- Provide an organized variety of building massing, architectural form and detail with human-scaled building components at ground level.

4.2.1.1 Standards

- Each building facade oriented to a public street or open space shall incorporate architectural scaling patterns that include three or more of the following elements:
 - Expressions of building structural elements such as floors (banding, belt courses, etc. not less than 1' deep and 4" wide), columns (pilasters, piers, quoins, etc. not less than 4" deep and 1' wide), and foundations (watertables, rustication);
 - Patterns of window and door openings that are emphasized through change of plane (not less than 4" deep), and/or the use of sills, lintels, mullions, muntins, and other scale providing elements;
 - Changes in material;
 - Changes in color;
 - Changes in texture;
 - Changes in material module or pattern;
 - Patterns of architectural ornament integral to the building materials.
- Each change of material shall involve at least 1" variation in wall plane. Reveals shall be not less than 1" deep and 1" wide.
- The RidgeGate PDD identifies view corridors and heights limitations that must be preserved and respected, as appropriate.

4.2.1.2 Guidelines

- Visible joint patterns and projected sills are encouraged.

4.2.2 Four Sided Fenestration

Concept:

- Provide a high degree of transparency at the lower levels of building façades.
- Insure the visibility of active pedestrian uses.

- Establish a pattern of individual windows at upper floors that provides a greater variety of scale through fenestration patterns, material variation, detail, and surface relief.

4.2.2.1 Standards

- No highly reflective glazing shall be permitted on the building facade (maximum reflectance factor of .20).
- Primary entrances into buildings shall be easily identified through design elements and detailing.



Building fenestration example

4.2.3 Roofs

Concept:

- Buildings must integrate all building systems within a complete architectural form.
- Respect the character of and views from the surrounding context.

4.2.3.1 Standards

- All rooftop mechanical systems shall be incorporated into the building form in a manner integral to the building architecture in terms of form and material. All mechanical, electrical and telecommunications systems shall be screened from view of surrounding streets and structures.
- Parapet roofs must be articulated to avoid a continuous warehouse-like form.

4.2.3.2 Guidelines

- Three-dimensional rooftops are encouraged.
- Darker colors are permitted for flat roofs.



Roof treatment examples

4.2.4 Materials and Color

Concept:

- Encourage human scaled buildings through the use of smaller material modules.
- Consistent use of quality materials appropriate to the urban environment.

4.2.4.1 Standards

- All façades facing public streets or open space shall be constructed on high quality materials including , without limitation,the following,
 - Masonry, including stone, brick, terra cotta, architectural precast concrete, cast stone and pre-fabricated brick panels.

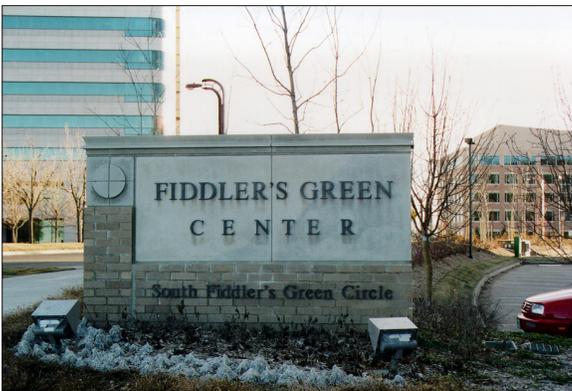




Building entry example



Signage example



Signage example

- Architectural metals, including metal panel systems, metal sheets with expressed seams, metal framing systems, or cut, stamped or cast, ornamental metal panels.
- Glass and glass block.
- Glazing systems shall typically utilize framing and mullion systems that stand out from the surface of the glass to provide scale and surface relief.
- In general, subdued colors typical of the muted native grasses, rocks and soil of the high Colorado plains and the natural setting are to be used as the primary colors.
- Intense colors are limited to accents only.
- The following materials are disallowed unless approved by variance:
 - Reflective glass curtain wall systems
 - Unfinished CMU Block
 - Cementitious siding panels

4.2.4.2 Guidelines

- Building materials should be selected with the objectives of quality and durability.

4.2.5 Signage

Concept:

- Create an organized and interrelated system of quality signs, sign structures, lighting and graphics.
- Create signs and graphic elements that are related to and/or respect the architecture of the building which they serve.
- Encourage a variety of signs and graphic elements that are compatible with the character of the surrounding district.

4.2.5.1 Standards

- Each building project must prepare a comprehensive signage plan defining locations, sizes, and types of information, identity and directional signage.
- Building mounted signs must be limited to the individual letters and logo of a single company name and shall be internally lighted. Rooftop signs are prohibited.
- Individual development project monument signs must not exceed 30 square feet in area.
- Pole mounted signs are prohibited.
- Strobe, flashing, blinking and portable signs are not allowed.

- Use of neon lighted signs is allowable in certain, limited applications.
- Maximum height of any letter or symbol shall not exceed four feet (4').
- Individual development project identification signage must recognize the overall project name (RidgeGate).
- Reference the I-25 Corridor Signage plan (addendum 1.0 to the Office District SubArea Plan) shall apply to all monument signage along I-25, as amended.

4.2.5.2 Guidelines

- Signs should fit within the architectural features of the façade and compliment the building's architecture. Signs should not overlap and conceal architectural elements.
- Signs must compliment the public architectural themes and materials of the local community.

4.2.6 Lighting

Concept:

- Provide a functional attractive unified lighting system throughout the lot that contributes to a high quality design and a feeling of security for users.

4.2.6.1 Standards

- Each building project must prepare a comprehensive lighting plan defining levels, locations and types of illumination proposed for the following:
 - Road and Parking areas
 - Pedestrian areas
 - Special Features/Accents
- Lighting shall not exceed eight (8') foot-candles at any locations, when measured on the ground, with certain, limited, exceptions such as under service station canopies or at building entrances.
- The maximum heights of light fixtures shall be thirty feet (30') above the ground.
- Metal halide, LED or other "white" lighting sources are required.
- Fixtures must complement those selected for the overall project streetscapes within the RidgeGate West Village.



Landscaping examples



Landscaping example



Landscaping example

4.3 Landscape

4.3.1 Landscape Character Zones

Concept:

- Establish distinct character and use zones that create identity, add value and limit high water use plant materials to high activity areas.

4.3.1.1 Standards

- A majority of the plant materials shall be of an indigenous or low water use plant palette.
- Small scale, high activity human use areas may utilize more water intensive plant palettes
- Minimum plant sizes are as follows:
 - Trees: Minimum 3" caliper (evergreens: 9' height - limited to 25% of total number of trees).
 - Shrubs: Minimum 1 gallon - limited to 25% of total number of shrubs.

4.3.1.2 Guidelines

- The DRC will adopt recommended plant lists.

4.3.2 Irrigation

Concept:

- Support the conservation of water resources

4.3.2.1 Design Standards

- A majority of landscaped areas must utilize drip or other low-volume application systems controlled by automatic computerized clocks.

4.3.3 Fences and Walls

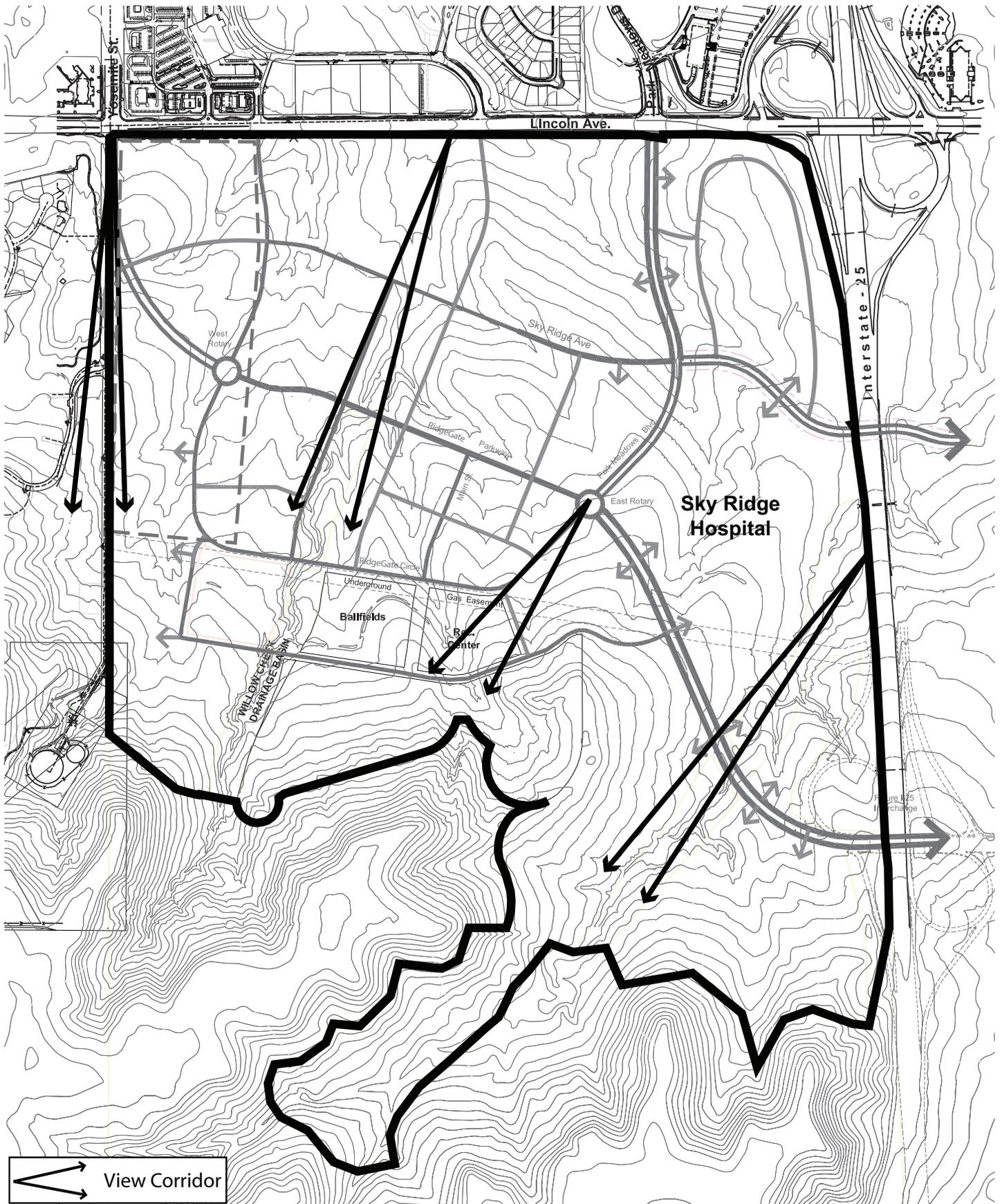
Concept:

- Minimize fences and walls in front and side yard areas or where visible from public streets and open spaces.

4.3.3.1 Design Standards

- No free-standing walls or fences in front yards over three feet (3') in height are allowed.
- Wall materials must directly relate to building materials.
- No permanent chain link fences are allowed.

+05/ Appendix



Note: The Sub-Area District boundaries are conceptual and may be refined by subsequent Sub-Area Plans.

Exhibit 2.0: Section 15 RidgeGate West Village Boundary

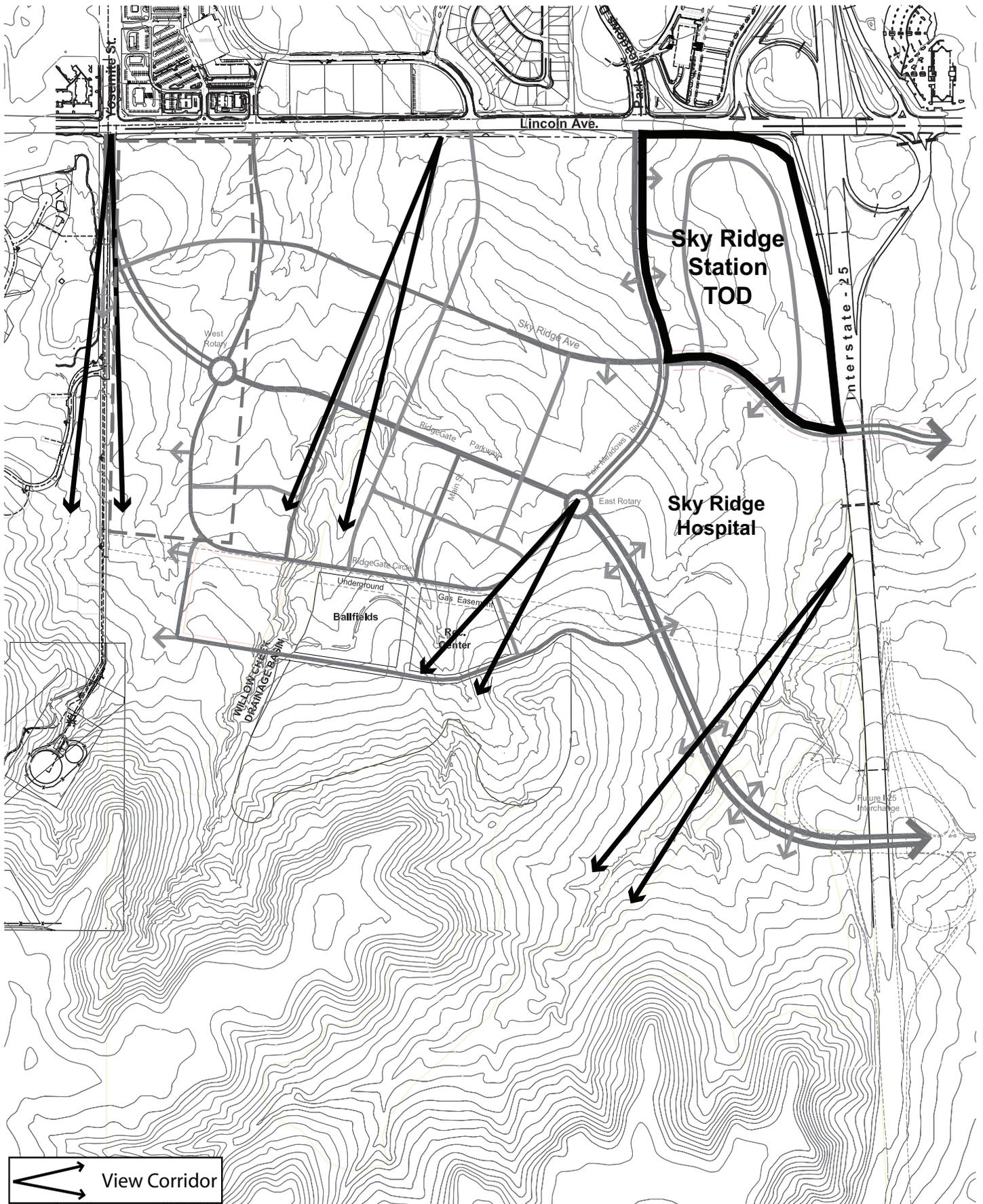
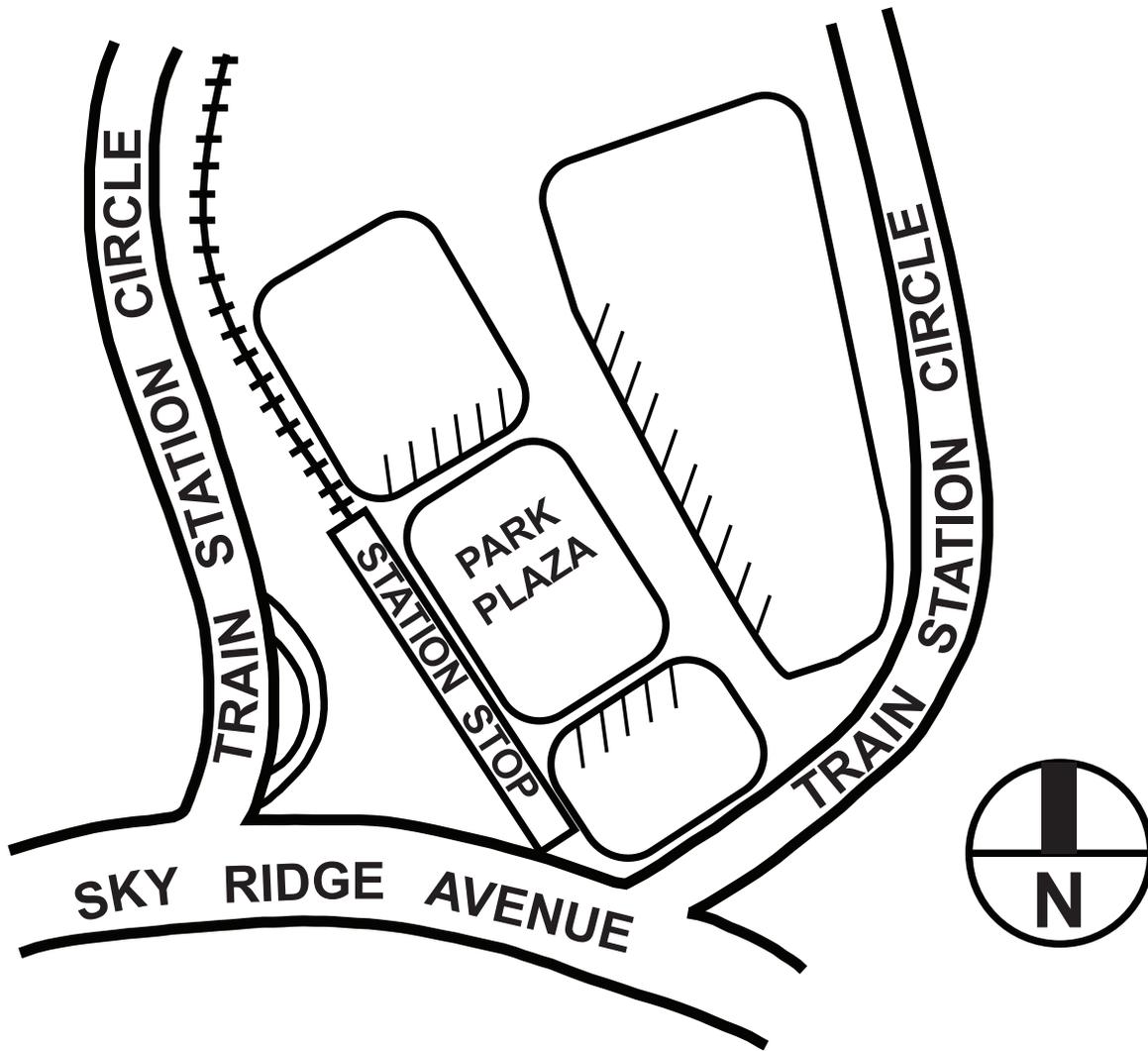


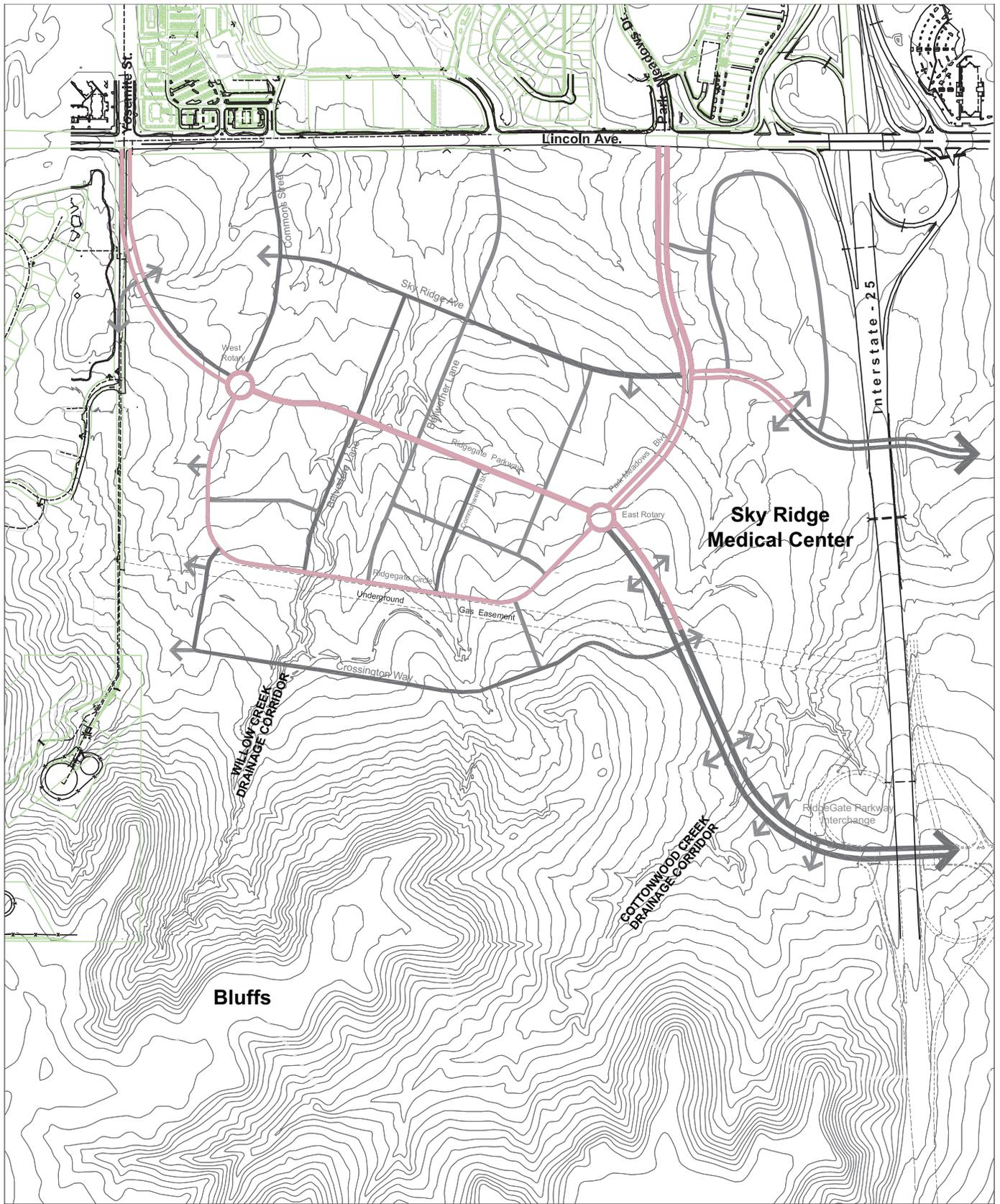
Exhibit 3.0: Sky Ridge Station TOD Sub-Area Plan Boundary



//// CORE COMMERCIAL

N.T.S.

Exhibit 4.0: Sky Ridge Station TOD Core Area



Phase One Roads —

Note: The Transportation Framework is conceptual. Final road alignments will be subject to refinement with Final Design Plans and Site Improvement Plans. Some Roads may be built in phased increments based on capacity needs.

Exhibit 5.0: Section 15 Transportation Framework Plan

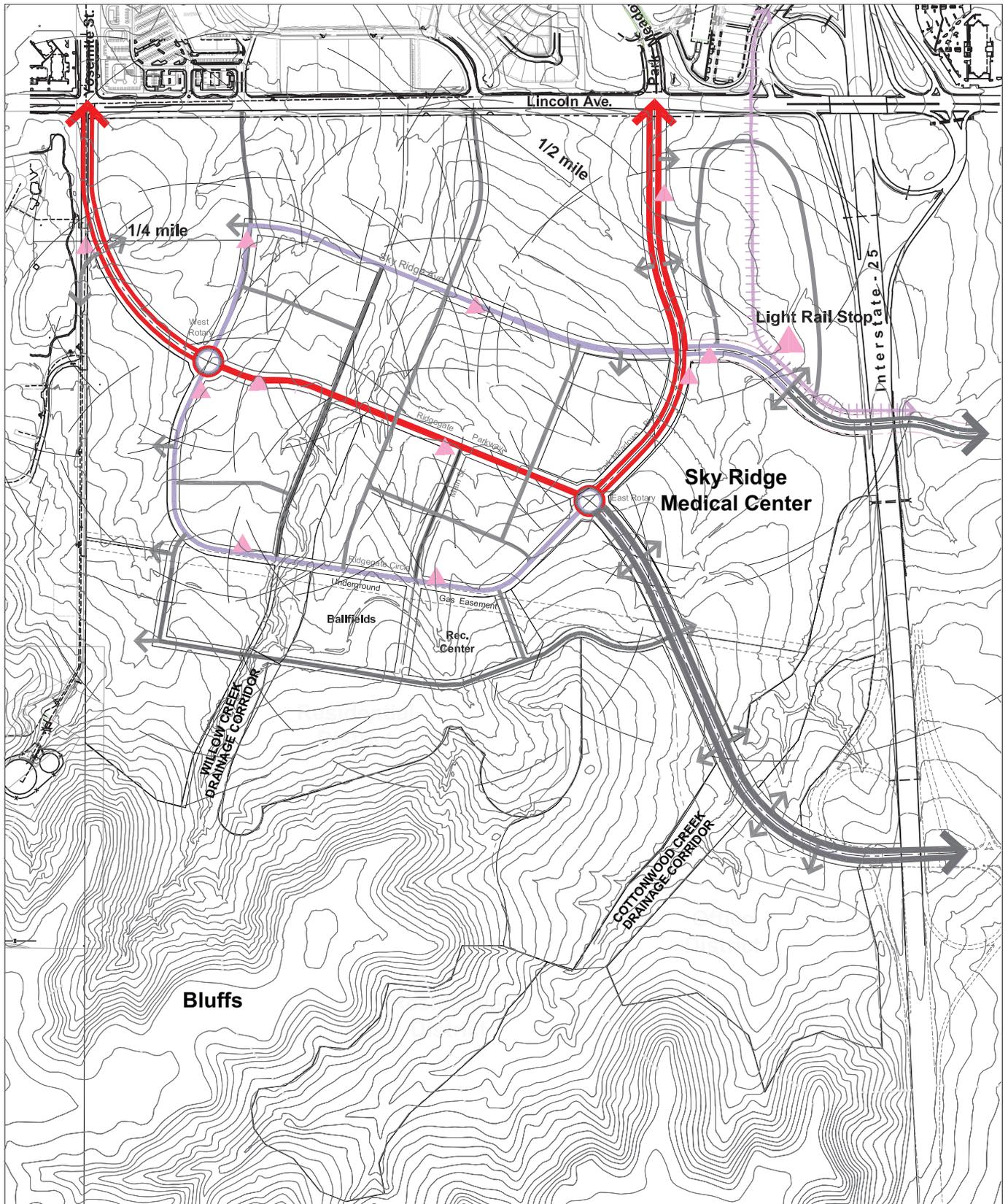
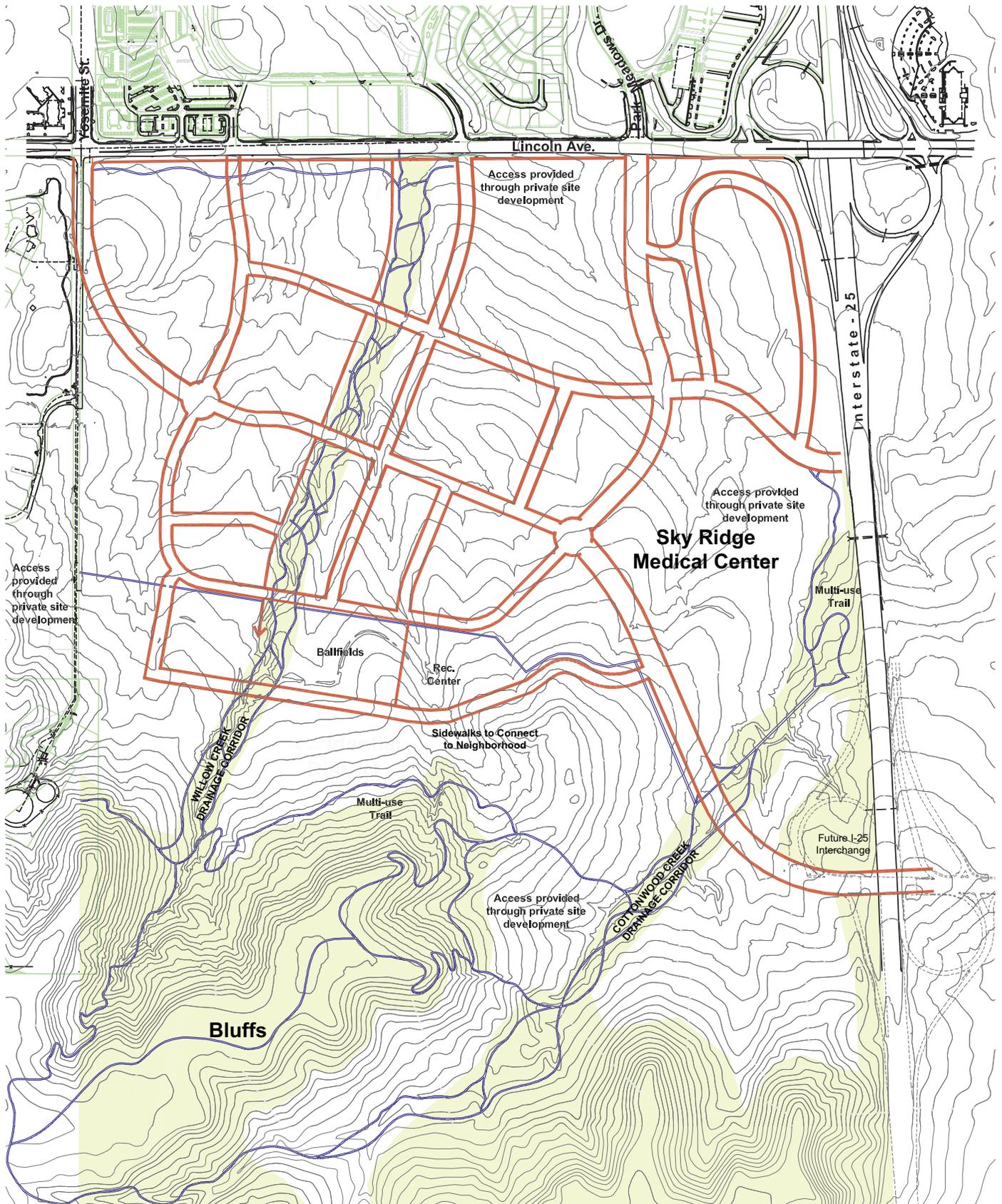


Exhibit 6.0: Public Transportation Framework Plan



Multi-use Trail ————
 Sidewalk ————

Note: The Pedestrian and Bike Access Framework is conceptual and may be refined with Final Road Design Plans.
 N.T.S.

Exhibit 7.0: Section 15 Pedestrian and Bike Access Framework Plan

