



Ridge  
Gate  
EAST

Sub-Area Plan for the East Villages

February 6, 2018





**PROJECT TEAM:**

**Coventry Development Corporation**



**City of Lone Tree, Colorado**



**DTJ Design, Inc.**



**Merrick & Company**



**Felsberg Holt & Ullevig**

The following text, photographs and illustrations contained within this document are a compilation of the project team's work, as well as photographs produced by others for the sole purpose of communicating a vision and design concept for the community of RidgeGate East.

The Project Team is committed to the environment and hopes that all who read this document are committed as well. Please be mindful of the environment and view this document in a paperless digital format whenever possible.



McKAY  
LANDING

# TABLE OF CONTENTS

## PART ONE

### Introduction

<b>1.1 RidgeGate Planning Concept</b>	<b>3</b>
<b>1.2 Purpose of Sub-Area Plans</b>	<b>3</b>
<b>1.3 Development Plan Submission Requirements</b>	<b>3</b>
<b>1.4 Public Participation Process</b>	<b>4</b>
<b>1.5 RidgeGate EAST Technical Supplement</b>	<b>4</b>

## PART TWO

### Framework Elements

<b>2.1 Vision</b>	<b>7</b>
<b>2.2 Guiding Principles</b>	<b>7</b>
<b>2.3 Subsequent/Supplemental Plans and Programs to be Developed</b>	<b>8</b>
<b>2.4 Schweiger Ranch</b>	<b>8</b>
<b>2.5 Village Planning Concepts</b>	<b>9</b>
<b>2.6 Land Use and Density Framework Plan</b>	<b>9</b>
<b>2.7 Parks and Open Space Framework Plan</b>	<b>23</b>
<b>2.8 Community/Public Facility Framework Plan</b>	<b>27</b>
<b>2.9 Circulation/Transportation Framework Plan</b>	<b>29</b>

## PART THREE

### Guidelines: Residential/Mixed-Use

<b>Residential/Mixed-Use Guidelines Checklist</b>	<b>33</b>
<b>3.1 Introduction</b>	<b>34</b>
<b>3.2 How to Use these Guidelines</b>	<b>35</b>
<b>3.3 Site Planning Guidelines</b>	<b>36</b>
<b>3.4 Architecture Guidelines</b>	<b>42</b>
<b>3.5 Landscape Guidelines</b>	<b>47</b>

## PART FOUR

### Guidelines: Commercial Mixed-Use

<b>Commercial/Mixed-Use Guidelines Checklist</b>	<b>51</b>
<b>4.1 Introduction</b>	<b>52</b>
<b>4.2 How to Use these Guidelines</b>	<b>53</b>
<b>4.3 Commercial/Mixed-Use Areas</b>	<b>53</b>
<b>4.4 Site Planning Guidelines</b>	<b>54</b>
<b>4.5 Architecture Guidelines</b>	<b>63</b>
<b>4.6 Landscape Guidelines</b>	<b>68</b>

## PART FIVE

### Miscellaneous Guidelines/Standards

<b>5.1 Attainable Housing Units (AHU)</b>	<b>73</b>
<b>5.2 Universal Design</b>	<b>73</b>
<b>5.3 Centennial Airport Noise and Height Requirements</b>	<b>73</b>
<b>5.4 Crime Prevention Through Environment Design</b>	<b>74</b>
<b>5.5 Accessory Dwelling Units (ADU)</b>	<b>75</b>

### Appendix

<b>Glossary</b>	<b>79</b>
<b>Circulation/Transportation Framework Plan</b>	<b>81</b>
<b>Street Types and Character</b>	<b>82</b>
<b>Other Resources</b>	<b>87</b>





PART ONE

INTRODUCTION

# INTRODUCTION

## 1.1 RidgeGate Planning Concept

RidgeGate consists of approximately 3,500 acres located in the City of Lone Tree, Colorado. It was annexed into the City in 2000 with the shared vision and commitment to accommodate growth in a walkable, mixed-use environment. RidgeGate's position at metro Denver's southern gateway, combined with its adjacency and access to the I-25 highway corridor and The Regional Transportation District's (RTD) Southeast Light Rail Line includes unique circumstances and opportunities.

RidgeGate Parkway and Lincoln Avenue connect the City Center to the RidgeGate East Residential and Mixed-Use Villages. The use of the drainage corridors serve as "green gateways" to connect people with nature.

Planning Areas delineated in the RidgeGate Planned Development District (PDD), have distinct edges defined by open space, which protect sensitive watersheds, native vegetation, and wildlife habitats. The open space network also provides for an integrated trail system connecting people with nature.

3

The portion of Ridgegate east of I-25 includes both the City Center and the RidgeGate East Villages Sub-Area Plans. As an extension of the City Center to the east, the RidgeGate East Villages benefit from the adjacency to I-25 and the two Southeast Line light rail stations. In response, the portion of the Villages near the transportation facilities are characterized by higher density housing that supports a walkable community and connectivity between the City Center and the Villages and to the light rail stations.

The Villages consist of approximately 1,840 acres bounded by Lincoln Avenue to the north, residential neighborhoods to the east, natural land forms and open space areas to the south, and the Happy Canyon Creek drainageway to the west. This area provides for the expansion of the RidgeGate community and will contain employment, retail, living and civic environments.

## 1.2 Purpose of Sub-Area Plans

As required by the PDD, individual Sub-Area Plans are to be developed and approved prior to or concurrent with platting. These Sub-Area Plans refine Planned Development District Planning Areas and add supplemental development framework and character guidelines while allowing for adaptation to market conditions and demonstrating realistic planning concepts. This document establishes criteria by which the City of Lone Tree will evaluate future Plats and Site Improvement Plans (SIPs) for developments to occur in RidgeGate East Villages.

In most instances, a variation from this Plan should be evaluated based upon two conditions: (1) a development proposal supports the vision for the RidgeGate EAST Villages and the intent communicated throughout this document; and (2) the variation from the Plan would not have a significantly detrimental effect on the ability to realize the vision and framework elements set forth in this document, albeit it in a different way than may be depicted in the Plan.

Variations will be evaluated through the Site Improvement Plan or Plat approval process. The City of Lone Tree Community Development Director, in consultation with the City Manager, will determine if an update to the Sub-Area Plan is required. All Sub-Area Plan amendments will be reviewed by the planning Commission and acted on by the City Council.

## 1.3 Development Plan Submission Requirements

### 1.3.1. Overview

Prior to the City process, any project within RidgeGate must go through the RidgeGate Design Review Committee (DRC) for review and approval. The DRC must approve the plans prior to proceeding through the Lone Tree planning process. The City of Lone Tree requires plans for development to go through staff review followed by a public review by the Planning Commission, and final approval by the City Council.

**Aerial  
photo of  
RidgeGate  
West  
Colorado  
Sky Shots  
(2017)**



### **1.3.2. Relationship between the Design Review Committee & the City of Lone Tree**

Prior to this City process, any project within RidgeGate must go through the RidgeGate Design Review Committee (DRC) for review and approval. The DRC must approve the plans prior to proceeding through the Lone Tree development review process. The City of Lone Tree requires plans for development to go through staff review followed by a public review by the City Planning Commission, and final approval by the City Council.

### **1.3.3. Relationship to City Regulations and Other Municipal Codes**

Where provisions of this Sub Area Plan do not address a particular subject, the relevant provisions of the City of Lone Tree Zoning Code, as amended, or any other applicable ordinances, standards and guidelines, or regulations of the City of Lone Tree shall be applicable.

### **1.4 Public Participation Process**

Two public open houses, a Planning Commission, and a City Council meeting were conducted in the fall of 2017 to share information about the proposed plan and to gather community comments. These open houses were attended by a total of approximately 50 members of the public.

## **1.5 RidgeGate EAST Technical Supplement**

Separate from the Plan, Lone Tree and RidgeGate have developed and will maintain and update the RidgeGate East Technical Supplement. This document contains technical information in support of the Plan including master plans for utility infrastructure systems, master traffic studies, and technical street standards for execution by the Rampart Range Metropolitan District (RRMD). The Technical Supplement is initially approved by the City Council. Updates and changes to the Supplement may be administratively approved by the City.



## **RIDGEGATE PLANNED DEVELOPMENT**

The largest area of growth in the southeast Denver region over the next few decades will be the master-planned community known as RidgeGate. It consists of approximately 3,500 acres located south of Lincoln Avenue, and both east and west of Interstate 25. A development plan created the vision of RidgeGate as an urban, mixed-use environment that will offer people a variety of

residential, employment, shopping and dining options. The community is currently home to the Sky Ridge Medical Center, the Charles Schwab Campus, the Lone Tree Recreation Center, the Lone Tree Arts Center, and a diversity of residential neighborhoods. RidgeGate will also be home to the future Lone Tree City Center just south of Lincoln Avenue and east of Interstate 25.



An aerial, high-angle photograph of a rooftop terrace. The scene is filled with people sitting at tables, some under large patio umbrellas. A central fire pit is visible, surrounded by people. The terrace is furnished with tables, chairs, and umbrellas, creating a social and outdoor dining environment. The overall tone is warm and inviting, with a focus on the layout and elements of the space.

PART TWO

# FRAMEWORK ELEMENTS

# VISION AND GUIDING PRINCIPLES

## 2.1 Vision

In keeping with the original community vision for RidgeGate as a “City in the Landscape”, this plan presents guiding principles to further the vision and create a community that directly relates to the dynamic landscape that exists on the east side of I-25. A modified urban grid has been carefully integrated with the rolling terrain, utilizing the existing natural land forms to define neighborhoods with a mix of compatible uses which provide strong pedestrian connections throughout the community. A central open space amenity within walking distance of all neighborhoods is at the heart of each Village. Modifying the urban form into landscape-focused Villages creates a dynamic and interconnected system of circulation elements between different destinations and uses. This balanced mix of land uses layered into the community fabric allows for the preservation and protection of the sensitive natural drainage corridors and habitat areas.

### THE VISION FOR RIDGEGATE EAST Villages

The RidgeGate East Villages is envisioned as a “City in the Landscape” with a balanced mix of uses carefully integrated with the site, utilizing the existing natural land forms to help shape three distinct and definable Villages and two commercial, mixed-use Districts. A central open space is carefully located at the heart of each Village within walking distance of all residences. An interconnected network of streets, sidewalks and paths link the Villages to major destinations, uses and public amenities. This balanced mix of uses layered into the community fabric ensures the preservation and protection of sensitive natural areas.

## 2.2 Guiding Principles

There are five key principles that will guide development of the community and provide overall direction to implement the vision.

### Modified Urban Grid

Develop an interconnected system of circulation elements between different destinations and uses to help distribute traffic and minimize congestion.

- Minimize short loops or cul-de-sacs.
- Provide collector-type roads that link each neighborhood or Village to regional arterials.
- Design roads to primarily follow greenways and connect parks and open space.
- Provide secondary open space at the center of each Village.

### Balanced Mix of Uses

Emphasize pedestrian walkability and connections by providing a variety of mixed-use opportunities within neighborhoods and within a quarter mile walking distance of a central amenity or public space.

- Provide pedestrian connections that link a variety of land uses.
- Use ground floor retail to activate the street.
- Allow housing and office uses above ground floor retail/commercial in key areas.
- Vary architectural character along the street for visual interest and hierarchy.
- Provide a diversity of uses or housing types that encourages walking.

### Open Space System

Create a planning framework with distinct edges defined by an open space system to protect sensitive natural areas to create a “City in the Landscape.” The open space system will include protecting the bluffs and providing an integrated trail network that connects to the City Center and the surrounding region.

- Protect existing natural features through sensitive planning land planning integration.
- Provide multiple connections within the community to regional and community open space and trails.
- Provide small neighborhood parks within easy walking distance of residents.

**Modified  
Urban Grid**



**Balanced  
Mix of Uses**



**Integrated  
Open Space  
System**



**Sustainable  
and Innovative  
Strategies**



**Integrated  
Community  
Centers and  
Community  
Programs**



**Parks and Recreation**

Local, neighborhood parks in the East Villages will be part of a comprehensive, planned system of parks integrated with the surrounding regional parks, trails and open space network. District park plans will support an active and healthy community by contributing to a safe, inclusive, connected and sustainable system of parks and recreation facilities where people of all ages and abilities can play, exercise, learn, relax and interact with art, culture and nature.

**Sustainability & Innovation**

Concentrate development into mixed-use neighborhoods to help reduce the cost of infrastructure, services and transportation, and preserve and integrate sensitive riparian habitats into the community design.

- Prioritize human health and the environment.
- Reduce transportation demand through connectivity.
- Minimize stormwater impacts.
- Protect natural and wildlife habitat.
- Integrate "Smart Cities" technologies that connect to the greater metropolitan region.

**2.3 Subsequent/Supplemental Plans and Programs to be Developed**

This Sub-Area Plan identifies several Plans and Programs to be developed in the future for approval by the City of Lone Tree including but not limited to the following:

- Attainable Housing Plan
- Comprehensive Wayfinding Signage Program
- Master Park Plan for each of the three Villages and two commercial Districts (Central Village Couplet District and Lincoln Avenue District).

**2.4 Schweiger Ranch**

The 38-acres of Schweiger Ranch are owned and operated by the 501(c)3 Schweiger Ranch Foundation. Schweiger Ranch will submit a Sub-Area Plan to guide the development of any future additional buildings on its property.

# VILLAGES PLANNING CONCEPTS

## 2.5 Village Planning Concepts

The overarching concept for RidgeGate East is to create a series of walkable Villages, each with its own unique identity and sense of place.

Three distinct neighborhood Villages and two commercial, mixed-use Districts establish the basis for the land uses defined for the RidgeGate East Sub-Area Plan. The core of each Village will provide opportunities for higher density “urban” forms, scale and character, that will transition to less dense forms and scales and more informal patterns, reflective of the natural site that defines the edges of each Village. The planning concept follows a more organic development pattern of streets and lots for low to mid density residential housing.

# LAND USE/DENSITY

## 2.6 Land Use and Density Framework Plan

The land use and density plan supports a flexible balance and relationship between retail, office, and residential that, when synergistically located within each of the Villages, creates a platform for interaction between land use, open space and other community facilities and amenities.

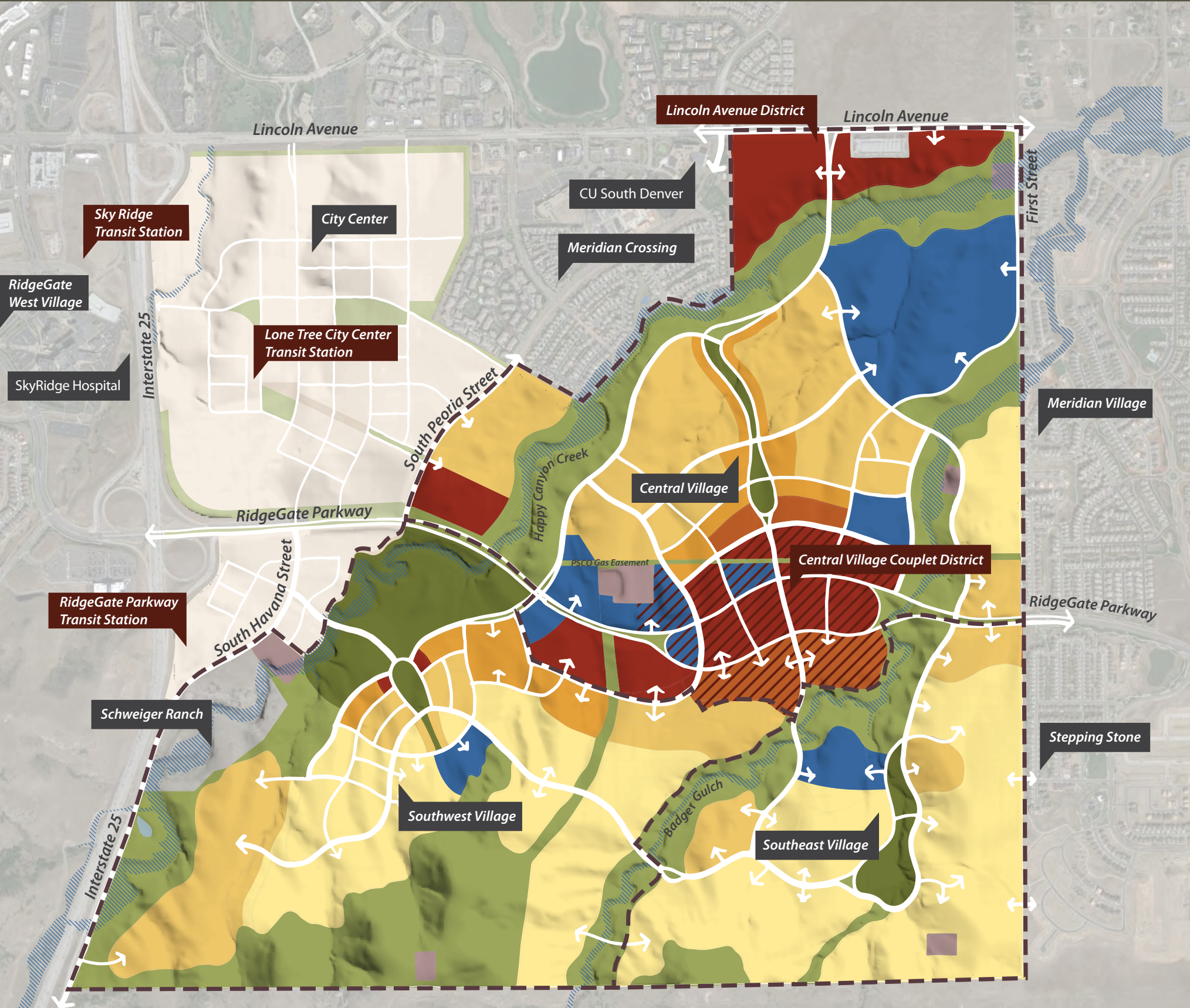
A variety of housing types and price ranges will result in a more stable community both economically and socially. The higher densities support activity centers that make for more vital neighborhoods and supports different lifestyle choices allowing for a diverse range of renters and buyers.

The commercial uses will include a wide variety of commercial, office and institutional buildings from single and multi-story stand alone in-line shops to large format building types.



Village Planning Concept

# LAND USE AND DENSITY FRAMEWORK PLAN



## Land Use and Density Framework Plan Legend

- |                               |                                   |                                  |                                |                          |                                   |                                  |
|-------------------------------|-----------------------------------|----------------------------------|--------------------------------|--------------------------|-----------------------------------|----------------------------------|
| Low Density Res.<br>3-6 du/ac | Medium Density Res.<br>5-12 du/ac | High Density Res.<br>12-25 du/ac | Multi-Family Res.<br>20+ du/ac | Mixed-Use/<br>Commercial | Institutional/<br>Public Facility | Central Village<br>M/U Core Area |
| Open Space                    | Regional/Village Park             | Utility/<br>Infrastructure       | City Center                    | 100-Year<br>Floodway     | Roadway<br>Network                | Parcel Access/<br>Connection     |

## 2.6.1. Central Village Concept/Character

The RidgeGate Central Village is the “heart” of the community planning framework for RidgeGate East Villages and is comprised of a commercial core that flanks RidgeGate Parkway and that serves the surrounding neighborhoods. RidgeGate Parkway is divided into two, one-way streets (or couplet street)

after crossing Happy Canyon Creek. This creates a park-like drive, with narrower street widths that encourage slower traffic and a pedestrian-friendly experience in the Central Village Couplet District. This Couplet District encourages a mixed-use environment that fronts the street edge to create a comfortable pedestrian-scale environment.

- 1 Incorporate public spaces that create opportunities for plazas and outdoor gatherings**



- 4 Create a Village green and central gathering place for neighbors north and south of the Central Village**

- 2 Connect greenways and neighborhoods creating a walkable Main Street/central spine**



- 5 Utilize the existing gas easement as a pedestrian promenade to connect Central Village to City Center**

- 3 Couplet street to allow for reduced scale and walkable community**



- 6 Encourage multifamily as a transition from commercial to lower density residential**





Note: This illustrative image is conceptual and is subject to change.

# CENTRAL VILLAGE COUplet DISTRICT



Central Village  
Couplet District  
Perspective  
Rendering

## RIDGEGATE EAST CENTRAL VILLAGE "COUplet DISTRICT"

Located on an existing ridge positioned between two greenways, Happy Canyon Creek and Badger Gulch, the RidgeGate East Central Village is the natural and ideal location for the heart of the residential community outside of the Lone Tree City Center. With RidgeGate Parkway serving as the main east-west thoroughfare it was essential to not allow it to become a physical barrier dividing the

community. In order to do so, it will be split into two one-way streets forming a couplet and bisected with a 'main street' that connects the northern and southern Villages of the community. At their intersections will be an eclectic mix of residential and commercial uses all situated within a walkable Village that will be a hub of activity and a desirable place to work and live.

**1** Locate an iconic building at the split of the couplet to signify arrival to the Couplet District



**2** Utilize continuous building massing to frame the main street and hide service areas for large scale retail/commercial uses



**3** Integrate a public plaza along main street as a central community gathering space and location for special events



**4** Terminate main street (north) at a Village green framed by streetscape and adjacent buildings



**5** Terminate main street (south) at an open space park with views and amenities that celebrate the greenway



**6** Locate amenity buildings adjacent to greenway edge for whole community enjoyment and access to the trail network



## 2.6.2. Lincoln Avenue District Concept/Character

Lincoln Avenue District is located at the northern gateway of the Central Village, and is planned for a wide range of commercial, employment, residential and institutional-type uses oriented towards the Lincoln Avenue corridor. The concept for the area is characterized by single and multi-story buildings, oriented towards Lincoln Avenue, and designed to accommodate a variety of retail and commercial uses for shopping, dining and general services. It is intended that the character will complement the Central Village and serve as a northern gateway to the overall RidgeGate East Villages.

The western portion of the Lincoln Avenue District has the potential to serve as a future extension/expansion of the CU South Denver Campus currently located directly to the east. The mixed-use environment will energize the other Villages by providing easy access to commercial and educational opportunities for residents and the surrounding region.

An important natural feature of this area is the Happy Canyon Creek, which borders the commercial area to the south. This drainage corridor is envisioned as part of a comprehensive open space and trail network for the project that will connect to the rest of RidgeGate East.

**1 Create a strong gateway element from Lincoln Avenue into RidgeGate East**



**4 Provide open space and trail connectivity to larger community along Happy Canyon Creek**



15

**2 Provide opportunities for education and employment**



**5 Create multi-tenant buildings with identifiable architectural style**



**3 Encourage multifamily housing adjacent to regional open space and trails**



**6 Incorporate anchor retail stores within the commercial shopping areas**





Note: This illustrative image is conceptual and is subject to change.

**7** Incorporate outdoor gathering areas into/ around commercial buildings



**8** Extend common areas into Happy Canyon Creek

### 2.6.3. Southwest Village Concept/Character

The Southwest Village is characterized by the Happy Canyon Creek drainage corridor which forms the western edge of the Village and is celebrated as a natural amenity that inspires the Village character and charm. The planning concept for the Village

reflects a development pattern, density and land uses that are compatible with the site's natural features and topography. The site naturally transitions from relatively flat ground on the north to relatively steep and rolling terrain to the south, approaching the bluffs where the grades exceed 20 percent.

**1** Create opportunities for small-scale pocket parks to foster social relationships, and neighborhood identity



**4** Incorporate a school within the neighborhood fabric to foster neighborhood identity

**2** Offer a mix of rear-loaded townhomes that define street-edge and provide transition in product types



**5** Integrate natural greenways into the neighborhood fabric

17

**3** Create a central "heart" defined by street-edge and building wall fronting the green



**6** Locate main entries/roadways along open space greenways to create sense of place and arrival



Note: This illustrative image is conceptual and is subject to change.

## 2.6.4. Southeast Village Concept/Character

The Southeast Village is defined by Badger Gulch that bisects the neighborhood Village. This regional open space corridor serves as the gateway into this Village from RidgeGate Parkway and provides an opportunity to connect with nature. The site topography consists of rolling terrain which makes this Village distinctive.

The sensitive integration of the neighborhood with the landscape can be achieved through the combination of a modified grid and cul-de-sacs. The use of cul-de-sacs would be utilized only in the case where the topography would preclude a through connection or require significant cut or fill to preserve the distinct landscape character within this neighborhood. Where possible, provide open-ended cul-de-sacs, to provide access to open space, trail connections and view corridors.

**1 Utilize natural open space as a gateway into the Village and neighborhoods**



**4 Incorporate thematic structures at key junctions along open space greenways**

19 **2 Create a blend of attached and detached homes to provide variety along the streetscape**



**5 Create a central park that reflects the site's natural setting**

**3 Orient houses onto roadways that front open space/greenways**



**6 Cluster homes around cul-de-sacs or courts in response to site topography**





Note: This illustrative image is conceptual and is subject to change.

### 2.6.5. Residential Product Diversity

The following comprises the range and scale of conceptual residential product diversity anticipated for RidgeGate East:

#### 1. *Low Density Residential (3 - 6 du/ac)*

- Single-family detached, conventional front-loaded and traditional rear-loaded products (5,000 - 10,000 sf lots)
- Single-family attached/duplex – both front-and rear-loaded products

#### 2. *Medium Density Residential (6 - 12 du/ac)*

- Single-family attached (2 and 3-plex buildings), townhomes (3-plex), both front-and rear-loaded products of 1 - 2 stories
- Small-lot cluster, single-family detached (2,800-3,500 sf), and attached product, cottages, and compact products, etc.

#### 3. *High Density Residential (12 - 25 du/ac)*

- Townhomes and Rowhouses, typically rear-loaded of 2 - 3 stories
- Condominiums, townhomes, and flat products of 2 - 3 stories

#### 4. *High Density/Multifamily Residential (20 - 70 du/ac)*

- For sale condos, and stacked flat products of 3 - 5 stories
- Tuck-under townhomes, 3 story structures, vertical mixed-use, and stacked flats over main-level commercial uses
- Product in this density may have structured parking structures wrapped with residential or active uses along the street to conceal the structure

### 2.6.6. Attainable Housing

Attainable Housing is intended to provide opportunities for people who work in the area to also live there, and to meet a need for affordable housing. Design of AHUs will be compatible with the character and context of the area in which they are located. The City and RidgeGate have agreed to AHUs as described in the Amended and Restated Annexation and Development Agreement with Respect to the East Side Property, and further details will be provided in a mutually agreed upon Attainable Housing Plan.



**Low Density Residential**



**Medium Density Residential**



**High Density Residential**



**Private Accessory Dwelling Unit**

**High Density/  
Multifamily  
Residential**



**Mixed-Use  
commercial/  
residential**



**Mixed-Use  
commercial /  
office**



**Mixed-Use  
retail/  
commercial**



**2.6.7. Commercial and Retail Product Diversity**

The following comprises the range and scale of conceptual commercial and retail product diversity anticipated for RidgeGate East:

1. Retail/Commercial Uses

- Includes large format and anchor retail stores, in-line retail stores, vertically-integrated uses, entertainment, service commercial, dining and restaurant uses.

2. Office Uses

- Includes corporate, professional, medical, office, and institutional uses.

3. Mixed-Use Project

- Includes retail/commercial on ground floor.
- Includes residential/office on upper floors.
- General building heights 3-6 stories.

4. PDD Allowances

- A maximum of 30 percent of the acreage of each Residential/Mixed-Use (R/MU) Planning Area can be developed with non-residential uses.
- A maximum of 40 percent of the acreage of each Commercial/Mixed-Use (C/MU) Planning Area can be developed with non-commercial uses.

## 2.7 Parks and Open Space Framework Plan

The regional trails and open space network preserves sensitive riparian and floodplain habitats, provides for integrated regional stormwater management, creates neighborhood edges, and connects Villages and individual neighborhoods with access to natural amenities. A majority of the system also provides passive recreational opportunities that incorporate East-West Regional Trail connections to the Reuter Hess Reservoir Recreation Area and buffers with native landscape character.

A Regional/Village Park will provide active and passive recreational uses for the larger RidgeGate and the City of Lone Tree Community. Local, neighborhood parks will be embedded into Villages, and Districts within Villages, to serve neighborhood recreational needs.

### 2.7.1. Public Park Concept

Over time and in alignment with the timing of development in the East Villages, a system of natural and developed parks will be implemented through collaboration between the City of Lone Tree, the South Suburban Park and Recreation District (SSPRD), the Rampart Range Metropolitan District (RRMD) and individual residential developers and their HOAs.

### 2.7.2. Park Master Plans

A Park Master Plan for each of the three Villages (Central, Southwest, and Southeast), and each of the two mixed-use Districts, shall be submitted for approval by the City prior to or concurrent with approval of the first residential plat within each of those three areas.

The minimum amount of local, neighborhood parkland within each area shall be based on 5 acres/1000 population projected within each area. Residential population multipliers shall generally be as follows: 2.8 people per dwelling unit for single-family detached, 2.0 people per dwelling unit for multifamily and attached dwelling units, and if attached/detached mix is unknown, 2.5 people per unit. Variations in population multipliers may be allowed by the City based on the specific circumstances of each project, and updated over time based on population and demographic data.



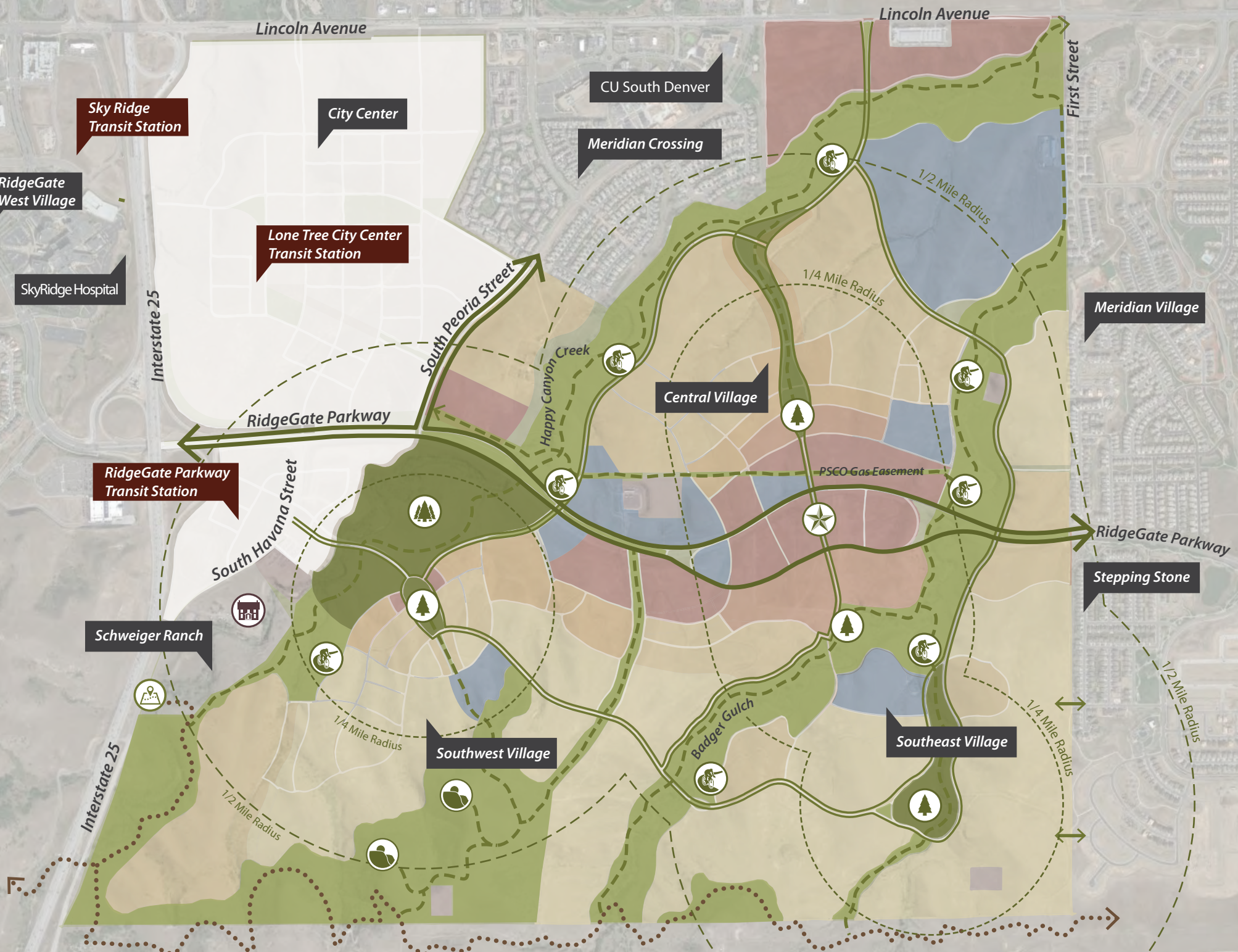
**Regional/  
Village Park  
example**



**Regional Trails  
and Open  
Space**



**Local/  
Neighborhood  
Park Concept**



## Parks and Open Space Framework Plan Legend

- 8' Pedestrian Walk and 12' Cycle Track
- 8' Pedestrian Walk
- Multi-Use Trail
- Potential Trail Connections
- East-West Regional Multi-Use Trail
- Schweiger Ranch
- Regional/Village Park
- Central Village Plaza
- Village Park
- Regional Trailhead & Parking
- Greenway Park & Trailhead
- Overlook Point

SCALE 0' 300' 600' 1200'

Neighborhood Park Dedication:  
5 acres per 1,000 population

As part of the 5 acres/1000 population requirement, there shall be at least one 5-10 acre park in each Village, with at least two of the Villages having a minimum 10 acre park that is consistent with South Suburban Park and Recreation District standards. These parks are intended to be larger to accommodate recreational needs of the east side community.

Park sites will receive full credit towards the total estimated park acreage requirement within each area, provided they are accessible to the general public, meet a demonstrated level of service need as determined by the City in consultation with South Suburban Park and Recreation District, and are suitable for development of improvements as intended.

The City may grant up to 25% credit toward the estimated neighborhood park acreage requirement within each Village or District for private amenities such as community pools, fitness and recreation facilities, landscaped courtyards, plazas, and similar private amenities, or when joint use of elementary school park facilities are readily available and open to the public for shared programming. Non-park landscaped areas owned and maintained by an HOA are ineligible in meeting the park requirement.

In limited cases such as isolated or independent multifamily or attached residential project Site Improvement Plans that cannot satisfy the local, neighborhood park requirement, cash-in-lieu of park land will be permitted by the City, based on the market value of the land acreage that would have been required, or as otherwise agreed to by the City. Cash-in-lieu shall be collected by the City, prior to issuance of a building permit. Funds collected shall be applied to the acquisition of public park land or for the improvement of the public park system on the East side.

#### **A. Park Master Plans Content**

Park Master Plans shall include, at minimum:

- Context map showing the larger system of existing and planned parks and open space within a one (1) mile radius around the Village or District.
- Concept plan of the entire Village or District, showing conceptual layout and uses of proposed development including roads, trails, open space and proposed park sites. Park sites should be labeled with approximate acreage.
- Projected population estimates and park land dedication requirement, based on formula in the Sub-Area Plan for the East Villages.
- Physical description of each planned park area, proposed uses, proposed entity responsible for maintenance, etc. The City may also require topo maps, drainage maps or other studies to confirm site suitability for proposed parks.
- Phasing/Triggers for implementation of the parks.
- Confirmation that necessary utilities are available for connection in adjacent streets.

25

**Strategically  
integrate  
parks into the  
community**



**Provide areas  
of Colorado  
native inspired  
landscapes for  
experiential  
play**

## **B. Park Master Plans Approval Process**

Parks Master Plans shall be submitted for City approval prior to or concurrent with the first residential plat within each of the three Villages and each of the two commercial Districts designated by this plan.

Parks Master Plans shall be accompanied by an application and fee in accordance with the City's Fee Schedule.

The Parks Master Plan will be reviewed for completeness by Community Development staff within 15 business days of receiving a complete submittal and any changes or requests for more information will be provided to the applicant. Plans shall be sent on a 21-day referral review period. Referral agencies shall include South Suburban Park and Recreation District, the Rampart Range Metropolitan Districts, Douglas County Division of Open Space and Natural Resources, and other agencies as determined by the Director.

Parks Master Plans are subject to review by the City's Recreation Advisory Committee, which will make a recommendation to the City Council for final approval.

## **C. General Park Standards**

Local, neighborhood parks will be subject to administrative City approval of a Site Improvement Plan, which will include review of the site layout, grading, lighting, landscaping, amenities, and structures if any. The administrative review process will include consultation with South Suburban Park and Recreation District, the Rampart Range Metropolitan Districts, and other agencies as determined by the Community Development Director.

Parks that will be maintained and operated by South Suburban Park and Recreation shall comply with that District's design requirements and Master Plan.

Pedestrian plazas, public gathering areas, and similar spaces are eligible to satisfy the parkland requirement.

Parks shall be designed to be safe and accessible for people of all ages and abilities.



**Locate residential/commercial uses that engage the regional open space**



**Reserve areas within community parks for education and discovery of the Colorado native landscape**



**Utilize innovative and naturalized storm water systems and trails within regional drainage corridors**



**Wherever possible, incorporate native landscape within urban environments**

## 2.8 Community/Public Facility Framework Plan

A variety of public facilities are planned for the RidgeGate East Villages to support and enhance public health and well-being, education and public safety. These community and public facilities are carefully located near the Central Village Couplet District, adjacent to RidgeGate Parkway and in close proximity to the major open space/trail network to encourage multi-modal accessibility.

Emergency facilities such as police and fire stations are located along Ridgegate Parkway for quick and easy access to the overall community. School sites are proposed within each Village and in close proximity to the major open space and trail networks. Alternative library sites have been proposed to provide flexibility in locating this important public amenity within the Couplet District.

A community-wide Recreation Center is also proposed at the western entry, adjacent to Ridgegate Parkway, Happy Canyon Creek and the Regional/Village Park, providing strong visibility and easy access to local residents and the surrounding area.

### 2.8.1. Public Land Dedication\*

#### 1. Schools

- A combined middle school/high school site
- Three elementary school sites (may be joint use with parks for partial park credit) as determined by Douglas County School District and the City of Lone Tree.
- The elementary school sites shown in the Sub-Area Plan are based on the conceptual plans and are subject to refinement as final plans/plats are developed.

#### 2. Fire Protection

- One site.

#### 3. City Police Station and Public Works Facility

- One co-located site.

#### 4. Library Site

- Three alternative sites.

#### 5. Recreation Center

- One site.

\* NOTE: Refer to Community/Public Facility Framework Plan (opposite page).



**Neighborhood Elementary School example**



**Community Recreation Center example**



**Library example**



**Public Works Facility example**



# COMMUNITY/PUBLIC FACILITY FRAMEWORK PLAN



**Community/Public Facility Framework Plan Legend**

Public Library (Potential Location)	Recreation Center	Schweiger Ranch
Fire Station	Combined Police and Public Works Facility	Middle/High School
		Elementary School

0' 300' 600' 1200'  
SCALE

## 2.9 Circulation/Transportation Framework Plan

The Circulation/Transportation Framework Plan will provide a strong interconnected network of streets, bikeways and sidewalks to create safe and convenient access throughout the Villages and to the surrounding area.

### 2.9.1. Roadway Phasing Concept

Several major roadways currently serve the site and surrounding area, including Ridgegate Parkway, currently a 2-lane arterial which provides regional access between I-25 and The Town of Parker to the east. This roadway will be expanded to serve the East Villages and incorporate a one-way couplet for a portion of the roadway, encouraging a walkable mixed-use environment.

The area is also served by South Havana Street and South Peoria Street which provide local and regional access. These roadways will continue to be important connectors for the City Center and the Villages, including access to I-25, E-470, Castle Pines Parkway and the RTD Southeast Line stations. As the expansion of the City Center and the RidgeGate East Villages progresses, roads will be added as warranted.

A series of connector streets provide access from the main arterials to the individual Villages. The connector streets are located adjacent to the major open space system, creating a definable open space edge that provides a strong connection to the larger regional open space network.

The internal road network is designed as a modified grid that provides multi-modal connectivity to major land uses, open space and amenities within the Villages and surrounding area. The modified grid utilizes smaller blocks that allow for easy walking distance to schools, neighborhood and community parks, commercial centers and regional open space amenities.

### 2.9.2. RidgeGate Parkway/One-Way Couplet

The design of RidgeGate Parkway within the Central Village encourages a walkable mixed-use environment via two one-way couplet streets with mid-block cross-streets and signalized intersections. The couplet design divides the east and west bound traffic into two separate road sections, creating a safer and more comfortable pedestrian environment that encourages a high level of street activity in and around the Couplet District. The couplet configuration also creates stronger connectivity



RidgeGate Parkway example



RidgeGate Parkway/one-way couplet example



RidgeGate Parkway/one-way example



Pedestrian/bike-way integration

within the Central Village and reduces the chance of the Parkway becoming a physical barrier within the overall community.

### **2.9.3. On-Street Parking**

On-street parking is planned on most Residential Collector and Residential Local Streets, to complement the Framework Plan concept, and alleviate the need for large off-street parking facilities.

### **2.9.4. Bike Lane Integration**

An off-street cycletrack, designed to accommodate 2-way travel for bikes, will be provided along RidgeGate Parkway and on-street bike lanes will be integrated into the secondary street system.

### **2.9.5. Bus/RTD Integration**

Public transit is another means of providing multi-modal transit option and reducing dependency on personal motor vehicle use. Bus stop locations will be determined by RTD in consultation with the City of Lone Tree.

### **2.9.6. Wayfinding Signage Program**

A comprehensive Wayfinding Signage Program will be developed for RidgeGate East.

### **2.9.7. RidgeGate Parkway Setbacks**

The parkway will be characterized as a continuous urban landscape corridor that creates a park-like experience with regularly spaced street trees and a tree lawn that frames the road. As the parkway transitions into the Couplet District, setbacks should be reduced and a more formalized landscape incorporating street furnishings and lighting will help to create a comfortable pedestrian environment.



**Bike Lane  
Integration**



**Bus/Regional  
Transportation  
District  
Integration**



An aerial photograph of a residential development, overlaid with a semi-transparent brown filter. The image shows a winding paved path or road that curves through a grassy area. In the background, there is a cluster of modern, multi-story houses with gabled roofs and large windows. The overall scene is a well-planned residential area.

PART THREE

# GUIDELINES

Residential/Mixed-Use

# GUIDELINES CHECKLIST

## Project Site Planning Guidelines

The Site Planning component contains Guidelines and Standards primarily related to the proper organization of buildings, open space, and circulation/parking elements.

### Neighborhood Design

### Setbacks and Public Space

### Building Height

### Integration with Topography

### Utilities and Equipment

### Service, Delivery and Loading

### Monument Signs

### Parking and Parking Requirements

## Project Architectural Guidelines

The Architecture component addresses the design, image, and function of various building types that include single-family detached, attached, townhomes and apartments, as well as mixed-use residential over retail buildings.

### Architectural Image

### Massing and Scale

### Roof Form/Material

### Building Materials and Color

### Facade Articulation

### Garages

### Lighting

## Project Landscape Guidelines

The Landscape component contains Design Guidelines and Standards tailored to on-site landscapes, designed to:

- Soften building architecture and integrate with surrounding landscape.
- Screen and buffer large areas of pavement and parking.
- Frame, enclose and engage public/common open space.
- Complement the natural landscape, using native materials and sustainable irrigation practices.

### Landscape Character/Identity

### Irrigation

### Fences and Walls

### Mailboxes

### Public Art

NOTE: Single-family detached projects are subject to the City of Lone Tree subdivision platting process. Single-family detached projects are not subject to the City's Site Improvement Plan process and therefore the City does not review, approve or enforce architecture and landscaping for single-family detached homes. However, architectural and landscaping plans for single-family detached projects are subject to the RidgeGate Design Review Committee review and approval process, and are enforceable by the DRC.

# INTRODUCTION

## 3.1. Introduction

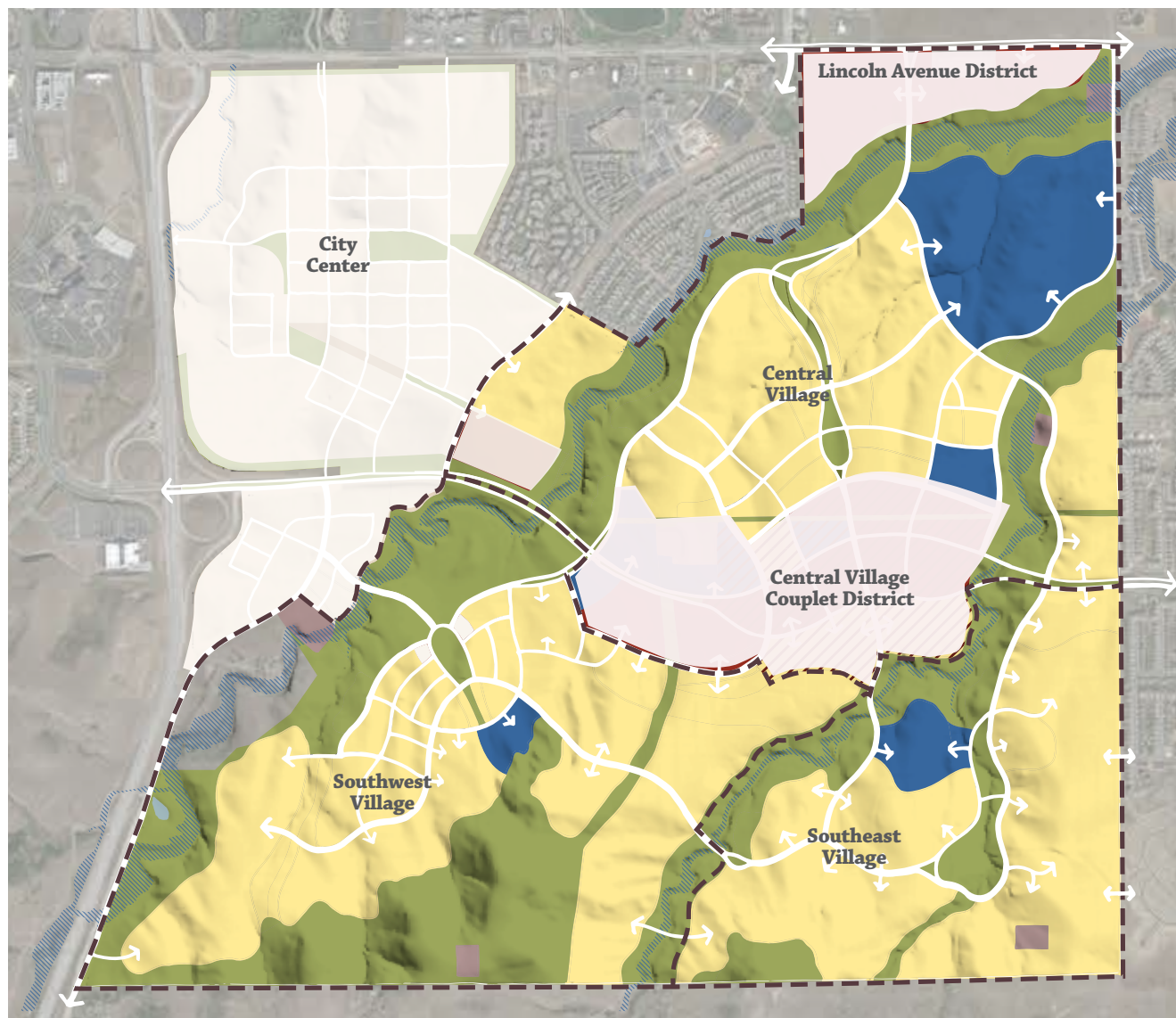
### 3.1.1. Sub-Area Context

The RidgeGate East residential community is located east of I-25 in Lone Tree, Colorado and near the RTD Southeast Rail Line. The RidgeGate East residential community expands south and east from the Lone Tree City Center (City Center).

The City of Lone Tree (Lone Tree) has set a high standard for its quality neighborhoods, great parks, service levels, cultural amenities, and areas of mixed use development. The RidgeGate East residential community, as part of this broader context, upholds and expands the vision for RidgeGate West and the City of Lone Tree.

### 3.1.2. Sub-Area Components

Residential/Mixed-Use Planning Areas components will reflect a range of densities with attached and detached housing types and styles encouraged through creative planning that minimizes streets with front-loaded garages or parking areas.



## 3.2. How to Use these Guidelines

### 3.2.1. Purpose and Intent

These Residential/Mixed-Use Guidelines provide prospective developers and builders with a clear statement of the design principles. The Guidelines are intended to assist in the identification and implementation of a design direction and level of quality.

The guidelines contained in this Part Three shall apply to any site to be developed for residential use within any of the three Villages. Specific guidelines are also established per residential type and density, including the following:

#### A. Single-Family

Single-family shall apply to detached single-family homes, attached single-family home products (townhomes, duplexes, triplexes, etc.) or other product types deemed similar by the City of Lone Tree.

#### B. Multifamily

Multifamily allows for a diversity of housing products and serves as transitional housing that may include multi-story structures, including stacked flat apartments and condominiums, with or without structured or under-building parking.

#### C. Mixed-Use

Mixed-Use creates a unique opportunity for a more versatile development product that provides a variety of services at the ground floor level creating synergy along the street and the upper levels serve the density demands for office or residential. (Refer to the Commercial/Mixed-Use Guidelines, Section 4, for detailed Mixed-Use Guidelines).



| Single-Family



| Multifamily



| Mixed-Use



# SITE PLANNING GUIDELINES

## 3.3. Site Planning Guidelines

The Site Planning Guidelines relate to the proper orchestration of buildings, building setbacks, open space, and circulation/parking elements related to, single-family, multifamily and mixed-use residential products.

### 3.3.1. Neighborhood Design

- Create residential neighborhoods that exhibit a sense of place, through a diversity of housing types, densities and price points, within a walkable, pedestrian friendly environment.
- Orient homes towards the street, supporting the concept of “eyes-on-the-street” neighborhoods.
- Connect internal neighborhood streets and paths to landmarks or amenity features such as parks, greens, squares, plazas, and community centers.
- Orient streets with terminal vistas of prominent buildings such as civic structures, places of worship, and clubhouses.
- Provide vehicular, pedestrian, and bicycle access to adjacent commercial properties, coordinated with sidewalks, alleys, and urban open space.

### 3.3.2. Setbacks and Public Space

Varying street setbacks promote streetscape diversity between structures and the street creating visual interest and diverse urban character. Consider the lot and block development and how the design integrates into the natural and existing site conditions.

#### A. Single-Family

The following guidelines shall apply to all single-family homes, attached single-family home products (townhomes, duplexes, triplexes, etc.) or other product types deemed similar by the City of Lone Tree.

- Orient buildings in a logical manner. Factors affecting location include, but are not limited to, street fronting location, orientation to public space, and topography.
- Organize building placement and floor-plan layout so as to promote active sidewalks, parks and public places.
- Vary the alignment of residential facades on adjacent homes so as to avoid a monotonous street frontage.
- Use stoops, porches and balconies to interact with the streetscape.

## Neighborhood Design

Develop visual variety along the street front by varying the building setbacks and floor-plan organization, while creating unified image along the street front through the landscape materials.



**Standards**

- Set back the front facade at least 10-feet (but not more than 20-feet) from the front property line. A front porch may encroach up to 5-feet into the setback area. With the exception of a roof overhang, no part of the building shall encroach within 5-feet of the property line, (see Figure 1).
- Side-yard setbacks shall not be less than 5-feet, except that a zero side-yard setback is permissible on one side of a lot, provided that the side-yard setbacks on the opposite side of the lot and the adjacent lot are a minimum 5-feet each. With the exception of a roof overhang, in no case shall two single-family detached residences be closer than 10-feet from each other, (see Figure 1).
- Along the length of a given block with front-loaded garages, setback the garages at least 5-feet from the front facade of the house on at least 75% of such conditions so that views of the garage door shall not dominate the street.
- Varied setbacks may be approved by the City based on topography, site conditions, environmental factors and/or easements.

SINGLE-FAMILY SETBACK AREAS*		
Location	Minimum	Maximum
Front Facade	10 Feet	20 Feet
Front Porch	5 Feet	20 Feet
Roof Overhang Encroachment	0 Feet	N/A
Interior Side-Yard (Exception Zero Side-Yard Setback)	5 Feet	N/A
Distance Between Residences	10 Feet	N/A
Street Side Setback	15 Feet	N/A
Rear Yard Setback	10 Feet	N/A
Accessory Structure	10 Feet	N/A

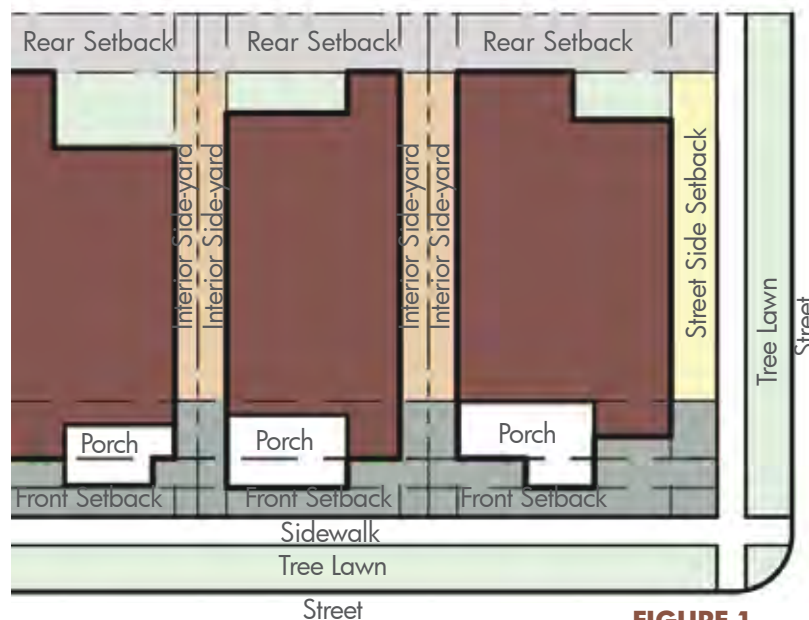
\* All setbacks measured from property line.

\* Single-family setback areas shall apply to all single-family homes, detached and attached single-family home products (townhomes, duplexes, triplexes, etc.) or other product types deemed similar by the City of Lone Tree.

**B. Multifamily**

- Create visual interest and a sense of human scale by varying the building form and materials.
- Step the architecture where multifamily housing is adjacent to surrounding lower density land use and/or properties.

**Setback Standards for Medium to High Density Single-family Residential Setbacks**



**FIGURE 1**



**Create eyes-on-the-street neighborhoods**

- Ensure that frontage setbacks and building alignment are compatible with other buildings along the street.
- Front the units toward the street with windows, visible entrances, and low front fences or walls.
- Provide dog run areas within the project site that are screened from nearby residences.

#### Standards

Building setbacks shall be proportional to the structure height.

- Architecture that is adjacent to an existing low density residential or commercial/office development may be up to 5 stories provided that the upper stories are stepped-back to minimize impact to lower density unit types, (see Figure 2).

#### C. Mixed-Use

- Encourage ground floor use that stimulates pedestrian activity through the quality of the mixed-use environment. Activate the pedestrian zone with glazing, porches, canopies and balconies along the street and internal unit frontage.
- Design the pedestrian experience, in which the user should be able to walk to and from adjacent buildings.
- Design plazas and courtyards between buildings to frame, highlight and emphasize views and points of access.
- Provide dog run areas within the project site that are screened from nearby residences.
- Incorporate clear, coherent connections to parks, plazas, transit stops and the multi-use trails and adjacent parcels.
- Provide outdoor gathering areas and patios in mixed-use areas.
- Create informal outdoor seating and gathering areas to encourage social interaction.

**Create visual interest and a sense of human scale by varying the building form, color and materials**



#### 3.3.3. Building Height

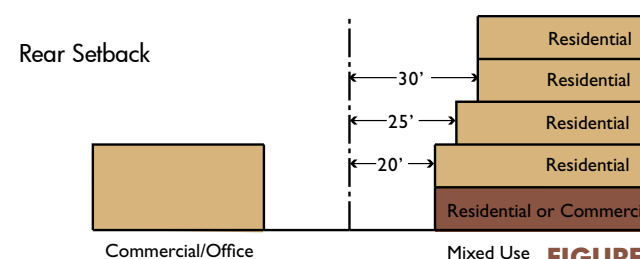
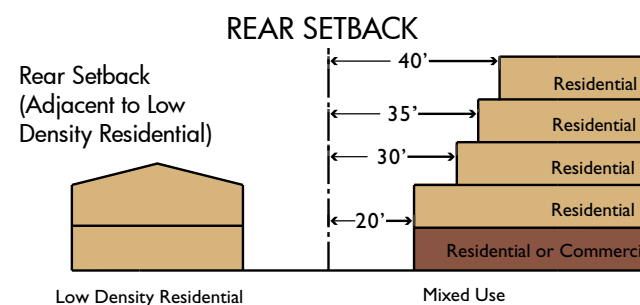
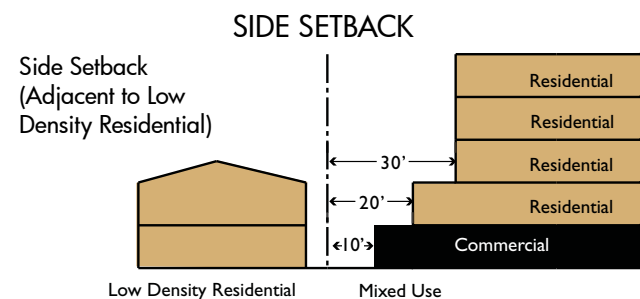
Ensure that buildings are compatible in height, scale, massing, and style with that of the surrounding neighborhood, based on the following guidelines:

- Differentiate individual buildings along the streetwall with transitions in building height per Figure 2.
- Maximum building heights shall conform to those set forth in the approved RidgeGate Planned Development District document (PDD).
- Maintain a compatible relationship between adjacent buildings such that the lot and scale of the larger buildings are similar at the project edges, (see Figure 2).

#### Standards

- For all residential units and other noise sensitive development to be constructed in the Centennial Airport Influence Area (AIA), the builder shall disclose to all prospective purchasers in writing, using the Centennial Airport Overflight Disclosure form, that the project is located in an area that could be subjected to numerous aircraft overflights and their associated effects. These effects include, but are not limited to: noise, smoke, dust, fumes and vibrations. Such disclosure shall also be noted on Plats and Site Improvement Plans (SIP's) and in sales contracts and closing documents.

#### BUILDING HEIGHT SETBACK



**FIGURE 2**

**Maintain a compatible relationship between adjacent disparate buildings types**

### 3.3.4. Integration with Topography

- Integrate buildings and structures with the overall topography of the site, utilizing varied floor plans and product types to minimize negative visual impact of walls and slope banks.
- Design and ensure a building's mass and foundations follows the slope and grade of the site.
- To the extent possible, blend the proposed grading naturally with existing topography.
- Utilize terraced retaining walls to minimize the visual impact of large retaining walls to public streets.

#### *Standards*

- The minimum distance between the face of a retaining wall and a building foundation shall be 5-feet.
- Walls in excess of 8-feet should be avoided. Walls above 8-feet should be tiered with 2-foot minimum landscape terraces.
- Walls must comply with the City of Lone Tree's policies and requirements.
- Walls greater than 4-feet in total height may require a PE Sealed structural wall design. For such walls, contact the City of Lone Tree Engineering Division for additional information about wall policies and requirements.

### 3.3.5. Utilities and Equipment

Locate and design utilities and equipment to protect the visual quality of the community, based on the following guidelines:

- Screen or recess utility panels adjacent to a building facade inside the property instead of along the sidewalk, in front of the building or within any adjacent open space.
- Integrate and locate all mechanical and electrical equipment, such as water meters, back flow preventers, electrical transformers, and similar devices, in the total design concept by locating them underground, in building recesses or in parkway strips.
- Design screening materials to be visually compatible with building materials and not detract from the site.

### 3.3.6. Service, Delivery and Loading (Multifamily/Urban Apartments)

Reduce the visual and functional impact of service areas on public roadways and spaces including:

- Create a safe street environment with the provision of a "designated" delivery parking area on the side of the building or within a side alley.
- Provide adequate space for on-street deliveries with a minimum dimension of 12-feet by 40-feet to be free and clear of obstructions at all times.
- Integrate pick-up and drop-off areas into the site to maximize service and site planning efficiencies.
- Consolidate trash and recycling receptacles into centralized areas internal to the project.

## Integration with Topography

Design and ensure a building's floor plan, mass and foundations follows the slope and grade of the site.



### 3.3.7. Monument Signs

An integrated program for signs should reinforce that the Villages and commercial Districts are part of the City of Lone Tree. City signs should be strategically located at key gateways. Other Village and District identification and wayfinding signs should incorporate reference to the City in the design of the signs.

Create a strong sense of place through an organized and interrelated set of community identification elements to include high-quality signs, sign structures, lighting, and graphics.

- Signage throughout RidgeGate East shall follow the Wayfinding Signs Program to be developed.
- Design mixed-use buildings to accommodate changeable tenant signs, including wall signs, projecting signs and window signs.
- Develop each neighborhood entry identification feature to reflect the character of the community areas and enhance the overall community streetscape.
- Create signs and graphic elements that are related to and/or respect the architecture that they serve.
- Landscape enhancements will be required for all monument signs.
- Create a hierarchy and sign types that serve the following:
  - » Community Monument Signs  
Potential locations for future monument signs for RidgeGate East include the following:
    - RidgeGate Parkway and Peoria Boulevard
    - RidgeGate Parkway at the eastern boundary of Lone Tree
  - » City of Lone Tree gateway and wayfinding signs as provided by the City.
  - » Neighborhood Signs/Entry Monuments
    - Neighborhood identity signs for each neighborhood
    - Key entry location for each Village

#### *Standards*

- All signs are subject to Sign Standards in Article XXIX of the Lone Tree Zoning Code, as amended, until such time as an alternative Sign Plan for RidgeGate East is developed and approved by the City. A sign permit is required from the City of Lone Tree Building Division for all new signs and modification to existing signs.



**Create a sense of entry through community identification elements**



**Propose neighborhood signs at key neighborhood entry roadways**



**Create entry monuments/signs that respect the architecture they serve**

### 3.3.8. Parking and Parking Requirements

#### A. Off-Street Parking (See Chart)

Provide adequate parking for residents and guests in a site-sensitive manner, with the following guidelines:

- Provide pedestrian connections from the parking areas/lots to building entries, street crossings and open space connections.
- Encourage shared parking between adjacent uses to reduce the number of vehicle parking spaces needed.
- Minimize the visual impact of parking lots along the overall streetscape or open-space frontage by locating them behind a building, low screen wall and landscape, or away from the street edge.
- Provide landscaped parking islands and landscape breaks at a rate of 1 per 15 parking spaces to reduce visual impact of parked cars, and filter and reduce stormwater runoff in parking areas.
- Design parking structures to be compatible with related buildings in massing, scale and materials.
- Encourage users to park once and walk among the mixed-use areas through innovative site design.
- Design loading and trash collection areas to ensure that drives and alleys are not blocked.
- Parking structures facing pedestrian oriented streets must incorporate facade treatments to conceal views of parked cars and are encouraged to incorporate active retail or commercial at street level edges.
- Consistent with the City Zoning Code, the Director of Community Development may recommend an increase or decrease in the minimum parking standards based upon documented comparisons, information from similar jurisdictions, evidence from qualified professionals, changes in driving patterns, or in consideration of unusual site design factors.

#### B. Snow Storage

- Accommodate and locate snow storage in areas that do not conflict with pedestrian plazas, walkways and building entrances. On-site snow storage may not be plowed into, nor stored within the public Right-of-Way.
- Locate snow storage and disposal areas where snowmelt can infiltrate into the ground, filter through a vegetated buffer or be directed to stormwater treatment areas.
- Snow shall not be plowed directly into streams or wetlands.
- Parking spaces may not be used for snow storage, except in cases where the parking spaces provided exceed the required minimum.
- Locate snow storage areas where there is high solar gain.

- Design site to accommodate snow plowing and snow storage. Create a snow removal plan.

#### C. On-Street Parking

- Provide on-street parking opportunities geared towards the needs of residents and visitors.
- Explore managed on-street parking strategies (meters, time limits, etc).

#### Minimum Off-Street Parking Ratios

<b>SFD</b>	- 2 spaces per single-family detached residence in either the garage or private parking lot or driveway apron
<b>SFA</b>	- 2 spaces per single-family attached residence shall be provided in either the garage, private parking lot or driveway apron - .25 parking spaces per unit shall be required for guest parking. A minimum of 25% of required guest parking must be satisfied on-site in parking lots or garages; the balance of required guest parking spaces may be allowed on immediately adjacent streets if available. On-street parking adjacent to ground-floor commercial uses cannot count towards residential parking requirements.
<b>MF</b>	- 1.5 parking space per one-bedroom unit, plus .25 guest space per unit - 2 spaces for 2- and 3-bedroom units, plus .25 guest space per unit with 25% of required guest parking satisfied on-site in parking lots or garages; the balance of required guest parking spaces may be allowed on immediately adjacent streets if available. On-street parking adjacent to ground-floor commercial uses cannot count towards residential parking requirements. - The number of on-site parking spaces for multifamily located within one-half mile of a light rail transit station may be reduced up to 20%.

# ARCHITECTURE GUIDELINES

## 3.4. Architecture Guidelines

RidgeGate East's residential neighborhoods are intended to reflect a full range of architectural products and styles with complementary street patterns.

### 3.4.1. Architectural Image

The Architectural image focuses on the design, image, and function of various building types that include single-family, multifamily and mixed-use residential and supports a wide variety of land uses and densities within RidgeGate East. The character and scale of the architecture will be based on the location within the overall project, the defined Villages, and other natural and man-made site considerations.

### 3.4.2. Massing and Scale

#### A. Overview

- Design human-scaled rather than massive or monumentally-scaled architecture.
- Create orderly, rhythmic, and proportional building masses which unify the building's form.
- Rest the building on a discernible base or pedestal designed to anchor the building to the ground.
- Express the structure of the building. Distinguish columns and structural bays to display how the building is being supported.

#### B. Single-Family

- Design homes as an integrated collection of volumes that complement a primary structure, rather than a single boxy building form.
- Add visual interest to the composition of the home profile by varying roof heights and providing smaller second-story building volumes.
- Crown homes with roofs that provide deep roof overhangs, consistent with the architectural style of the home.
- Use single-story building masses such as covered porches as transitional elements to second-story building volumes.
- Segment buildings through the use of pop-outs and building projections.
- Break-up rear building masses using multiple roof plane breaks and wall planes that add visual interest to the rear facade.
- Integrate deck roofs seamlessly into the fabric and form of the home.
- Design house foundations as a natural extension of the ground plane.

#### C. Multifamily

- Celebrate the corner by increasing building mass such as through the use of towers and turrets designed to reflect a higher intensity of activity.
- Use architectural elements such as roof eaves, cornice elements, material bands, and consistent window rhythms.

42

## Architectural Image

The character and scale of neighborhoods will be based on the location within the overall project and other natural and man-made site considerations



- Design human-scale building elements for facades that are oriented to a public street or open space that includes the following:
  - Expressions of building structural elements, such as floors (banding, belt courses, etc.), columns (pilasters, piers, quoins, etc.), and foundations (masonry wainscot).
  - Patterns of window and door openings that emphasize a change of wall plane material, color, texture, or pattern including, the use of sills, lintels, mullions, muntins, and other scale-providing elements.

#### **D. Mixed-Use**

Create a vibrant urban environment utilizing structures in the mixed-use areas with unique architecture and block cohesiveness that facilitate a positive pedestrian experience.

- Provide prominence at street intersections to enhance the streetscape and street vistas by increasing the scale of the block massing at corner conditions.
- Consider vertical breaks, changes in material, or changes in wall plane to emphasize the corner conditions.
- Design building masses which create street-walls designed to frame and define the streetscape.
- Return/wrap primary facade materials at alley or service locations a distance that covers pedestrians field of view when walking along sidewalk.

#### **3.4.3. Roof Form/Material**

- Create interest between structures and along street faces with articulated and varied roof forms.
- Create both horizontal and vertical roof articulations from the primary street frontage. A variety of roof breaks (roofs that turn a corner or change elevation) should be provided.
- Create roof pitches and overhangs which complement the architectural style of the home, designed to shed snow, shade windows, and convey roof runoff.
- Integrate roof gutters and downspouts into the design of the home, appearing as a continuous architectural element.



**Low Density Residential**



**Medium Density Residential**



**High Density/ Mixed-Used Residential**



**Add visual interest to the composition of the home profile by varying roof heights and pitches**



#### *Standards*

- More contemporary roof types/styles shall be reviewed on a case-by-case basis such as: Flat, Half Vaults, and Sheds.
- Roof materials may include ceramic, slate, concrete tile, laminated multiple-ply composition shingles, metal seam or other materials. Wood shake roofs are prohibited. Other roof materials may be reviewed on a case-by-case basis.
- Roofs shall maintain a minimum 3:12 and maximum 12:12 roof pitch.
- Solar panels shall only be mounted on building roofs.
- Laminated or multiple-ply composition shingles shall have a multi-dimensional appearance and a minimum 30-year warranty.
- Metal seam roofs must have integral color that is compatible with the facade.
- Screen roof-top mechanical, electrical and telecommunications systems from surrounding streets and structures, regardless of location.
- Colors should reflect or complement the surrounding natural environment.
- Roofs shall not be reflective.

#### **3.4.4. Building Materials and Color**

- Select quality and durable building materials that are complementary to the neighborhood and architectural style.
- Use a color palette to delineate color ranges and combinations that reflect or complement surrounding homes and neighborhood.

#### **A. Wall Materials**

- Incorporate building materials that reflect the architectural style of the home.
- Use building materials with strong textures and rich colors.
- Use stucco finishes that are not overly exaggerated or irregular striking a balance between bland stucco textures and overly elaborate surfaces.
- Use native and locally made materials where possible.
- Use materials that reflect the local architectural palette including board and batten, brick, clapboards, cementitious siding (i.e. hardy board), shingles, and stone, rather than imported or foreign materials such as adobe and clay Mission tile.
- Use brick or stone masonry, or stucco foundations (when appropriate for the architectural style of the home) rather than exposed concrete foundation walls.

#### *Standards*

- Attenuate noise with the use of suitable building materials, appropriate construction methods, and innovative building design, as provided in the International Building Code as adopted the City of Lone Tree.
- Exterior Insulation and Finish System (EIFS) is prohibited.

#### **B. Wall Color**

Building colors must complement the architectural style of the structure.

- Relate color variety to changes of materials, such as building base, facade, and roof.

### **Roof Form/Material**

Create roof pitches and overhangs which complement the architectural style of the home



### *Standards*

- Residential building exteriors may be finished in painted cementitious “Hardiplank” siding, brick, stone, cast stone, or stucco. Architectural metal siding may be used as building accents. Vinyl siding is prohibited.

### **C. Masonry Requirements**

The following materials shall be considered Masonry:

- Brick, Natural Stone, Cast Stone, Integrally Colored Split or Burnished Face Concrete Masonry Units (CMU), Portland Cement Plaster (Stucco) with Acrylic Finish Coat. Man-made veneer stone is prohibited.

### **3.4.5. Facade Articulation**

#### **A. Awnings and Balconies (Multifamily)**

- Utilize awnings and balconies to add dimension and interest to the streetscape.
- Locate and design awnings/canopies to prevent drainage onto sidewalks to minimize icing and runoff.
- Provide balconies to add habitable outdoor space for multifamily.
- Use awnings to help energy efficiency, protect pedestrians from the elements and provide shade for ground-level uses.
- Consider using awnings or shading devices to manage glare at ground level retail.
- Provide strong columns, piers, and posts rather than thin, flimsy, weak-appearing supports.

### *Standards*

- Avoid building features that encroach into the public right of way such as awnings, roof overhangs, balconies and patios. If, and when, allowed, a license agreement request must be submitted as part of the SIP process. Such features must maintain ADA accessibility and retain a minimum 8-foot clearance above the adjacent ground surface or walk.

### **B. Windows and Doors**

- Locate windows and doors at the lower levels of building facades to enhance building function, form and scale and to create a lively streetscape.
- Provide high quality doorways to express quality workmanship and relate to the architectural style of the building.
- Articulate the facade with well-defined building entrances, including projecting and recessed facade features.
- Primary entrances into buildings shall be identified through elements and detailing that orient to streets, plazas, public drop-off areas, and public spaces, not to interior blocks or parking lots.
- Encourage the same window style and patterns on all sides of the same structure consistent with the entire building.
- Contemporary designs may consider using unique horizontal window patterns and locations.

### *Standards*

- Window frames other than wood shall be anodized, electrostatically painted or vinyl coated.
- Wood frames shall be aluminum-clad, painted, sealed or stained.
- Transparent glass shall possess a minimum 60 percent light transmittance factor. Reflective and mirrored glass with a visible reflectivity greater than 10 percent is prohibited.
- Integrate window fenestrations into the function and design of the building, creating indoor/ outdoor connections that celebrate views and daylighting.

45

**Relate color variety to changes of materials, such as building base, facade and roof**



**Utilize awnings and balconies to add dimension and interest to the streetscape**

### 3.4.6. Garages

- Streetscapes shall not be dominated by garage faces through providing a variety of recessed, front, and side loaded garage types along the street.
- Design garages to be consistent with the architecture of the primary structure.
- Encourage garages that are alley-loaded. If an alley-loaded garage is not possible, a side-loaded garage is preferable to a front-loaded garage.

#### *Standards*

- Streetscapes shall not be dominated by garage faces through providing a variety of recessed, front, and side loaded garage types along the street.
- Driveways shall be a minimum of 19-feet in length to reasonably accommodate a parked car without overhang of sidewalk.

### 3.4.7. Lighting

- Provide front porch wall lanterns or pendent lights for single-family, that reflect the architectural style of the home.
- Provide shielded or cut-off lighting fixtures designed to prevent nuisance glare.
- Complement the architectural style with appropriate fixtures.
- Use appropriate site and building lighting to highlight the architecture while providing safe and vibrant environments and unifying the streetscape through compatible fixtures.
- Create safe and attractive environments at night while minimizing light pollution and glare; strategies such as down-lighting are encouraged.
- Use controls, daylight sensors and/or timers to control exterior architectural lighting.
- Use energy efficient lighting such as high-efficiency LED, smart sensors and low-level lights.



**Articulate the facade with well-defined building entrances that establish a rhythm and add visual interest to the block face**



**Create safe and attractive environments at night while minimizing light pollution and glare**

# LANDSCAPE GUIDELINES

## 3.5. Landscape Guidelines

RidgeGate East's residential neighborhoods respect the natural terrain of this unique Colorado landscape. The landscape guidelines are designed to promote the softening of building architecture, screen large expanses of pavement and frame and engage formal open space.

### 3.5.1. Landscape Character/Identity

Create a landscape that is sustainable, attractive, comfortable, and complementary to the natural and man-made environment.

- Design the landscapes in a high traffic/activity area (including mixed-use where mixed-use and higher density housing occur) with durable materials.
- Integrate dwelling units with the natural environment by working with existing landforms, natural features and vegetation.
- Vary the landscaping along the street frontage within a consistent theme of street trees and palette of materials.
- Avoid sight triangle conflicts at street corners, and/or between driveways and pedestrian or bicycle ways.
- Reference the Lone Tree recommended plant/tree list.

#### Standards

- Maintain an open and accessible walkway along and through the Xcel gas easement/promenade.
- Landscaping of lots within the underground Xcel Gas Easement will be required to file a Landscape Encroachment Application and comply with the requirements of Xcel Energy.

### 3.5.2. Irrigation

RidgeGate is intended to be developed in a manner which is responsive to many environmental considerations. The most important is water conservation.

- Plant drought tolerant and low-water requirement plants.
- Select plants with similar water requirements to be planted together.
- Grade residential lots in a manner that minimize on-site irrigation water from flowing into areas of native vegetation.
- Avoid spray heads that over-spray to impervious areas including sidewalks and roadways.
- Use an in-ground sprinkler system including drip or other low-volume application systems with controllers linked to rain sensors.
- Utilize drip/low-volume application systems controlled by controllers linked to rain sensors. One rain sensor per two controllers is required.
- Ensure adequate watering for the establishment of native vegetation.
- Ensure watering is reduced to sustaining levels following establishment.

### 3.5.3. Fences and Walls

Enhance the streetscape and open space through fences and walls that reflect the architectural style and open space landscape character including the following:

- Incorporate fencing that is relatively open while still providing a level of screening and privacy.

## Landscape Character

Create a landscape that is sustainable, attractive, comfortable, and complementary to the natural and man-made environment



- Design project edges to complement natural landscape, when adjacent to existing natural open space through the use of open fencing, setbacks, and compatible landscaping.
- Design walls and fences including color, and material to be consistent with building architecture or blend with the landscape design and surrounding natural landscape.
- Create attractive retaining walls to avoid excessive slopes and limit stormwater disturbance.
- Design fences that are punctuated by appropriate pilasters, pillars, columns or other elements.
- Use higher quality design and materials for fencing along public streets and open space areas.
- Incorporate landscaping within close proximity of fences and walls for screening and softening.
- Minimize fences and walls in front and side yard areas or where visible from public streets and open spaces
- Through context-sensitive site design, minimize the use of retaining walls due to long-term maintenance costs implications.

#### *Standards*

- Front yard fences are allowed in combination with a retaining wall only if the total height from the sidewalk does not exceed 3-feet.
- Rear yard fences shall have a maximum height of 6-feet and must be set back 10-feet from the building's rear facade.
- Individual retaining wall shall not exceed 8-feet in height, or should be terraced into multiple walls.
- Open fencing is required adjacent to parks and open space and the urban trail.
- Permanent chain-link fences are prohibited.
- Individual retaining wall shall not exceed 8-feet in height, or should be terraced into multiple walls.

#### **3.5.4. Mailboxes**

Locate and design mailboxes in strategic locations within the neighborhood, considering easy walking distance to homes and common areas to include the following:

- Group mailboxes together when possible. Mailboxes may be located in one or multiple community buildings that serve multiple neighborhoods.
- Locate site furnishings, such as benches, trash receptacles, signage, drinking fountains, shade structures, or bicycle racks near mail collection areas to promote community gathering.

- Provide a secure location for community mailboxes that are visible from adjacent streets and houses to enhance security, and integrate with surrounding architectural character.

#### **3.5.5. Public Art**

Where public art is included in a project, the following apply:

- Utilize public art to enhance streetscape and high visibility common areas.
- Locate art in a way that considers relationship to architectural and natural features, landscape design, and public common areas.
- Consider the social context and other uses of the artwork or space.
- Ensure the siting of each art piece considers public safety, liability issues, vehicular, and pedestrian traffic patterns.



**Integrate public art into the site design**



**Place centrally located mailboxes within the neighborhood and combine with other site furnishings**



A sepia-toned photograph of a city street scene. The image shows a row of multi-story buildings with various architectural details, including windows, balconies, and awnings. Pedestrians are visible walking on the sidewalk and crossing the street. The sky is filled with soft, wispy clouds. The overall tone is warm and historical.

PART FOUR

# GUIDELINES

Commercial/Mixed-Use

# GUIDELINES CHECKLIST

## Project Site Planning Guidelines

The Site Planning component contains Guidelines and Standards primarily related to the proper organization of buildings, open space, and circulation/parking elements.

**Lincoln Avenue District**

**Central Village Couplet District**

**Central Village Couplet District:  
Block Face Configuration General Guidelines**

**Building Height**

**Integration with Topography**

**Utilities and Equipment**

**Service and Delivery**

**Parking and Parking Requirements**

**Monument Signs**

## Project Architecture Guidelines

The Architecture component addresses the design, image, and function of various building types that include, higher density residential, apartments, mixed-use and stand-alone commercial, retail and office.

**Architectural Image**

**Massing and Scale**

**Facade Articulation**

**Building Materials and Color**

**Roof Form and Materials**

**Lighting**

**Building Signs**

## Project Landscape Guidelines

The Landscape component contains Design Guidelines, and Standards tailored to on-site landscapes, including landscapes designed to:

- Soften building architecture and integrate with surrounding landscape.
- Soften and screen large areas of pavement.
- Frame, enclose and engage public/common open space.

**Landscape Character/Identity**

**Irrigation**

**Fences and Walls**

**Site Furnishings**

**Lighting**

**Public Art**

**Street Planting Areas**



# INTRODUCTION

## 4.1. Introduction

### 4.1.1. Sub-Area Context

The RidgeGate East commercial and Mixed-Use community is located east of I-25 in Lone Tree, Colorado and is located near the RTD Southeast Rail Line. The RidgeGate East residential community expands south and east from the Lone Tree City Center (City Center).

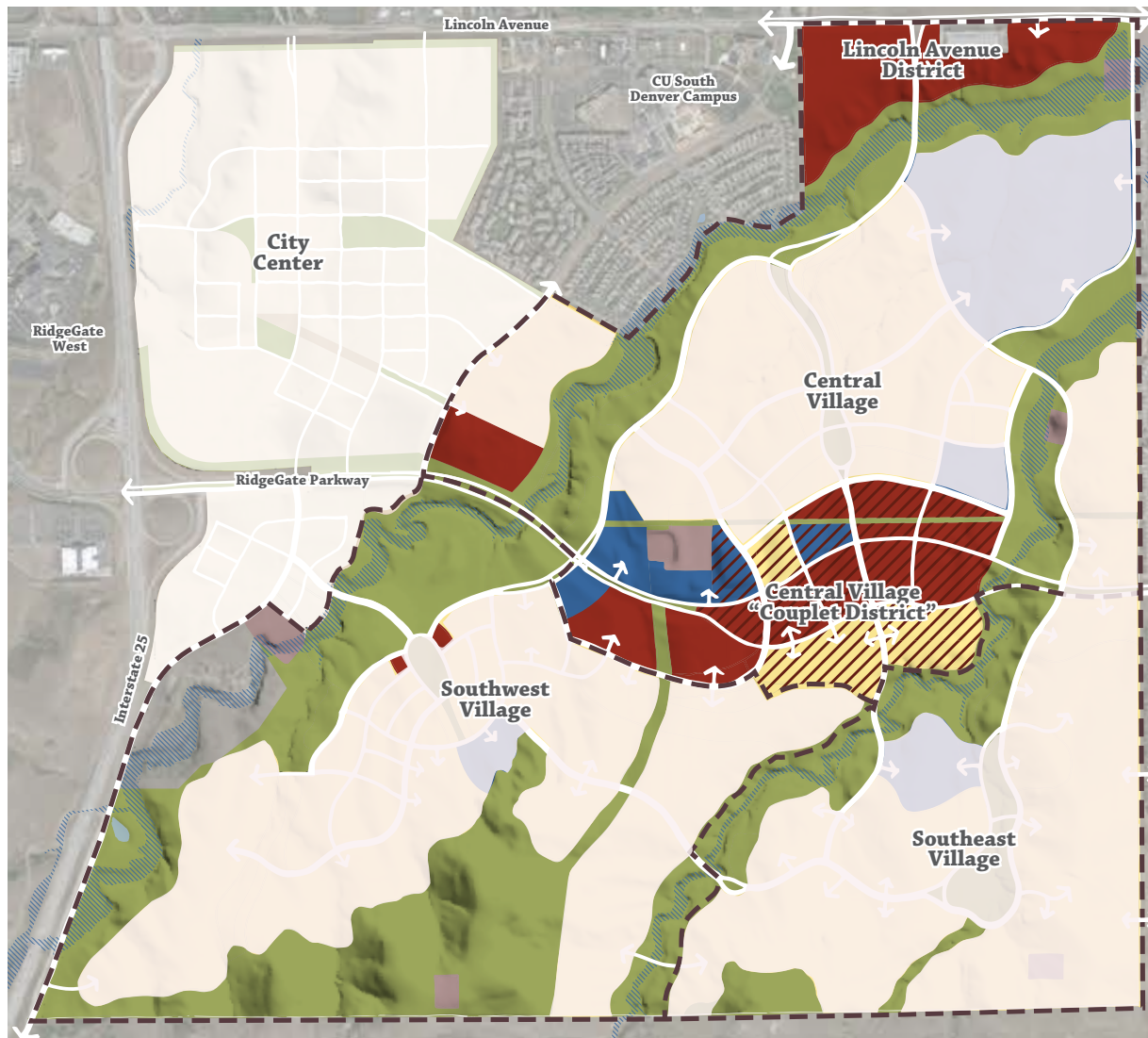
The Central Village contains two commercial Mixed-Use Planning Areas or Districts. The Central Village Couplet District is located in the heart of the community along RidgeGate Parkway, while the Lincoln Avenue District is a high visibility area adjacent to Lincoln Avenue and the CU South Denver Campus. The Central Village Couplet District provides a commercial mixed-use heart to the RidgeGate East Villages.

The City of Lone Tree (Lone Tree) has set a high standard for its quality neighborhoods, great parks, level of services, cultural amenities and areas of mixed use development, and the West Village of RidgeGate has been helping to further diversify and strengthen the quality of Lone Tree.

### 4.1.2. Sub-Area Components

The Commercial/Mixed-Use Planning Areas are mixed-use neighborhoods that emphasize commercial uses. The Commercial/Mixed-use Planning Areas are intended for a mix of uses in a pedestrian-friendly environment. They rely upon an interconnected street network and connections to open space, which defines the District edges.

**Commercial /  
Mixed-Use Areas  
(Central Village  
Couplet District  
and Lincoln  
Avenue District)**



## 4.2. How to Use these Guidelines

### 4.2.1. Purpose and Intent

The Commercial/Mixed-Use Guidelines (C/MU) provide prospective developers and builders with a clear statement of the design principles. The Guidelines are intended to assist in the identification and implementation of a design direction and level of quality, and they establish criteria for more specific commercial and residential land uses and building types, including the following:

#### A. High Density/Multifamily Residential

High Density residential uses creates the ability to transition to mixed-use in a way in which the community structure blends seamlessly and provides visual interest within the Central Village. Uses include residential, apartments and condominiums, with option for non-residential uses at the ground floor (though not required).

#### B. Mixed-Use (Commercial/Residential)

The overall character of the Commercial Mixed-Use Areas creates a commercial program with offices, shopping, dining and services in a vertically and horizontally-integrated pedestrian friendly environment. These mixed-use areas create a unique opportunity for a more versatile development product that provides a variety of commercial and retail uses and services at the ground floor level, creating a synergy along the street, while the upper levels serve the density demands for office or residential.

#### C. Commercial (General)

The character of the Commercial/Mixed-Use Areas blend with the overall community and provide goods and services within and beyond RidgeGate East. Commercial Areas identified in the land use plan will be high quality, well integrated and provide visual interest, while complementing the surrounding architectural and landscape character. The commercial uses may include a wide variety of commercial, office and institutional buildings, from single and multi-story stand alone, in-line shops and large format building types.

## 4.3. Commercial/Mixed-Use Areas (Districts)

### 4.3.1. Area Description/Overview

There are two primary Mixed-Use Commercial Districts located within the RidgeGate East Central Village, each with distinct development uses, pattern and character. These areas are identified on the Framework Plan as the Lincoln Avenue District, and the Central Village Couplet District.

#### A. Central Village Couplet District

The Central Village Couplet District is at the heart of the RidgeGate East, a mixed-use environment with opportunities for integrated ground floor retail, including shops, dining, cafes, and entertainment, with higher density residential, commercial and office buildings interspersed throughout the Couplet District. The District will also integrate civic and institutional uses that will serve the community and adjacent neighborhoods. The Couplet District will be served by RidgeGate Parkway which forms a couplet with two, one-way streets and a system of interconnected roads and sidewalks that collectively form a walkable and attractive pedestrian-friendly environment.

#### B. Lincoln Avenue District

The Lincoln Avenue District is located south of Lincoln Avenue and can accommodate a range of commercial, office, institutional, education, and cultural uses benefiting from good visibility and access to Lincoln Avenue. This area will serve as a gateway into RidgeGate East and provide connectivity to the Central Village and surrounding residential neighborhoods.

# SITE PLANNING GUIDELINES

## 4.4. Site Planning Guidelines

The Site Planning component contains Guidelines and Standards primarily related to the proper organization of buildings, open space, and circulation/parking elements.

### 4.4.1. Lincoln Avenue District

The Lincoln Avenue District represents the northern gateway into RidgeGate East and the Central Village. The architectural and landscape character in this area plays an important role in introducing the project as an integrated, community commercial area focused around regional and neighborhood open space.

- Provide significant architectural or landscape features at the corner where the building is set back further than 20-feet from the property line to emphasize the public streets and enhance the streetscape.
- Clearly define pedestrian access from public sidewalks, parking areas, and transit stops to building entrances.
- Setback buildings from Lincoln Avenue right-of-way a minimum of 75-feet. Screen walls and signs may encroach into the setback area up to 15-feet.
- Place parking predominantly behind buildings, and where it is visible to the street, use low walls and vegetative screening.

### 4.4.2. Central Village Couplet District

RidgeGate East's Central Village Couplet District represents an important part of the Commercial/Mixed-Use Areas. The following Guidelines provides a planning strategy based on a block -face configuration for the streets and blocks within the Couplet District.

### 4.4.3. Central Village Couplet District: Block Face Configuration General Guidelines (Building Setback Line)

The block face guidelines apply to the Couplet District and combines standards for site layout, building setbacks and streetscape (as shown in Figure 3 and Figure 4). The principles behind these guidelines provide for flexibility among compatible uses to allow developers and builders to respond to changing market demands. General block face guidelines include:

- Orient primary building storefront openings towards the street adjoining walks, streets or plazas as opposed to rear parking areas.
- A continuous building frontage and block corner articulation is encouraged, where possible, to be constructed along public street edges, (see Figure 3, Block Face guidelines).
- Create a pedestrian-oriented environment by ensuring the ground floors of buildings are permeable and interesting and avoid "dead zones" along the street.
- Create distinctive corner architecture and building masses to frame the corner and create usable outdoor gathering space at corners such as common areas, dining areas, cafés etc.
- Create a formal rhythm of street trees and consistent pedestrian lighting, site furnishings, planters, and banners that continues along both sides of the public streets within the Couplet District.
- Locate primary parking areas behind the principal building away from the public realm and screen from the primary public streets, where possible.
- Orient service functions towards the rear of buildings and away from the public view. They shall be screened from the street.

## Frame Plazas with Buildings

Design plazas and central squares between buildings to frame, highlight and emphasize views and points of access.



- Continue the block face urban form where parking fronts onto a street through strategies such as low walls, and/or landscape.

### A. Block Face: Urban Form and Site Design

Frame the street with buildings to promote an attractive and active pedestrian environment with continuous block faces including.

#### 1. *Focal Architecture*

Consider designating areas appropriate to create distinctive focal architecture as a response to a building's location within a prominent area that helps define the image of the Couplet District. This may include centering a building on the axis view, or expressing the corner, entry, roof or building wall to respond to that location that serve as destinations and identifiable architectural features. Suggested locations have been identified to assist in locating these architectural focal points within the Couplet District, (see Figure 3).

#### 2. *Central Square*

The building orientation and the building setback zone along the main street supports a continuum of walkability with places to pause which include the central square. The central square provides a "place" to relax and supports programmed activities at the heart of the Couplet District, (see Figure 3).

#### 3. *Pedestrian Connections*

The Couplet District should be designed to create a pleasant walking experience within the internal blocks that connect commercial, residential, and public outdoor spaces and to develop a strong network of sidewalks and pedestrian walkways that provide opportunities for pedestrians to walk safely and conveniently.

#### 4. *Large Format Retail/Commercial Buildings & Parking Fields*

Integrate large format retail and commercial centers into the fabric of the surrounding community through complementary architecture and responsive site design.

- Encourage large format retail and commercial centers to locate the primary building facade visible to adjacent public street(s).
- Provide a pedestrian friendly environment through complimentary architecture that creates pedestrian scaled facade articulation at the ground floor along public street(s).

### B. Block Face: Street Types

A hierarchy of street types will guide the architectural and landscape design and provide both an attractive streetscape as well as a safe and comfortable pedestrian environment. Streetscapes and pedestrian connections are defined by the following street classifications:

#### 1. *"Main Street" Character*

The "Main Street" represents the core of the Couplet District. It is pedestrian friendly with architecture that fronts the street and provides areas for outdoor dining and cafes.



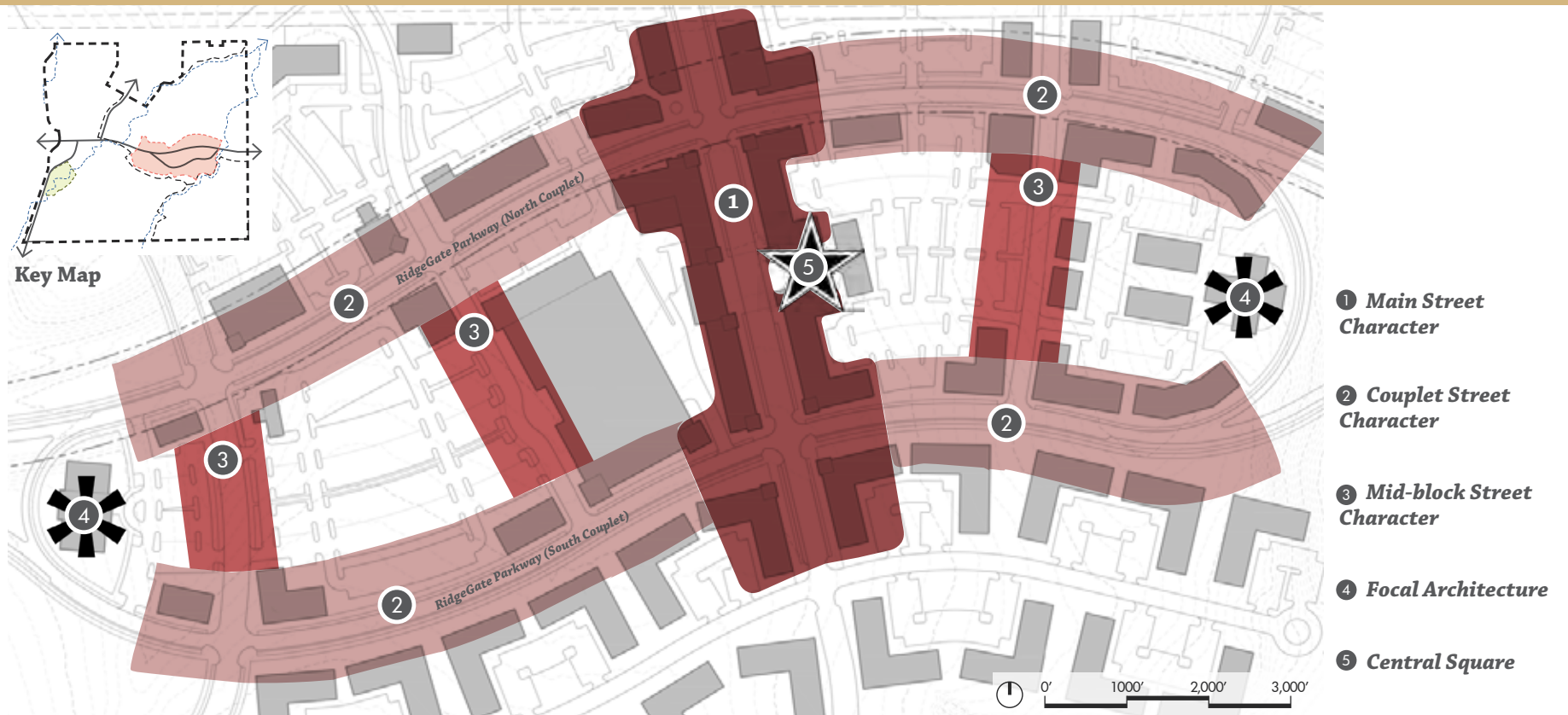
**Provide focal architecture with character at key locations/destinations within the District**



**Create a strong network of sidewalks and pedestrian connections**

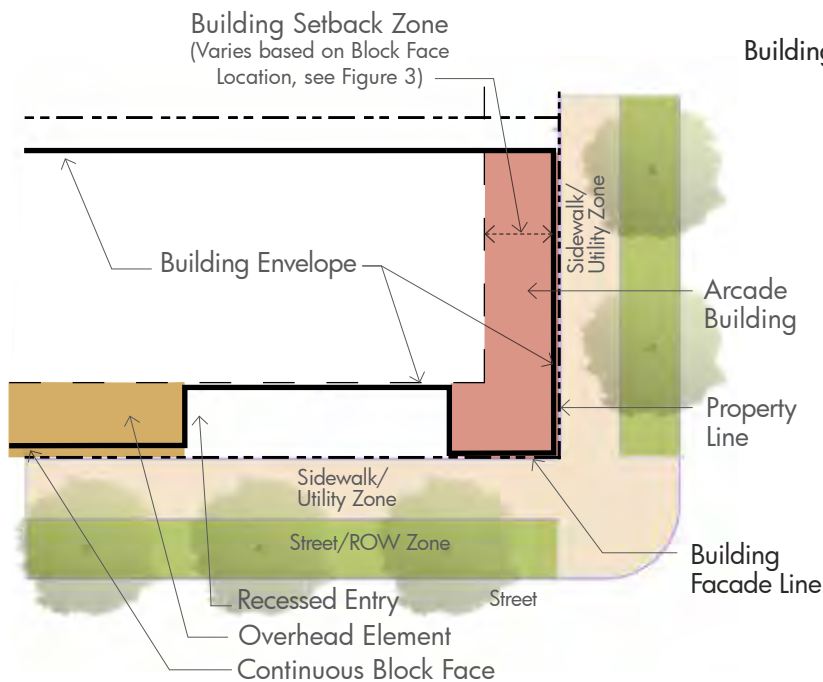


**Provide a pedestrian friendly Main Street with architecture that fronts the street and provides areas for outdoor dining**



Central Village Couplet District: Block Face & Setback Configuration Diagram

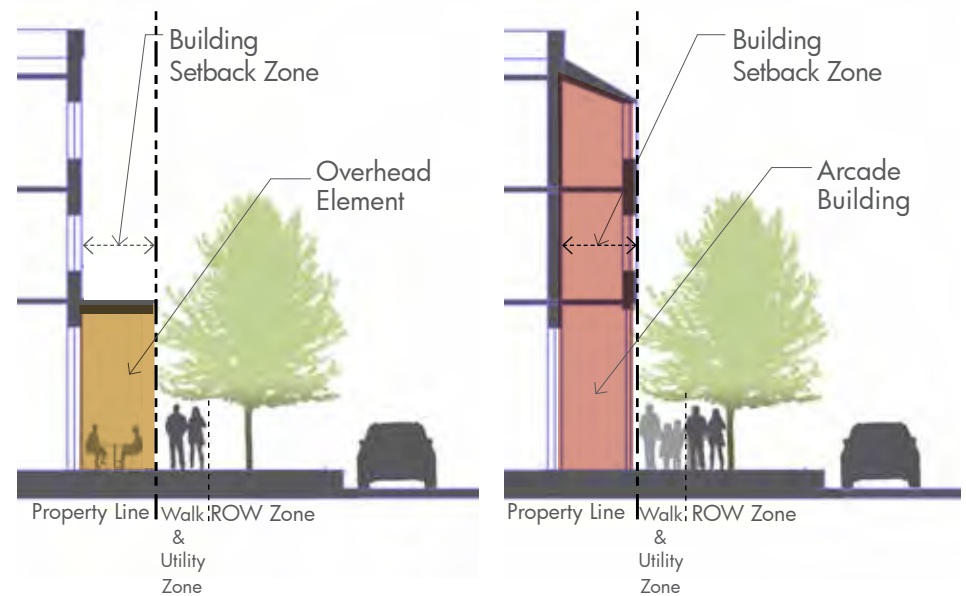
FIGURE 3



NOTE: Building setback is based on Block Face guidelines and Street Character (See description, opposite page)

PLAN:  
Building Setback Zone

Building Facade Line is the location of the vertical plane of a building along a street frontage.



NOTE: Building setback is based on Block Face guidelines and Street Character (See description)

ELEVATION:  
Overhead Element

ELEVATION:  
Arcade Building

Central Village Building Setback Zone: Plan and Elevations

FIGURE 4

The street provides parallel parking for easy access to restaurants and retail, and creates a linear hub of activity between the one-way couplet that incorporates a central urban plaza for programmed activities.

- Allow for a central square and pedestrian gathering place to be located in the mid-block area of the Main Street, to accommodate programmed events and activities.
- A reduction in the building frontage maybe be allowed where outdoor seating, dining, plazas, building entry features and other public amenities that contribute to the streetscape are included.

### 2. RidgeGate Parkway Couplet Street Character

The couplet is the one-way segment of RidgeGate Parkway through the Central Village, and includes architecture that fronts the street edge with a mix of uses, and defines the “street wall” to create a comfortable pedestrian-scaled environment.

- Along the one-way couplet streets, a continuous building frontage is encouraged such that the primary or secondary building facades are located within 25-feet of the property line of the street, (see Building Setback Zone, Figure 4).

### 3. Mid-Block Street Character

The Mid-Block Streets are potential secondary public or private streets that parallel Main Street and break the Couplet District into smaller walkable blocks that encourage the “park-once” concept, creating a pedestrian friendly core. Parallel parking may be considered for easy access to restaurants and retail.

57

**Encourage architecture that fronts the street edge (couplet street)**



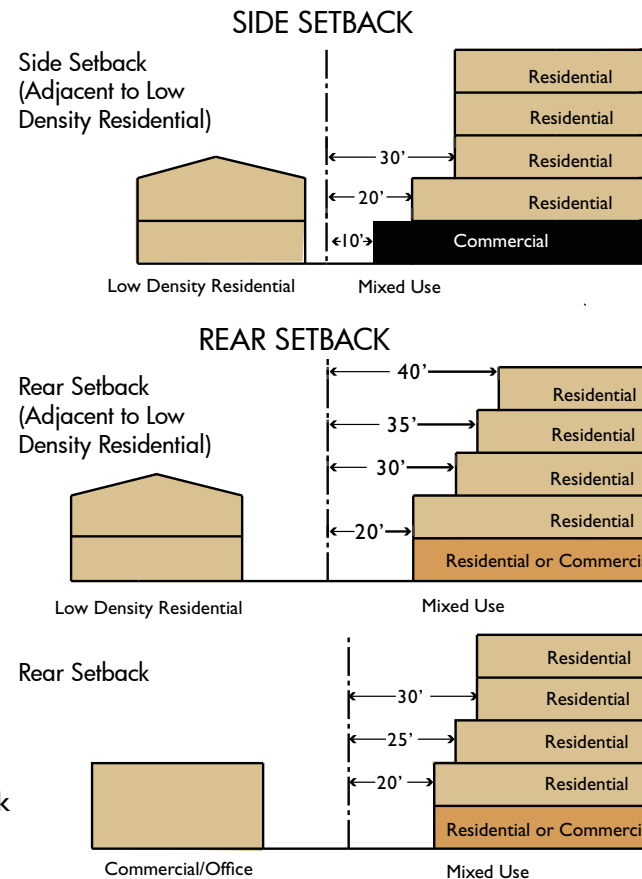
ack

### 4.4.4. Building Height

Integrate building mass with the adjacent neighborhoods through the use of compatible pattern, proportion, and scale transitions based on the following guidelines:

- Differentiate individual buildings along the street by variations in building height.
- Maintain a compatible relationship between adjacent buildings such that the lot and scale of the larger buildings are similar at the project edges.
- Transition from lower buildings to taller buildings in a thoughtful way considering the scale and massing transitions of adjacent buildings.

## BUILDING HEIGHT SETBACK



**Maintain a compatible relationship between adjacent disparate buildings types**

**FIGURE 5**

#### *Standards*

- Building heights shall conform to those set forth in the approved RidgeGate Planned Development District document (PDD).
- Along Main Street, provide a continuous building frontage such that 75% or more of the primary building facade is located within 10-feet of the property line of the street, (see Figure 3 and Figure 4).
- Building setbacks shall be proportional to the structure height. Architecture that is adjacent to an existing low density residential or commercial/office development may be up to 5 stories provided that the upper stories are stepped-back to minimize impact to lower density unit types, (see Figure 5).
- Applicants are responsible for providing any prior required notice to the Federal Aviation Administration (FAA) of the proposed construction or alteration of a structure that may constitute an obstruction to the safe navigation, landing, or takeoff of aircraft at a publicly used airport, as required by regulations at 14 C.F.R. Part 77 and as triggered by a structure that exceeds Notification Surfaces as shown on a map included in the RidgeGate East Technical Supplement. Copies of all written communications with and final determinations by the FAA shall be submitted to the Planning Division prior to approval of a plat or a Site Improvement Plan (SIP) application. Upon the issuance by the FAA of a determination of no hazard to air navigation which includes mitigation measures, the mitigation measures recommended by the FAA may be required. If an FAA 7460 Determination of No Hazard includes mitigation measures that result in significant and adverse changes to the airport's instrument landing system (ILS) or equivalent operations, then the project's plat or SIP may be denied by the City. Upon the issuance by the FAA of a determination of hazard to air navigation, the plat or SIP application shall be denied.

#### **4.4.5. Integration with Topography**

Integrate buildings and structures with the overall topography of the site, utilizing varied floor plans and product types to minimize the negative visual impact of walls.

- Design and ensure a building's mass and foundation follows the slope and grade of the site by stepping the building floor plan with the grade.
- Utilize terraced retaining walls to minimize the visual impact of large retaining walls to any public street.
- The minimum distance between the face of a retaining wall and a building foundation shall be 5-feet.
- Walls in excess of 8-feet should be avoided. Walls above 8-feet should be tiered with 2-foot minimum landscape terraces.
- Walls greater than 4-feet in total height may require a PE Sealed structural wall design. For such walls, contact the City of Lone Tree Engineering Division for additional information about wall policies and requirements.

#### *Standards*

- Walls must comply with the City of Lone Tree's policy and requirements.



**Design and ensure a building's mass and foundation follows the slope and grade of the site**

#### 4.4.6. Utilities and Equipment

Locate and design utilities and equipment to protect the visual quality of the community, based on the following guidelines:

- Minimize the visual impact of utilities on the streetscape and public open space areas.
- Consolidate utilities for multiple units and structures.
- Integrate and locate all mechanical and electrical equipment, and similar devices in the total design concept.
- Provide masonry walls, fences and/or landscaping of sufficient height and density to screen utilities.
- Screen or recess utility panels adjacent to a building facade inside the property instead of along the sidewalk, in front of the building, or within adjacent open space.
- Locate trash enclosures near building service entrances that are easily accessible by service vehicles.

##### *Standards*

- Equipment visible from public spaces must be screened. If equipment is consolidated into one large area, the area should be located out of a major traffic area and pedestrian zones.
- Rooftop mechanical, communications and service equipment including satellite dishes and vent pipes must be screened from ground level public view by parapets, walls, fences or other approved means.
- Design trash enclosures and utility and equipment screens to harmonize with the building's architecture in terms of materials, texture and color.

#### 4.4.7. Service and Delivery

Reduce the visual and functional impact of service areas on public roadways and spaces by providing off-street loading considering the following:

- Create a safe street environment with the provision of a “designated” delivery parking area on the side of the building or within a side alley.
- Share or co-locate service areas with adjacent buildings where possible.
- Locate and provide a buffer such as masonry walls, fences and/or landscaping of sufficient height and density for service, recycling, and delivery areas (minimum 6-foot high walls).
- Provide adequate loading and maneuvering space for trucks and other service vehicles.
- Look for opportunities to design service and delivery functions into the building architecture/structure.
- Designate parking spaces for delivery and service vehicles within private alleys where possible.

##### *Standards (Multifamily/Apartments)*

- Provide adequate space for on-street deliveries with a minimum dimension of 12' by 40' and to be free and clear of obstructions at all times.
- Provide a minimum dimensional standard for off-street loading/delivery space to have a clear horizontal dimension of 12-feet by 30-feet, exclusive of platforms and piers, and a clear vertical dimension of 14-feet. Combining loading facilities between adjacent buildings is encouraged (see Figure 6).
- Loading spaces shall abut the building being served.
- Delivery vehicles shall not block the project drive or alley.
- Vehicle Drop Off Zones are to be clearly marked with striping/deletion of parallel parking.

### Off-Street Loading/Delivery

Off-Street Loading/Delivery parking reduces congestion and conflicts between vehicular/bicycle traffic by reducing unwarranted stopping of loading and delivery trucks in the drive lane

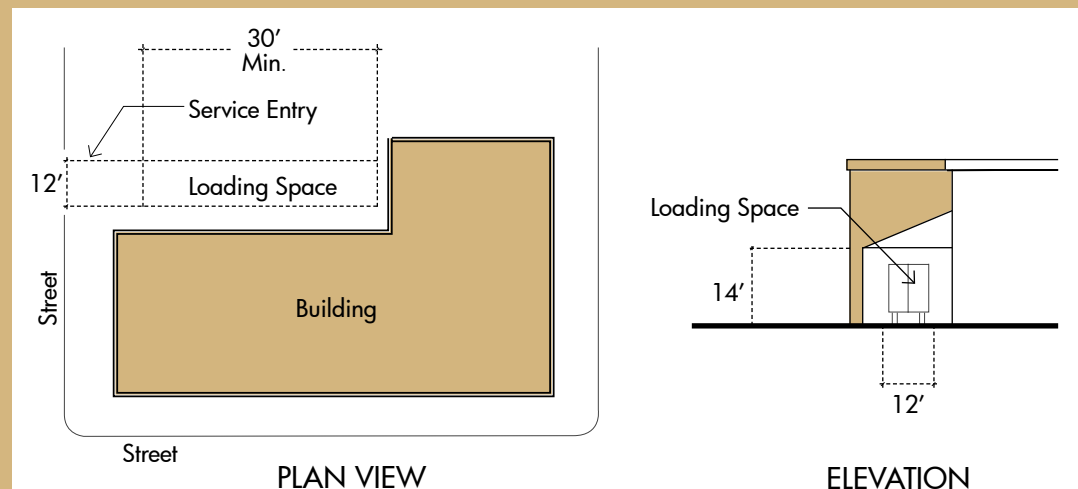


FIGURE 6



#### 4.4.8. Parking and Parking Requirements

##### A. Off-Street Parking (See Chart)

Provide adequate parking for residents and guests in a site-sensitive manner including the following:

- Provide pedestrian connections from the parking areas/lots to building entries, street crossings and open space connections.
- Design the site circulation to minimize the conflict between pedestrians and vehicles by orienting car parking spaces to minimize the number of traffic aisles that pedestrians must cross.
- Segment surface parking lots into smaller areas by using landscaped medians where appropriate. Planted medians in parking fields should accommodate an 8-foot wide landscape strip.
- Ensure proper car overhang space exists in the landscaped areas.
- Limit curb cuts so that there are fewer places where cars are crossing the sidewalk.
- Parking lots should be predominantly located behind buildings and not visually prominent along streets.

##### *Standards*

- Surface parking lot bays shall extend no more than 15 parking spaces without an intervening canopy tree(s) in an interior landscape island or landscape peninsula.
- Landscaped parking islands shall be provided at the ends of on-grade parking bays and should be a minimum of 8-foot in width from back-of-curb to back-of-curb. Landscaping is designed as trees, and vegetation with decorate gravel and wooden mulch.

##### B. Snow Storage

- Accommodate and locate snow storage in areas that do not conflict with pedestrian plazas, walkways and building entrances.
- Locate snow storage and disposal areas where snowmelt can infiltrate into the ground, filter through a vegetated buffer or be directed to stormwater treatment areas.
- Snow shall not be plowed into streams or wetlands.
- Parking spaces may not be used for snow storage, except in cases where the parking spaces provided exceed the required minimum.
- On-site snow may not be plowed into, nor stored, within the public ROW.

Minimum Off-Street Parking Ratios	
Use	Minimum*
Commercial	General Retail/Shopping Centers: - 3 per 1000 sf
Office	- 3 per 1000 sf
Restaurant	- 10 per 1000 sf of internal floor area, plus 1 space per employee on maximum shift, plus .5 space per 100 sq. ft. of outside seating area
Multifamily	- 1.5 spaces per unit plus .25 spaces per unit for guest parking

\*Defer to the City of Lone Tree's Municipal Code for uses not listed.

\*For maximum parking requirements, defer to the City of Lone Tree's Municipal Code.

### C. On-Street Parking

Create the type of environment where it is easy for people to walk between destinations through good urban design and parking policies using the following:

- Provide on-street parking to improve access to nearby land uses, create a buffer zone between pedestrians and vehicles, and reduce traffic speeds by narrowing the perceived travel way.
- Maximize on-street parking and explore alternative parking strategies (meters, time limits, etc).
- On-street parking immediately adjacent to the project may be considered in satisfying the overall project parking requirement.
- Incorporate vehicle 'drop-off zones' where appropriate.

### D. Parking Structures

Provide safe and efficient vehicular structured parking while minimizing the negative visual impacts commonly associated with stacking by incorporating the following:

- Avoid parking structures dominating the streetscape and public realm by locating them behind the building or a landscape wall and/or vegetative screen.
- Utilize structured parking where practical to consolidate parking and create a more compact, walkable environment.
- Parking structures, where utilized, shall be compatible in massing, scale, and materials with the buildings they support or surround.
- Incorporate bicycle and motorcycle parking in garages.
- Include vehicle charging stations in garages.

### E. Shared Parking

Create a walking environment that is both pleasant and interesting to encourage customers to park once and make several trips without moving their cars and support walkable retail street concepts.

- Vary the mix of uses to promote shared parking between uses that experience peak parking at different times of the day
- Allow developers to count spaces provided by another nearby land use towards their minimum parking requirements if the two uses have different peak usage times.
- Reduce land devoted to parking through shared parking incentives and agreements.
- Increase communication and coordination between individual businesses, business areas, and neighborhood residents by developing a comprehensive parking management strategy.



Provide a central pedestrian corridor to create a pedestrian friendly environment



Encourage the "park once" concept along mid-block street



Clearly create an outdoor pedestrian space and connections at building entries



Incorporate entrances to parking structures into the building design and minimize impact to the pedestrian realm

#### 4.4.9. Monument Signs

Create a sense of place through an organized and interrelated set of community identification elements to include high-quality signs, sign structures, lighting, and graphics.

- Create an organized and interrelated system of quality signs, sign structures, lighting and graphics that are compatible with the architectural character of the Commercial Area.
- Signs should not overlap and conceal architectural elements.
- Refer to The City of Lone Tree Sign Code and Design Guidelines for requirements with respect to the size, number, and design of signs.

##### *Standards*

- All signs are subject to Sign Standards in Article XXIX of the Lone Tree Zoning Code, as amended, until such time as an alternative Sign Plan for RidgeGate East is developed and approved by the City. A sign permit is required from the City of Lone Tree Building Division for all new signs and modification to existing signs.
- Signs throughout RidgeGate East shall follow the Wayfinding Sign Program to be developed.



Create an organized interrelated system of quality signs



Encourage a variety of signs, materials, and graphic elements that are compatible with the Village character

62



Establish a hierarchy of sign sizes and types

# ARCHITECTURE GUIDELINES

## 4.5. Architecture Guidelines

The vision is to provide human scale in the architecture through change, contrast and intricacy of facade form, color and/or materials of structures that face public streets and sidewalks. The aesthetic will evolve and change as different needs, programs, and uses dictate.

### 4.5.1. Architectural Image

The Architectural Image focuses on the design, image and function of various building types that include a wide variety of land uses and densities within the Central Village.

### 4.5.2. Massing and Scale

#### A. Overview

Incorporate architectural massing that reduces buildings into a series of scale-giving elements and include the following:

- Provide a variety of building types and massing, including architectural form and detail utilizing human-scaled building components at ground level.
- Design building masses to frame and define the streetscape.
- Create orderly, rhythmic, and proportional building masses which unify the building's form. Excessive, overly articulated, or fragmented building masses are discouraged.
- Accentuate building corners, vertically and horizontally, creating focal points to terminate vistas and emphasizes key destinations.
- Express the building's structural elements, such as floors (banding, belt courses, etc.), columns (pilasters, piers, quoins, etc.), and foundations (masonry wainscot).

- Create patterns of window and door openings that emphasize a change of wall plane material, color, texture, or pattern and other scale providing elements.
- Changes of materials, color, texture, must be based on changes in wall plane.
- Design building elements (recessed windows, exposed columns, reveals, entry recesses, etc.) to create interesting visual patterns and rhythms.

#### B. Mixed-Use

Utilize the building massing and scale to support lively and inviting public spaces with high quality design by incorporating the following:

- Create a vibrant urban environment utilizing structures with unique architecture and block cohesiveness.
- Celebrate the building corners by increasing building mass through the use of towers and turrets designed to reflect a higher intensity of activity.
- Provide a strong ground-floor building form designed to anchor the building to the ground and distinguish it from the building's upper story.

#### C. Large Format Commercial/Retail

Create large format buildings and accessory structures that reflect the architectural character of the overall project by incorporating the following:

- Reduce the apparent mass and bulk of large format retail buildings by articulating the building facades to increase variety and visual interest.
- Use smaller structural bays to break-up larger building masses designed to reduce perceived scale.
- Use architectural elements such as roof eaves, cornice elements, material bands, and consistent window rhythms to distinguish the top and bottom of the building.

63

**Incorporate outdoor seating and site furniture to encourage pedestrian activity in the Couplet District**



**Incorporate architectural features that reduce buildings into a series of scale-giving elements**

- Consider vertical breaks, changes in material, or changes in wall plane to emphasize the corner conditions.
- Rest the building on a discernible base or pedestal designed to anchor the building to the ground.

#### 4.5.3. Facade Articulation

Articulate building facades that help pedestrians establish a sense of scale by expressing building structure and individual floors.

- Articulate the facade with well-defined building entrances, including projecting and recessed facade features that will establish a rhythm and add visual interest to the block face.
- Primary entrances into buildings shall be identified through elements and detailing that orient to streets, plazas, public drop-off areas or public spaces.
- Promote facade articulation by segmenting building walls into a series of individual structural bays to create visual rhythms.
- Provide strong columns, piers, and posts rather than thin, flimsy, weak-appearing supports.
- Express the horizontal position of each floor in the upper-story facade of a building using the following:
  - Change in material
  - Reveal or recess
  - Masonry belt courses
  - Repetitive bands of individual recessed window openings
  - Continuous cornice element

#### *Mixed-Use (Ground-floor Retail):*

- Create retail storefronts designed to optimize interior daylighting while encouraging window shopping and patronage.
- Segment storefronts through the use of vertically repeating architectural elements and facade articulation techniques that include the following:
  - Columns
  - Punctuating building corners with material changes
  - Projecting trellis elements or colonnades
  - Storefront windows
  - Wall plane projections or recesses

#### *Standards*

- Ground floor main entry facades that face public streets, drives, or plazas shall have arcades, display windows with vision glass, back-lit glazing elements, entry elements, awnings, or canopies along no less than 60 percent of the building linear frontage.
- Provide the minimum traditional storefront transparency based upon the following standards:
  - 70 Percent Transparent Material
  - 30 Percent Solid

#### *Large Format, Free-Standing Commercial Buildings:*

- Articulate Large Format building elevations to increase facade variety and visual interest.
- Use projections, recesses, arcades, awnings, color and texture to reduce the visual size of any unglazed walls.

### Facade Articulation

Provide a high degree of transparency at the ground level that will establish a rhythm and add visual interest to the block face.



### Standards

- Incorporate architectural features and building elements that are consistent with the primary building facade for all sides and rears of buildings over 75-feet in length and visible from any public street or common area. (Typical architectural features are included below).
- Architectural features shall include towers, entry elements, brick, or cast stone colonnades with trellises, pilasters, towers, flat metal canopies, awnings, projected brick elements, vision glass, display windows, back-lit frosted glass elements, or an expression of architectural bays where areas between columns are indented.

#### A. Windows and Doors

- Locate windows and doors at the lower levels of building facades to enhance building function, form and scale and to create a lively streetscape.
- Encourage a complementary window style and patterns on all side of the structure.
- Contemporary designs may consider using unique horizontal window patterns and locations.

#### B. Awnings and Balconies

Utilize awnings and balconies to add dimension and interest to the streetscape.

- Provide balconies to add habitable outdoor space for multifamily.
- Use awnings to help energy efficiency, protect pedestrians from the elements, and provide shade for ground-level uses.
- Design awnings to complement the architectural style of the building.

### Standards

- Do not use continuous awnings. Awnings shall be segmented conforming to structural bays.
- Avoid building features that encroach into the public right of way such as awnings, roof overhangs, balconies and patios. If, and when, allowed, a license agreement request must be submitted as part of the SIP process. Such features must maintain ADA accessibility and retain a minimum 8-foot clearance above the adjacent ground surface or walk.

#### 4.5.4. Building Materials and Color

Select building materials with the objectives of high quality and durability. Utilize accent colors with the intent to complement, enhance, and provide a balanced context within the mixed use project.

- Select quality and durable building materials that are complementary to the neighborhood and architectural style.
- Use a color palette to delineate color ranges and combinations that reflect or complement surrounding area.
- Create interesting and expressive wall surfaces by the careful articulation of wall surfaces, textures, and application of color.
- The use of stucco is discouraged within 32-inches of the ground.

#### A. Wall Materials

Design commercial buildings using the following material options:

- Use durable, urban oriented building materials. Wood shall only be used as an accent material for minor architectural elements, such as corbels, brackets, and window shutters.
- Select durable building materials that will resist the harsh Colorado elements and the effects of aging.
- Use building materials with strong textures and rich colors.

## Materials and Colors

All accent colors are intended to complement, enhance, and provide a balanced context within the mixed-use commercial areas.



- Avoid building materials that may streak, fade, stain, mildew, attract dirt, or generate glare.

#### *Standards*

- Artificial veneer stone is prohibited.

### **B. Masonry Requirements**

- Brick, Natural Stone, Cast Stone, Integrally Colored Split or Burnished Face Concrete Masonry Units (CMU), Portland Cement Plaster (Stucco) with Acrylic Finish Coat.
- Decorative concrete, using form-liners, should be discouraged.

#### *Standards*

- Exposed concrete foundation walls are prohibited.
- Exterior Insulation and Finish System (EIFS) is prohibited.

### **C. Wall Color**

- Finish buildings with paint colors that reflect the architectural style of the structure.
- Relate color variety to changes of materials, such as building base, facade, and roof.
- Finish contemporary structures with compatible colors that reflect modern architectural styles
- Compatible, bright colors will be considered for accents.
- Encourage warm earth tone colors that reflect the surrounding natural landscape.

#### **4.5.5. Roof Form and Materials**

Create consistent building roof forms that reflect the architectural style of the surrounding context.

- Use a variety of roof planes or breaks along street facades with articulated and varied roof forms.
- Design roof forms to correspond to building functions, including pitched roof forms to identify and accentuate building entrances and staircases.

- Create roof pitches and overhangs which complement the architectural style of the building.
- Integrate roof gutters and downspouts into the design of the building, appearing as a continuous architectural element.
- Use a consistent roof pitch for all pitched roof forms.
- Use roof materials that are complementary to the architectural style of the building.
- Create roof forms that respond to Colorado's unique climate. Use pitched roof forms or large flat roof overhangs to shed winter snow, provide summer shade, and shelter pedestrians from the elements

#### *Standards*

- Roof materials may include ceramic, slate, concrete tile, laminated multiple-ply composition shingles, metal seam or other materials. Wood shake roofs are prohibited. Other roof materials may be allowed on a case-by-case basis
- Metal seam roofs must have integral color that is compatible with the facade.
- Colors should reflect or complement the surrounding natural environment.
- Roofs shall not be reflective.
- More contemporary roof types/styles shall be reviewed on a case-by-case basis such as: Flat, Half Vaults, and Sheds.
- Terminate the top of flat-roofed buildings with a distinctive cap, including a distinctive cornice or parapet wall.
- Screen roof-top mechanical, electrical and telecommunications systems from surrounding streets and structures, regardless of location.

#### **4.5.6. Lighting**

Provide light fixtures that are designed to respect, enhance, and contribute to the architectural style, detailing, and elements of a building.

**Create consistent building roof forms that reflect the architectural style of the surrounding context**



**Select lighting fixtures that complement and enhance the architecture of the surrounding buildings**

- Provide front porch wall lanterns or pendent lights for residential that reflect the architectural style of the home.
- Provide shielded or cut-off lighting fixtures designed to prevent nuisance glare.
- Complement the architectural style with appropriate fixtures.
- Use appropriate site and building lighting to highlight the architecture while providing safe and vibrant environments compatible with the streetscape.
- Create safe and attractive environments at night while minimizing light pollution and glare; strategies such as down-lighting are encouraged.
- Use controls, daylight sensors and/or timers to control exterior architectural lighting.
- Use energy efficient lighting such as high- efficiency LED, smart sensors and low-level lights.

#### *Standards*

- Accent and/or decorative lighting consistent with the architectural character of the project.
- LED/solar/smart controllers are required.
- The height should be based on the area of illumination and reduce light trespass, and light pollution. Maintain a maximum height of 25-feet.
- Anchors and Shops: One light for every 60-foot of curb frontage, average.
- Pad Buildings: One light for every 45-foot of frontage at pad entry facade. (A minimum of two lights shall be provided regardless of length of frontage).

#### **B. Large Format Retail/Commercial**

Create a strong image and reduce visual clutter. Provide a solid base for all freestanding signs.

- Provide a solid base for all freestanding signs.
- Design signs to be in character with the surrounding architecture.

#### *Standards*

- All signage is subject to Sign Standards in Article XXIX of the Lone Tree Zoning Code, as amended, until such time as an alternative Signage Plan for RidgeGate East is developed and approved by the City. A sign permit is required from the City of Lone Tree Building Division for all new signage and modification to existing signage.
- Signage throughout RidgeGate East shall follow the Wayfinding Signage Program to be developed.



**Design wall mounted signs that integrate with the second story architecture**



**Integrate signs into the overall architectural features of the facade in a way that complements the building's architecture**

#### **4.5.7. Building Signs**

##### **A. Mixed-Use Commercial**

Create a lively shopping environment by selecting signs that fit within the architectural features of the facade and complement the architectural image by incorporating the following:

- Use window and awning signs that complement storefront architecture.
- Building wall-mounted signs shall be placed between the first and second floors.
- Design signs so they do not overlap or conceal architectural elements.
- Balance auto-oriented signs with the pedestrian-oriented signs through an established hierarchy of types.



# PROJECT LANDSCAPE GUIDELINES

## 4.6. Landscape Guidelines

The project Landscape Guidelines are tailored to addressing issues primarily related to on-site landscapes, designed to soften building architecture, soften and screen large expanses of pavement and frame and enclose formal open space.

### 4.6.1. Landscape Character/Identity

Create a distinct landscape character that creates identity, adds value, and is water-efficient to include the following:

- Utilize street trees to shade sidewalks and soften building architecture, creating an attractive and pleasant pedestrian-oriented environment.
- Provide distinct landscaped pedestrian corridors and walks that extend the character of the architecture into the parking areas, creating safe pedestrian plazas and islands that connect to the building entrance.
- Landscape project edges to complement natural landscape, when adjacent to existing natural open space.
- Select planting compositions and types that are appropriate to the climatic and environmental conditions of the site.
- Create landscape buffers designed to screen and soften side and rear building elevations where visible from a public street.
- Utilize trees to frame and enclose interior-oriented parking courts.
- Utilize landscape medians and islands to segment large parking area into a series of “outdoor rooms.”
- Select plant material that can withstand urban environments, shade and shadow, heat index and the effect of snow removal/storage strategies along roadways.
- Landscape the area in front of a blank wall that faces public streets.
- Reference the City of Lone Tree’s approved plant/tree list.

#### *Standards*

Landscaping of parking lots within the underground Xcel Gas Easement will be required to file a Landscape Encroachment Application and comply with the requirements of Xcel Energy.

### 4.6.2. Irrigation

RidgeGate is intended to be developed in a manner which is responsive to many environmental considerations, one of the most is water conservation through the following:

- Landscape areas with drought-tolerant vegetation that is suitable to the local climate,, requires low amounts of water, and is designed with Xeric principles.
- Limit higher intensity irrigation and landscape areas to prominent and high traffic areas.
- Limit higher intensity irrigation and landscape areas designed to accent active spaces and semi-private spaces.
- Select plants with similar water requirements to be planted together.
- Ensure adequate watering for the establishment of all vegetation, including native plant material.
- Utilize in-ground sprinkler systems including drip or other low-volume application systems controlled by automatic computerized clocks.
- Minimize the use of spray irrigation directly around the perimeters of buildings and foundations.

#### *Standards*

- Utilize drip/low-volume application systems controlled by controllers linked to rain sensors. One rain sensor per two controllers is required.
- Irrigation shall not “over-spray” to impervious areas including sidewalks and roadways.

### 4.6.3. Fences and Walls

Design fences and walls that are consistent with building architecture, including color and material that enhance and complement streetscape, open space, and public outdoor areas by incorporating the following:

- Design parking lot screen walls that harmonize and complement building architecture and that are richly integrated with the landscape character.
- Integrate wall color, material, and design to complement the natural environment or building architecture.
- Punctuate the wall-line with appropriate pilasters, pillars, columns, and other elements.
- Provide higher quality design and materiality for higher visibility walls and fencing along public streets and open space areas.
- Create attractive retaining walls to avoid excessive slopes and limit stormwater disturbance. Walls should not exceed 8-foot height, or should be terraced into multiple walls.

- Construct retaining walls with materials that blend with the natural environments, and complement the surrounding architecture.
- Incorporate landscaping within close proximity of fences and walls for screening and softening.

*Standards*

- The finished side of all fencing which is visible from a public area or right-of-way will face outward.
- Standard unfinished concrete block and unfinished concrete are not allowed.

**4.6.4. Site Furnishings**

Select site furnishings, including trash receptacles, benches, bike racks and planters, that are visually attractive and coordinated to create a sense of character and identity for the project. Utilize site furnishings to create human scale along the sidewalk areas and associated public spaces.

- Select site furnishings that complement the design of the adjacent architecture and streetscape.
- Select site furnishings that are durable for urban areas and that are comfortable for people to use.
- Group street furnishings at strategic locations including, corners and near retail areas to create an inviting place to sit and socialize.
- Locate benches in areas that receive direct sunlight in winter and provide shelter from the winds and are shaded in summer.
- Provide bicycle parking in visible locations near building entrances and pedestrian walkways. Ensure that these locations do not conflict with pedestrian circulation.
- Use decorative tree guards, where appropriate, to give added protection to young trees.
- Charging stations should be encouraged in parking areas

*Standards*

- All site furnishings must be easy to maintain and vandal-resistant.

**4.6.5. Lighting**

Utilize lighting to foster safe environments, including:

- Meet illumination and coverage requirements to ensure safe and efficient lighting.
- Reduce light pollution. Light should not cast off-site.
- Focus all lighting downward or narrowly focused on its intended target such as signs, parking and pedestrian walkways.
- Utilize lighting products that reflect the character of the development.
- Parking lots will be appropriately lit and consistent with Lone Tree lighting and illumination standards.



**Select site furnishings that are coordinated and create a sense of character and identity for the Couplet District**



**Provide bicycle parking in visible locations near building entrances and pedestrian walkways**



**Use lighting to foster safe pedestrian environment**

#### 4.6.6. Public Art

Where public art is included in the project, the following guidelines shall apply:

- Locate art in a way that considers relationship to architectural and natural features, landscape design, and public common areas.
- Consider the social context and other uses of the artwork or space.
- Ensure the siting of each art piece considers public safety, liability issues, vehicular, and pedestrian traffic patterns.



Utilize public art to enhance streetscape and high visibility common areas

#### 4.6.7. Street Tree Planting Areas

For street trees located in urban hardscape areas, provide a minimum of a 5' x 8' wide openings around the tree well.

- Utilize recycled plastic tree grates, paver/brick surrounds with porous fill, ground covers and or raised planters in tree pit area.



Raised landscape planter

#### Standards

- Cast iron trees grates are not allowed in the public right-of-way per the City of Lone Tree's Landscape Design Guidelines .
- Refer to the City of Lone Tree's Landscape Design Guidelines for Public Rights-of-Way for details.

Metal tree guard



Landscaped tree well



Recycled plastic tree grate



A blue-tinted photograph of a landscape. In the foreground, there is a grassy slope. In the middle ground, a road with white lane markings runs horizontally. To the right of the road, a single tree stands. In the background, a utility pole is visible, and the horizon shows a flat landscape with some distant structures.

PART FIVE

MISCELLANEOUS  
GUIDELINES/STANDARDS

# MISCELLANEOUS GUIDELINES/STANDARDS

## 5.1. Attainable Housing Units (AHU)

The shared intent of the City and the developer is to provide for attainable housing units, or AHUs, that provide opportunities for people who work in the area to also live there, and to meet a need for affordable housing. Design of AHUs will be compatible with the character and context of the area in which they are located. The City and RidgeGate have agreed to AHUs as described in the Amended and Restated Annexation and Development Agreement With Respect to the East Side Property, and further details will be provided in a mutually agreed upon Attainable Housing Plan.

## 5.2. Universal Design

Universal design refers to broad-spectrum ideas meant to produce buildings, products and environments that are inherently accessible to older people, people with or without disabilities. An appropriately designed entrance to a building that allows all people to enter the building in the same way has a minimally sloped entrance walk, a landing for transitioning from outdoors to indoors.

- Sidewalks: Walks and trails should be a minimum of 5-foot wide and have smooth, level walking surfaces to provide easy access.
- Universal Routes: Project is designed so that all users follow the same routes through the site.

## 5.3. Centennial Airport Noise and Height Requirements

Applicants are responsible for providing any prior required notice to the Federal Aviation Administration (FAA) of the proposed construction or alteration of a structure that may constitute an obstruction to the safe navigation, landing, or takeoff of aircraft at a publicly used airport, as required by regulations at 14 C.F.R. Part 77 and as triggered by a structure that exceeds Notification Surfaces as shown on a map included in the RidgeGate East Technical Supplement. Copies of all written communications with and final determinations by the FAA shall be submitted to the Planning Division prior to approval of a plat or a Site Improvement Plan (SIP) application. Upon the issuance by the FAA of a determination of no hazard to air navigation which includes mitigation measures, the mitigation measures recommended by the FAA may be required. If an FAA 7460 Determination of No Hazard includes mitigation measures that result in significant and adverse changes to the airport's instrument landing system (ILS) or equivalent operations, then the project's plat or SIP may be denied by the City. Upon the issuance by the FAA of a determination of hazard to air navigation, the SIP or plat application shall be denied.

For all residential units and other noise sensitive development to be constructed in the Centennial Airport Influence Area (AIA), the builder shall disclose to all prospective purchasers in writing, using the Centennial Airport Overflight Disclosure form, that the project is located in an area that could be subjected to numerous aircraft overflights and their associated effects. These effects include, but are not limited to: noise, smoke, dust, fumes and vibrations. Such disclosure shall also be noted on Plats and Site Improvement Plans (SIP's) and in sales contracts and closing documents.

## 5.4. Crime Prevention Through Environment Design (CPTED)

A balance between the land use mix and a cohesive site design is important for environmental, economic, aesthetic and safety reasons. The East Villages Land Use Plan sets up a development framework to which CPTED principles will apply. Based on the four identified principles within the CPTED, the design and character throughout the community will be a consideration in the Project Site Planning, Architecture and Landscape Guidelines set forth in the Sub-Area Plans. The following principles are to be utilized throughout the development of each neighborhood in order to provide a safe and secure built environment:

### A. Natural Surveillance “Eyes on the Street”

- Channel the flow of activities to put more people (observers) near potential crime areas.
- Improve sight lines from within and around built structures through the use of windows and lighting.

### B. Natural Access Control

- Properly locate entrances, exits, fencing, landscaping and lighting to discourage criminal activity.

### C. Territorial Reinforcement

- Create clear boundaries between private and public space through the use of physical elements such as fences, pavement treatment, art, and signs, etc.
- Create spaces that are clearly defined and have direct site lines and transparency into the area.

### D. Maintenance and Management

- Develop a strong sense of the identity and image of the community that creates a sense of ownership and pride.
- Clearly define public, semi-public, and private areas with spatial boundaries.
- Select materials and finishes that can be sustained over time. For example, use plant materials that at full maturity will not block sight lines.

## 5.5. Accessory Dwelling Units (ADUs)

Accessory Dwelling Units (ADUs) are independent housing units within a home (such as a basement), attached to a home, or above a detached garage. ADUs proposed as detached accessory cottages are not permitted. ADU Framework and Requirements

- **Use.** Encourage ADUs on single-family detached residential lots where they meet the minimum requirements below. ADUs may be incorporated into a new or an existing dwelling. ADUs above a detached garage must be built at the same time as the garage. ADUs would not be allowed on residential lots for attached development. This includes townhomes, apartments, or condos (unless the ADU is above a detached garage), as there would likely be insufficient parking, infrastructure, yard or unit space to accommodate ADUs; the unit configuration makes it more difficult to build ADUs in these developments; and building requirements, such as firewalls may preclude such.
- **Density addressed by size.** To help maintain the residential character of the neighborhood:
  - » Size - maximum of 600 sq. ft.
  - » Number allowed - maximum of 1 ADU per
  - » Bedrooms – maximum of 2 bedrooms
- **Parking.** ADUs shall not result in excess automobiles, defined as any vehicle or vehicles owned or operated by members of a household that cannot be accommodated on the dwelling unit's driveway or parked in a common lot. A minimum of 3 total off-street parking spaces per lot shall be provided, and the spaces must meet minimum side setbacks of 5-feet.
- **Covenants.** Covenants for detached residential development should allow for ADUs.
- **Safety.** ADUs must meet building and fire codes.
- **Services.** ADUs shall be connected to the water and sewer utilities of the principal dwelling unit and may not have separate water and sewer service. Such service shall be deemed adequate by the service provider
- **Setbacks.** For the sake of good neighbor relations and appearance, 5-foot side and 15-foot rear yard setbacks would be established for attached ADUs.
- **Height Limits.** ADU shall have a maximum building height of 25-feet. The idea is that an apartment as an attachment to a house or above a garage should not loom over a neighbor's property.
- **Rental Term.** To realize the benefits of ADUs for providing affordable housing, ADUs shall not be used

for short-term rentals (less than 12 weeks).

- **Ownership.** The owner of the lot shall occupy either the principal unit or the ADU to help ensure the property is maintained. The ADU shall not be converted to condominiums.
- **Application Process.** The ADU application process is an administrative permit process with the City of Lone Tree, completed concurrent with building permit issuance, and may include, among other requirements, a requirement for a deed restriction.
- **Appearance.**
  - » The building design for attached ADUs and ADUs above garages, must be consistent with the principal dwelling unit. For instance, the roof pitch of the attached accessory dwelling, and the colors, trim, and the windows should be complementary with the primary building.
  - » A secondary entrance shall not be added in the front of the main house; no stairs to the ADU in the front yard of the primary building.
  - » Any exterior stairs to a detached garage shall run parallel to the garage.



**ADUs should be designed to be consistent with the principal dwelling unit**



**ADU's shall be a good neighbor by meeting minimum setback and off-street parking**



**Page intentionally left blank.**



# APPENDIX

MAXI  
GRILLE  
←  
Serving  
Lunch & Dinner  
M-F 11:00am - 2:00pm M-S 12:00pm - 10:00pm  
1000 N. 1st St. Tallahassee, FL 32301  
Phone: 904-224-1111



# GLOSSARY

**Arcade Building:** is a covered walkway supported by a line of columns and arches on one or both both sides.

**Block Face Configuration:** street frontage that utilizes buildings, site walls or landscape to provide a “continuous” or semi-continuous edge along the lot-line.

**Block:** the aggregate of private lots, passages, rear lanes and alleys, circumscribed by public streets.

**Building Face:** an exterior building wall. For the purposes of this document, it is one which is visible from a public street, alley, or public trail or open space and subject to the guidelines herein.

**Cycletrack:** an off-street, two-way traffic lane for bikes.

**Civic Building:** a building designed specifically for a civic function. Civic Buildings include buildings for not-for-profit organizations dedicated to arts, culture, education, recreation, government and transit. These buildings are encouraged to be high image buildings and may exceed the standards set out in the Commercial Mixed-Use Guidelines.

**Concrete Masonry Unit:** a standardized building block used for construction that is made from pre-cast concrete.

**Focal Architecture:** an architectural response to a prominent site location such as at the intersection corners. It may include centering a building on the axis view, or expressing an entry, roof or building wall to respond to that location.

**Floor Area Ratio (FAR):** a unit to measure density that is calculated by dividing the overall area of a building by the area of the parcel.

**Guidelines:** provide guidance as to how one may meet the design intents and establish provisions for specific design elements, components or topics related to site planning, architecture design and landscape design.

**Large Format Retail:** large, free-standing, cuboid, generally single-floor structure built on a concrete slab. Commercially, big-box stores can be broken down into two categories: general merchandise and specialty stores which specialize in goods within a specific range, such as hardware, books etc.

**Modified Urban Grid:** the development of an interconnected system of circulation elements between different destinations and uses that adapts to the sites characteristic topographical condition, emphasizing connections between neighborhoods and individual uses and creating a hierarchy of through-streets with shared access between land uses.

**Mid-Block Street:** the Central Village Couplet District development pattern is based on traditional urban characteristics with short block lengths and a system of highly connected thoroughfares, local streets and alleys that break the commercial core into smaller, walkable blocks.

**Mixed-Use:** areas with a mixture of uses, often within a single structure, that is typically urban in nature and supportive of a pedestrian supportive environment.

**Off-Street Loading/Delivery:** off-street loading/delivery parking is primarily located within the Central Village Couplet District and designates parking space for delivery and service vehicles within alleys (where possible) to eliminate “double-parking” of delivery vehicles within the traffic lane.

**One-Way Couplet:** the RidgeGate Parkway divides the six-lane thoroughfare into two, divided one-way couplets that allows the roads to have a narrower street width, which encourages slower traffic and creates a more pedestrian-friendly environment in the Central Village. The narrower streets are also more comfortable and safer for pedestrians to cross.

**Planning/Guiding Principles:** establish a framework to guide development of the RidgeGate East Villages/ Areas.

**Planned Development District (PDD):** a regulatory document adopted by the City of Lone Tree that was prepared to guide development for the entire 3,500 acre RidgeGate community.

**RidgeGate East Villages:** describes the four distinct neighborhoods that diverge and engage at the Central Village.

**Setback Line:** the “Setback Line” principles are focused primarily within the Central District as a line or “zone” in which a certain percentage of the building façades must be built. The Setback Line is measured from the property line adjacent to the public street right-of-way throughout the Central Village.

**Street Wall:** when buildings and street trees physically define the street corridor particularly through proportions between height and width in an area to create places that are comfortable to pedestrians.

**Smart Cities Technology:** information and communication technology (ICT) is used to enhance quality, performance and interactivity of urban services, to reduce costs and resource consumption and to improve contact between citizens and government.

**Site Improvement Plan (SIP):** as enabled by the City of Lone Tree Planning Division, an SIP is required for the development of new buildings and related site improvements and for exterior modifications to existing buildings and site improvements, as determined by the Director. SIPs are required for all single-family attached, multifamily and nonresidential development, and includes at a minimum: cover sheet, site plan, landscape plan, irrigation plan, grading plan, site furnishings plan, lighting plan, building elevations, drainage report and plan, erosion control study and plan, and materials/color sample board.

**Shared Parking Ratio:** the planning concept within the Central Village Couplet District allows for parking spaces that are available to more than one use or property. The shared parking ratio varies according to multiple functions in close proximity, which are unlikely to require the use of such spaces at the same time. The total parking requirement per project may be reduced by a certain factor through a shared use agreement and is shown as a calculation.

**Streetscape:** the urban element that establishes a major part of the public realm. The streetscape is composed of thoroughfares (travel lanes for vehicles and bicycles, parking lanes for cars, and sidewalks or paths for pedestrians) as well as the visible private frontages (building facades and elevations, porches, yards, fences, awnings, etc.), and the amenities of the public frontages (street trees and plantings, street furnishings, streetlights, etc.).

**Street Wall:** the front façade of a building where it is built on or close to the street boundary. It is an important urban design element because it defines the public realm along a street corridor.

**Shared Landscapes:** functional gathering spaces within neighborhoods, such as gardens, play areas, plazas, picnic areas and grouped mail boxes to foster community engagement.

**Universal Design:** the design and composition of the built environment so that it can be accessed, understood and used to the greatest extent possible by all people regardless of their age, size, ability or disability.

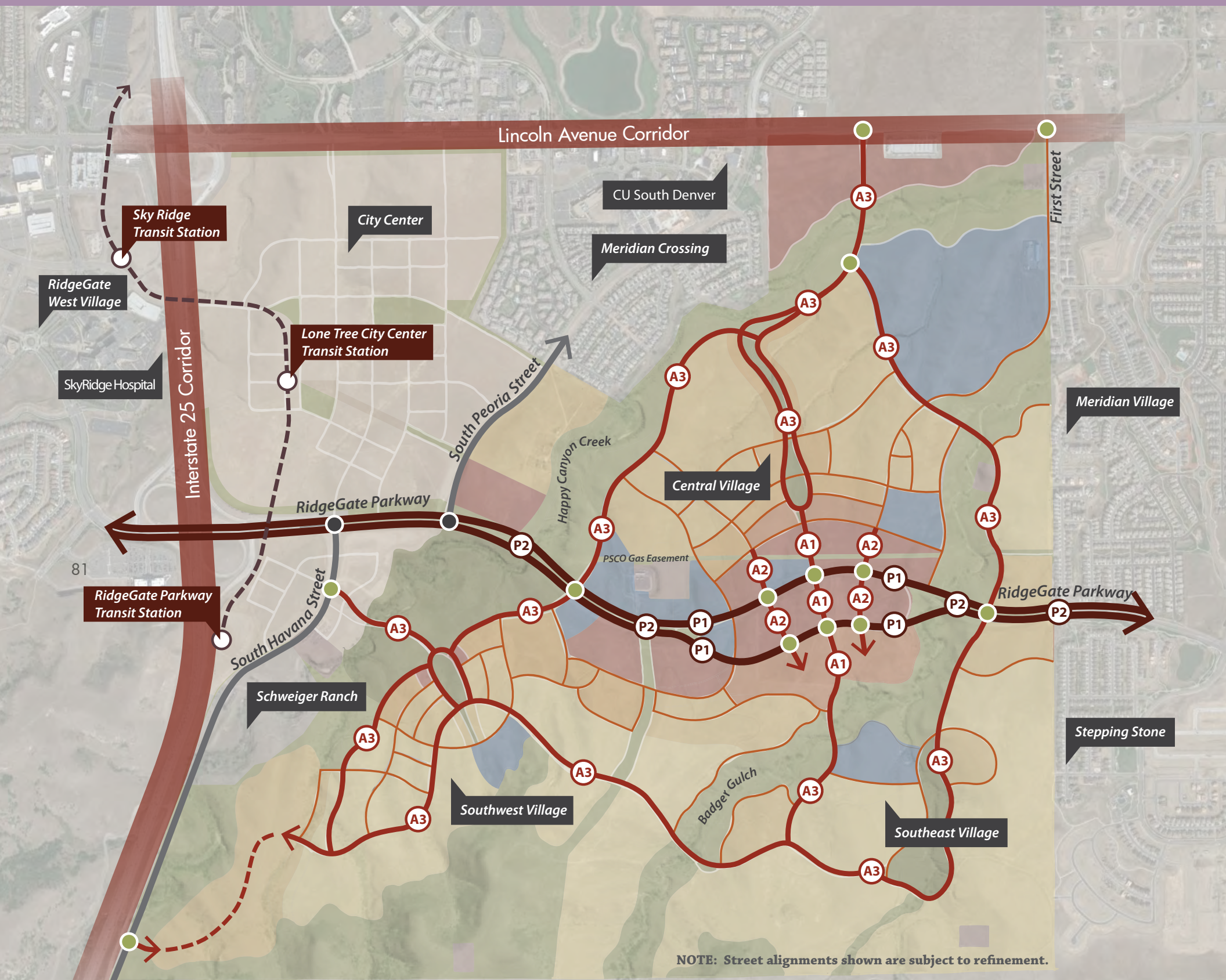
**Central Village:** mid-to-high density, mixed use commercial area located central to the overall community and served by RidgeGate Parkway.

**Village:** the development pattern for RidgeGate East has been organized into distinctive areas which are defined by natural open space corridors and existing road networks and are referred to as “Villages.”

**Village Green:** each Village contains a formalized, common open space, park and/or amenities that serve the adjacent neighborhood, typically at the center of the Village, within easy walking distance of most residences.

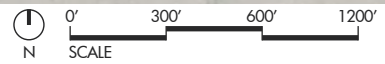
**Village Gateways:** the enhancement of various riparian corridors that currently transect the project site as significant “natural open space entrances” providing a transition between and into the Villages and Commercial Areas.

# CIRCULATION/TRANSPORTATION FRAMEWORK PLAN



## Transportation Framework Plan Legend

- |   |   |   |  |   |
|---|---|---|--|---|
|  Major Arterial & Couplet P1, P2     |  Residential Collector A1, A2, A3    |  Residential Local Street |  Existing Streets         |  Street Type See Street Sections |
|  Intersection Proposed Signalization |  Intersection Existing Signalization |  RTD Southeast Line       |  RTD Station with Parking |  RTD Station without Parking     |



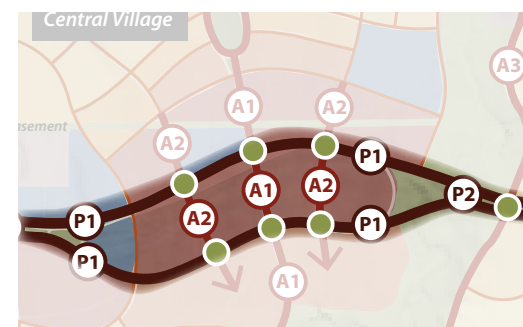
## Street Types and Character

Typical street prototypes and street classification were previously developed for the RidgeGate Community, including RidgeGate Roadway Standards and Guidelines, and the Walk and Wheel Report. Due to the nature of the RidgeGate Parkway design and couplet concept and other RidgeGate East planning concepts, the typical

sections illustrated within the Walk and Wheel Report were refined to meet the needs of the RidgeGate East Sub-Area planning concept. Refer to the following typical street prototypes, the RidgeGate East Technical Street Standards Supplement for more detail, and to the City of Lone Tree Landscape Design Guidelines for the Public ROW.



**P1 - RidgeGate Parkway (Couplet Street in Couplet District)**



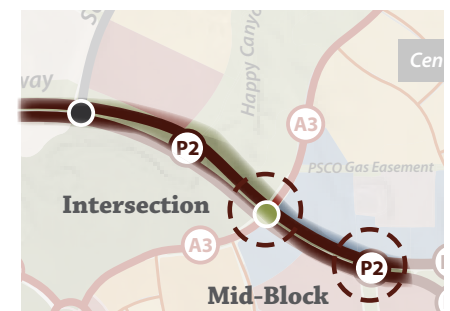
**P1 - RidgeGate Parkway**



83

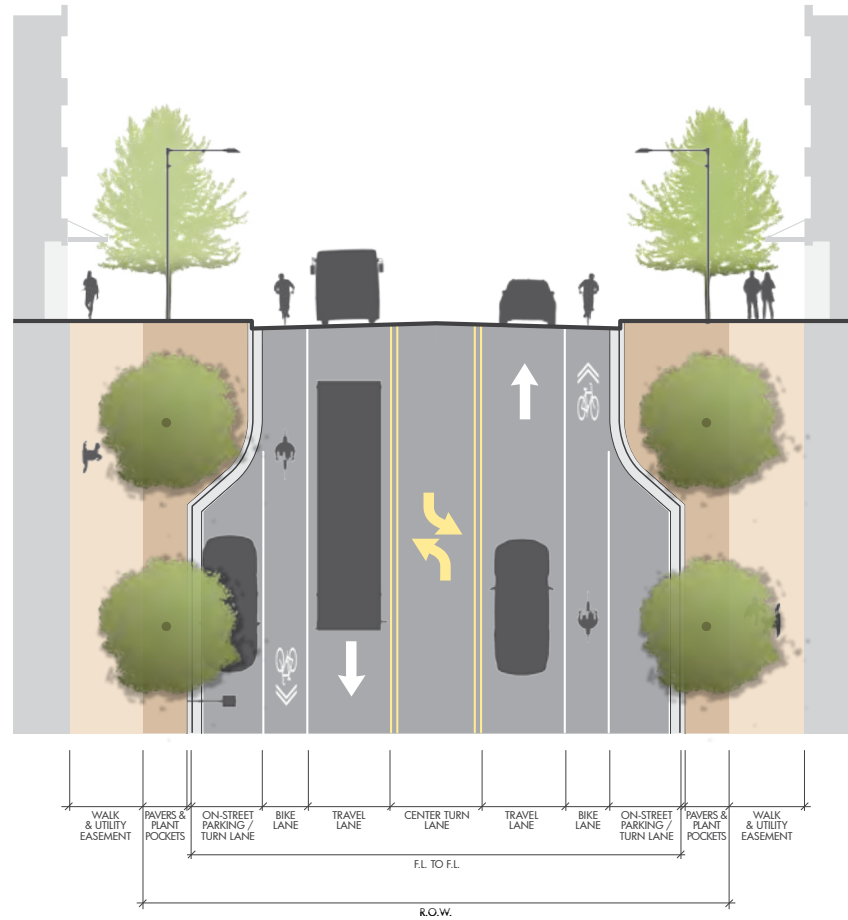
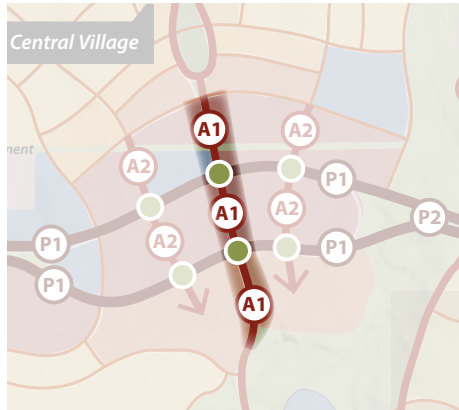


**P2 - RidgeGate Parkway**



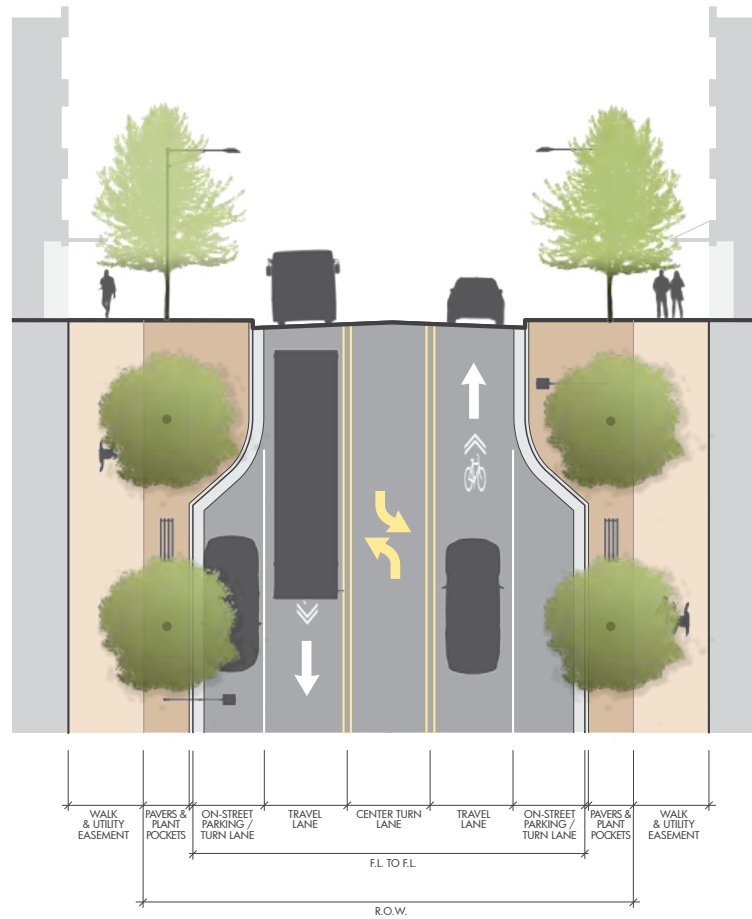
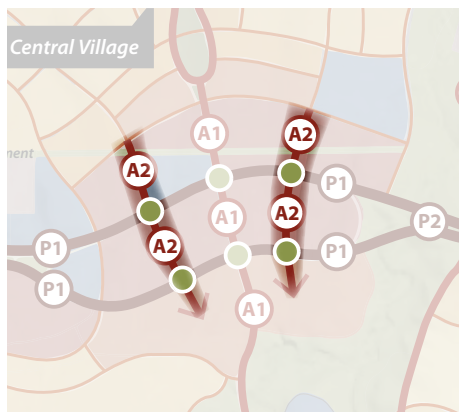


**A1 - Major Residential Collector**



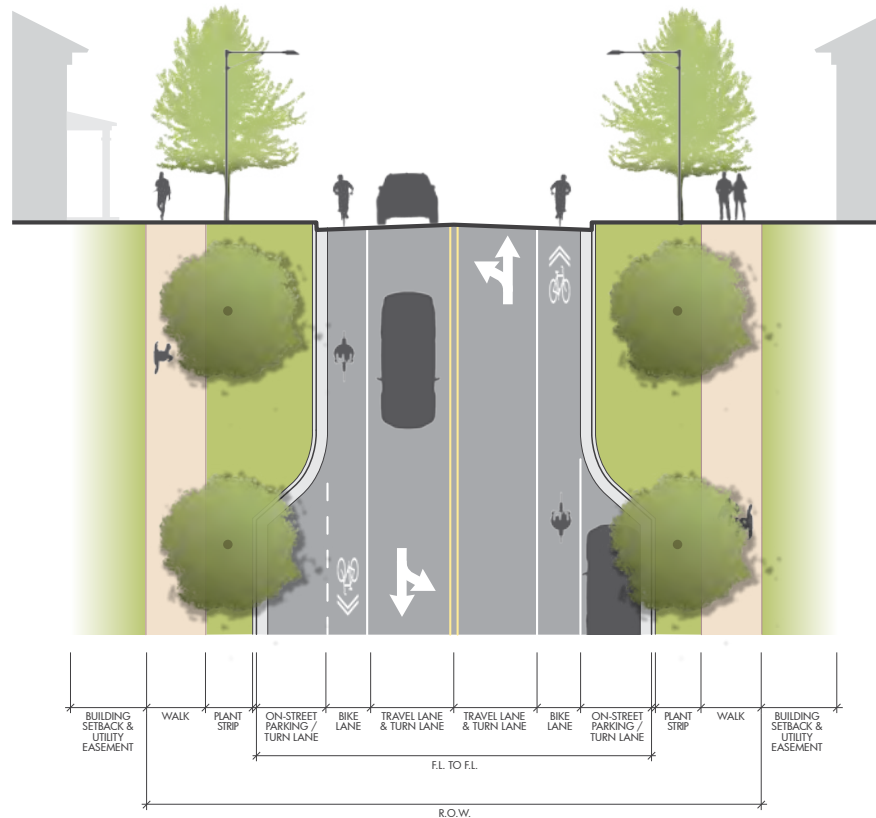
**A1 - Residential Collector (Main Street in the Couplet District)**

**A2 - Major Residential Collector Mid-Block Cross Street**



**A2 - Residential Collector (Mid-Block Cross Street in the Couplet District)**

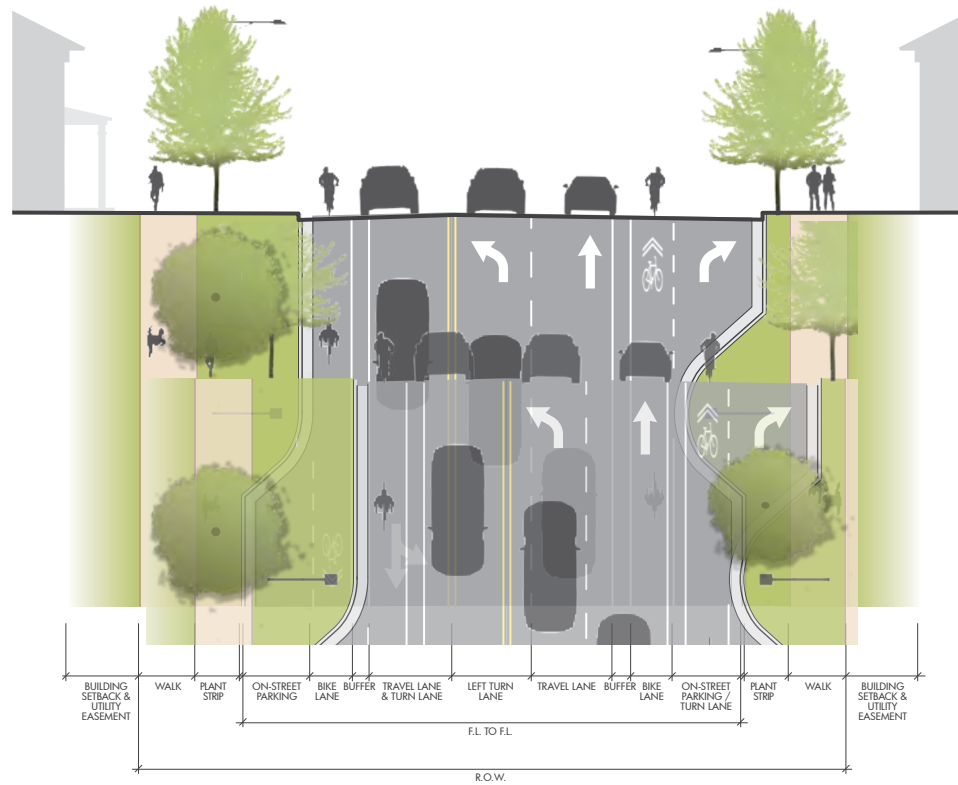
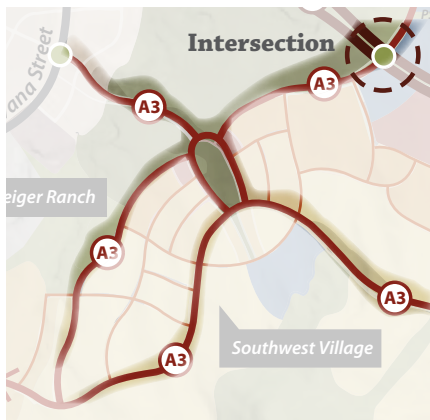
**A3 - Residential Collector (Standard)**



**A3 - Residential Collector (Standard)**

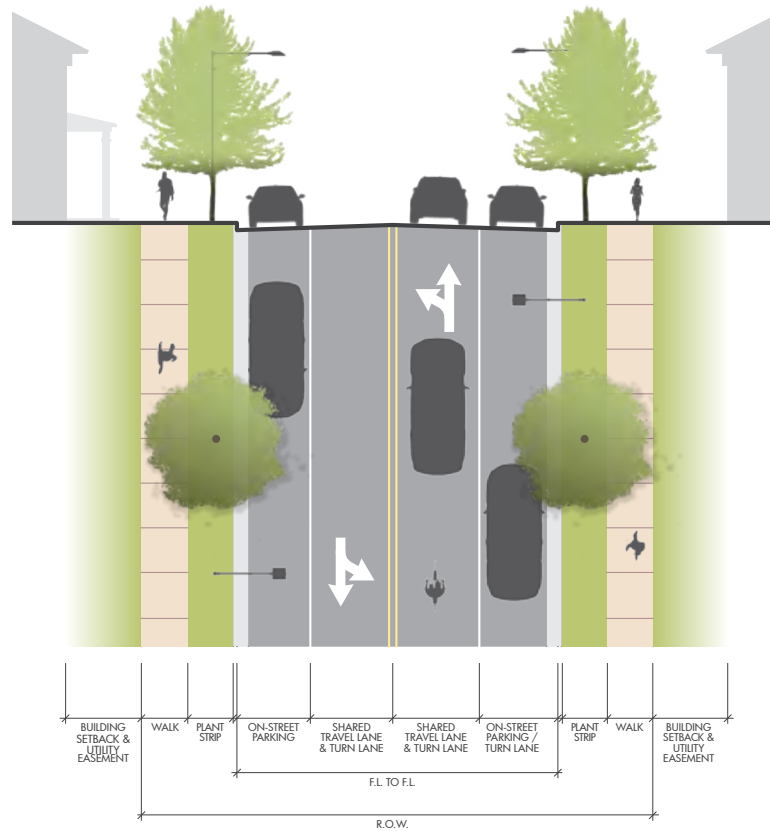
85

**A3 - Residential Collector Intersection**



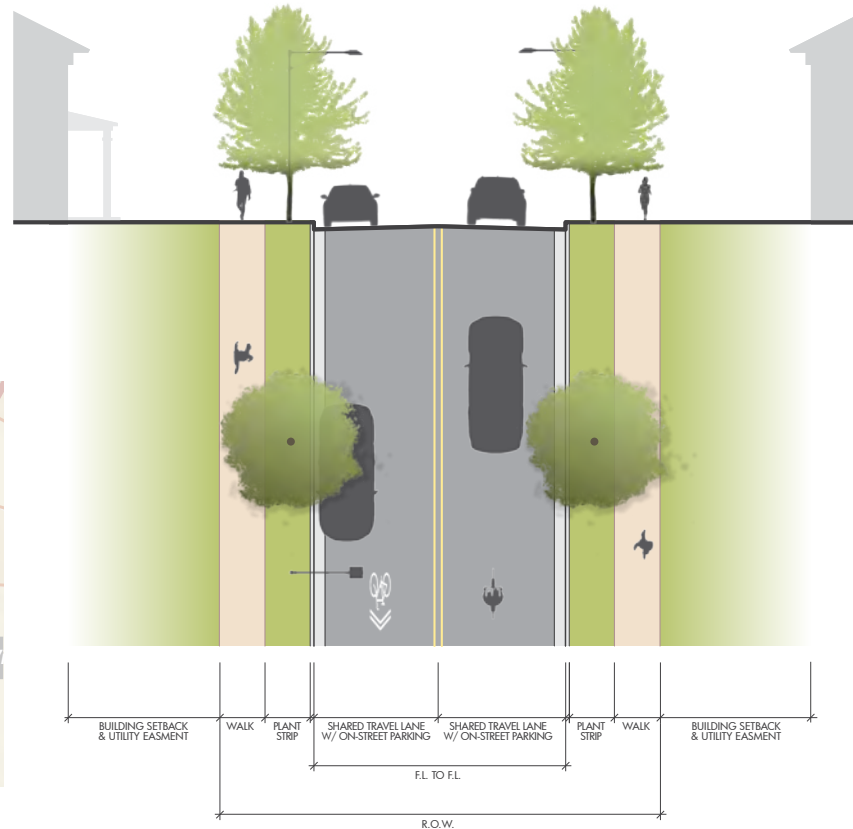
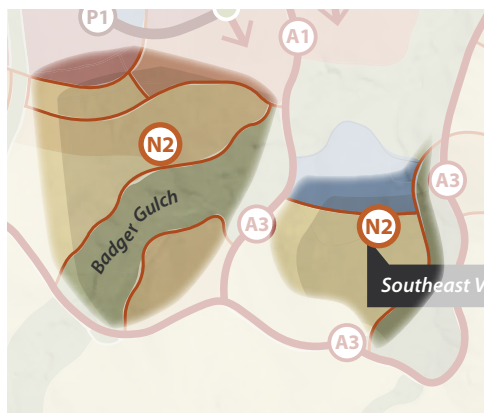
**A3 - Residential Collector (at signalized intersections with RidgeGate Parkway)**

**N1 -  
Residential  
Local Street:  
Type I**



**N1 -  
Residential  
Local Street:  
Type I**

**N2 -  
Residential  
Local Street:  
Type II**



**N2 -  
Residential  
Local Street:  
Type II**

# OTHER RESOURCES

1. City of Lone Tree Walk & Wheel Report, Prepared by Fehr & Peers, January, 2015.
2. City of Lone Tree Chapter 16 Zoning - Lone Tree.
3. RidgeGate, 5th Amendment, Planned Development District, as amended.
4. Ridgegate East Technical Supplement, as amended.
5. Centennial Airport Influence Area (AIA) Map.
6. The City of Lone Tree Comprehensive Plan



SILAHY  
TECHNOLOGY  
PARK  
MAIN STREET  
HYPERLOOM  
FLATIRON  
COORINA



DT  
DESIGN