

Murphy USA Parking Variance Request

Pursuant to Section 16-26-30 of the City of Lone Tree Municipal Code, Murphy USA is seeking a reduction in the minimum parking requirements for this development proposal.

Background

The following summarizes the parking requirements contained in Article XXVII of the Municipal Code.

Gas Station/Repair Garage:	1 space per employee on maximum shift, plus 2 spaces per bay or stall
Retail or Wholesale, Convenience:	1 space per 250 square feet of floor area
Gas Station @ 12 stalls x 2:	24 spaces required
Retail @ 2,824 / 250:	11.296 = 11 spaces*
Max Employees:	2
Total parking spaces:	37
25% Administrative Variance:	Allows a reduction of 9 spaces (25% = 9.25)*
Parking required with Administrative Variance:	28 parking spaces

*Note: Section X.B.11 of the Park Meadows Town Center PD states that fractions resulting from calculations shall be rounded off as follows: fractions of $\frac{1}{2}$ or more shall be rounded to the next higher whole number, and fractions lower than $\frac{1}{2}$ shall be rounded to the lower whole number.

Request

Murphy USA is requesting a reduction to allow a total of twenty-six (26) parking spaces as follows:

- Parking spaces at the fueling positions under the canopy: Twelve (12)
- Parking spaces at the storefront: Six (6) plus one (1) ADA – Seven (7)
- Parking spaces along the western property line: Seven (7)
- Total Parking Spaces Provided: Twenty-six (26)

Site Plans for three Murphy facilities that are currently open and operating are included with this Variance Request. The approved Site Plans for these locations are attached for reference. The parking provided at all three sites is in compliance with the jurisdictional requirements – no variances were required. All of these sites are currently operating and functioning with no issues related to parking.

- 15147 E. Mississippi Avenue, Aurora, CO 80012

Standard parking spaces provided at storefront:	8
ADA parking spaces provided at storefront:	1
Parking spaces under the canopy:	12
Total parking spaces provided:	21

- 14900 E. Colfax Avenue, Aurora, CO 80011

Standard parking spaces provided at storefront:	7
ADA parking spaces provided at storefront:	2
Parking spaces under the canopy:	16
Total parking spaces provided:	25

- 5640 Central Park Boulevard, Denver, CO 80238

Standard parking spaces provided at storefront:	7
ADA parking spaces provided at storefront:	2
Parking spaces under the canopy:	16
Total parking spaces provided:	25

Based on existing conditions and parking provided at other Murphy USA facilities, the requested parking variance will not adversely affect the operation of the proposed Murphy USA fueling station. The parking provided is appropriately designed for the anticipated demand and the site's convenience-oriented use, which typically generates short customer visits and rapid parking-space turnover. Accordingly, the Murphy USA station will operate safely and efficiently with the requested variance.

Employee Parking shall be designated for two of the parallel spaces along the western property line, furthest from the store.

The delivery times for trucks including fueling trucks and the convenience store delivery trucks are expected to be during off-peak hours and would not interfere with the internal site queuing and circulation. The delivery vehicles will stage adjacent to the parallel parking spaces along the western property line, and/or utilize any vacant parallel parking spaces, as may be appropriate. The truck turn route for the fueling trucks go through the Air/Vac station location, but since the trucks would enter during off-peak times and the Air/Vac station use time is low, no conflict is anticipated.

Justification

Sec. 16-26-30. - Variance; approval criteria.

No variance shall be granted unless each of the following criteria in subsections (a) and (b) are met:

- a) A variance may be granted only where it can be demonstrated that such:
 - 1) Is sensitive to and compatible with adjoining existing and planned land uses;
Response: A reduction in parking will not have any negative impacts on the existing surrounding uses. The proposed reduction in parking is compatible with surrounding land uses and the broader development pattern of the area. Convenience stores with fueling stations primarily serve short-duration visits, resulting in high turnover rather than long-term parking demand. The proposed parking supply is sufficient to accommodate peak demand while avoiding excessive paved areas. Additionally, the reduction maintains an appropriate site layout that supports safe vehicle circulation.

2) Will not adversely impact the natural environment through unwarranted or undesirable grading, altering of drainages or vegetation removal;
Response: A reduction in parking will not have a negative impact on the natural environment, as the reduction in parking will allow for more landscaping and minimize grading activities and removal of existing landscape vegetation that would be required to accommodate the additional parking spaces. As a result, the proposed variance supports environmentally responsible site design and reduces potential impacts to the natural environment.

3) Will not increase or otherwise exacerbate known wildfire risk to the site or to adjacent properties or structures;
Response: A reduction in parking will not have any impact on wildfire risk to this site or adjacent developments. The development will comply with all applicable fire safety requirements, including building codes, fuel safety standards, and fire department access requirements. Adequate fire access lanes and circulation will be maintained regardless of the reduced parking count, ensuring emergency response capabilities remain fully compliant.

4) Maintains a desirable balance with the overall bulk and massing of building architecture; and
Response: Providing only the parking necessary to support the use allows the site design to remain proportional to the building and canopy structures. Excessive parking areas can visually dominate a site and detract from the architectural character of the development. The proposed reduction ensures that the building and fueling facilities remain the primary site features rather than large expanses of asphalt. This approach promotes a balanced and cohesive site design.

5) Promotes other community goals as set forth in the City's Comprehensive Plan, such as a well-planned, high-quality and, where appropriate, compact development.

Response: A reduction in parking will not negatively impact the overall community goals or the objectives of the Comprehensive Plan. In fact, the reduction in parking will allow for a reduction in the paved area on the site. The requested parking reduction advances general planning and design principles, including:

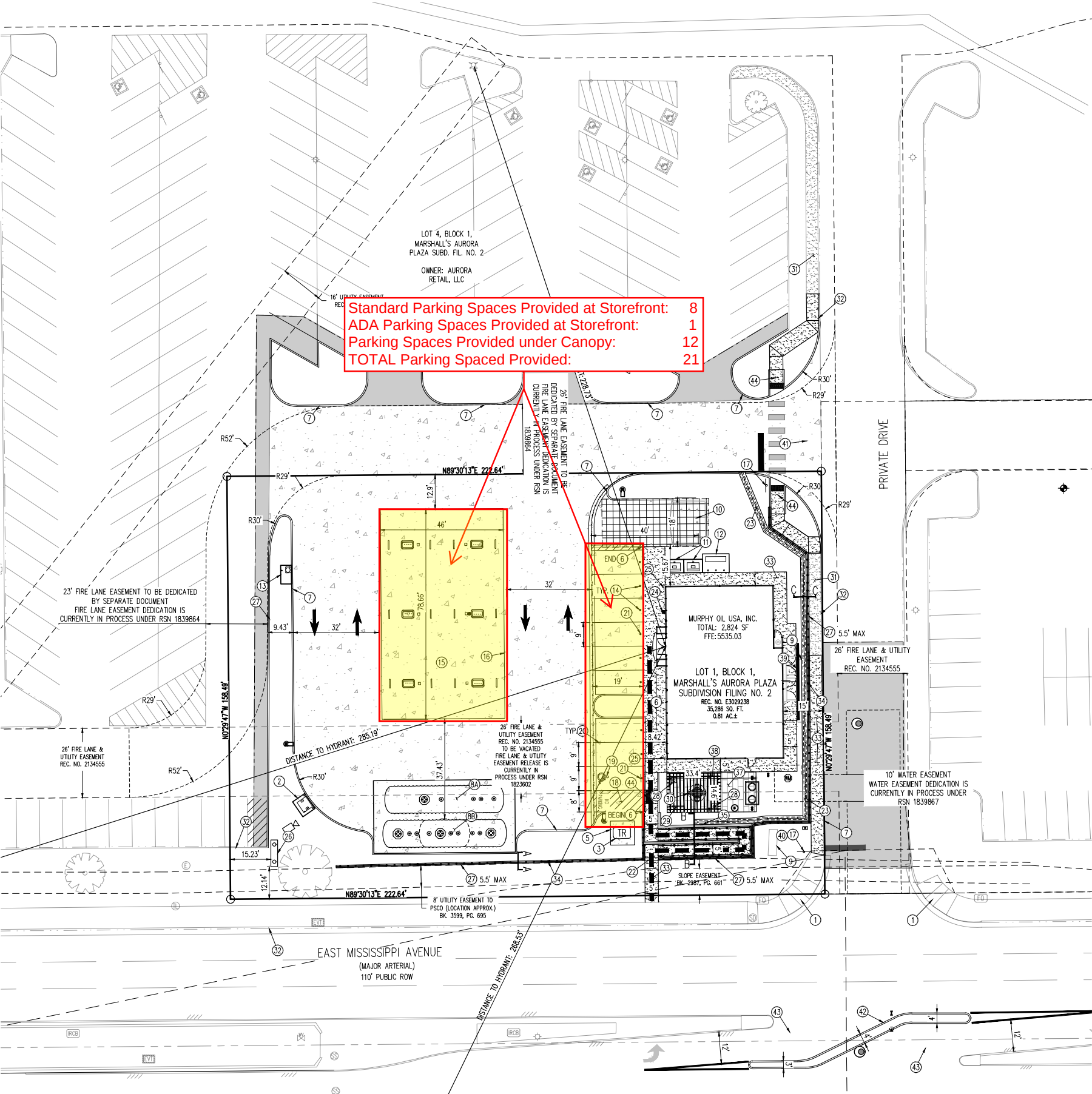
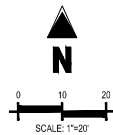
- *Encouraging efficient land use and compact development*
- *Reducing unnecessary impervious surfaces*
- *Supporting high-quality site design and aesthetics*
- *Promoting environmentally responsible development practices*

b) A variance may be granted, provided that no substantial detriment to the public health, safety and welfare is created, that the intent and purpose of this Chapter is not impaired and that no active zoning violations exist on the site, unless such violations have been otherwise approved by the Director.

Response: The requested variance will not create any detriment to public health, safety, or welfare. The proposed parking is adequate to serve the anticipated demand for a convenience store and fueling station, which primarily involves short-duration

visits and high turnover. Vehicle circulation, emergency access, and site safety will be maintained in accordance with applicable municipal and fire safety standards. The reduction also avoids excessive paved areas that could contribute to stormwater runoff or unnecessary site disturbance. Furthermore, there are no known zoning violations associated with the site. The requested variance maintains the intent and purpose of the zoning regulations while allowing a more efficient and context-sensitive site design.

MURPHY FUEL AT MARSHALL PLAZA
CONDITIONAL USE AND SITE PLAN AMENDMENT WITH ADJUSTMENT
LOT 1, BLOCK 1, MARSHALL'S AURORA PLAZA SUBDIVISION FILING NO. 2
LOCATED IN THE SOUTHEAST QUARTER OF THE SOUTHEAST QUARTER OF SECTION 18, TOWNSHIP 4 SOUTH, RANGE 66 WEST OF THE 6TH P.M.
CITY OF AURORA, COUNTY OF ARAPAHOE, STATE OF COLORADO

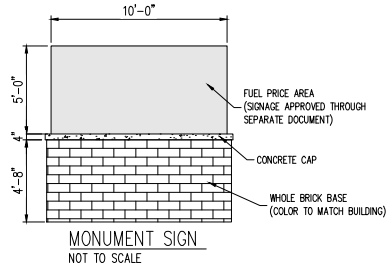


Standard Parking Spaces Provided at Storefront: 8
ADA Parking Spaces Provided at Storefront: 1
Parking Spaces Provided under Canopy: 12
TOTAL Parking Spaced Provided: 21

- SITE SCHEDULE**
- 1 EXISTING ACCESSIBLE RAMP TO REMAIN
 - 2 PROPOSED ATM
 - 3 PROPOSED TRANSFORMER
 - 4 PROPOSED CONCRETE WALK AROUND BUILDING AND FRONT OF PARKING TO BE FLUSH WITH PAVEMENT PER MURPHY'S STANDARD DETAIL
 - 5 PROPOSED CONCRETE TRANSFORMER PAD
 - 6 PROPOSED CURB TRANSITION - SIDEWALK TO BE FLUSH WITH PARKING
 - 7 PROPOSED CURB AND GUTTER
 - 8A PROPOSED UNDERGROUND STORAGE TANKS (25,000 GAL REGULATOR UNLEADED)
 - 8B PROPOSED UNDERGROUND 3 COMPARTMENT TANK (8,000 GAL E-0 ETHANOL FREE REGULAR UNLEADED, 10,000 GAL PREMIUM UNLEADED, 8,000 DIESEL EXHAUST FLUID)
 - 9 EXISTING SIGN TO REMAIN (ADJUST TO FINISHED GRADE)
 - 10 PROPOSED TRASH ENCLOSURE
 - 11 PROPOSED 7'X10' CONCRETE SLAB FOR PROPANE TANKS
 - 12 PROPOSED 5'X7' CONCRETE PAD (2 TOTAL) FOR ICE UNITS
 - 13 PROPOSED AIR VACUUM UNIT WITH 4'X7' CONCRETE SLAB
 - 14 PROPOSED SINGLE GUARD POST
 - 15 PROPOSED FUELING STATION WITH OVERHEAD CANOPY
 - 16 EDGE OF CONCRETE SLAB UNDERNEATH THE CANOPY
 - 17 PROPOSED STOP SIGN
 - 18 PROPOSED BLUE 4" WIDE PAINTED STRIPES, 2" O.C. @ 45°
 - 19 PROPOSED ACCESSIBLE PARKING SYMBOL
 - 20 PROPOSED 4" WIDE WHITE PARKING STALL PAVEMENT STRIPING
 - 21 PROPOSED ADA PARKING SIGN MOUNTED ON BOLLARD
 - 22 PROPOSED ACCESSIBILITY RAMP
 - 23 PROPOSED 1' CONCRETE U CHANNEL
 - 24 PROPOSED KNOX BOX (SEE ARCH SHEETS)
 - 25 PROPOSED EMERGENCY SHUT-OFF SWITCH AND FIRE EXTINGUISHER (SEE ARCH SHEETS)
 - 26 PROPOSED MONUMENT SIGN
 - 27 PROPOSED RETAINING WALL (HEIGHT VARIES, 5.5 FT MAXIMUM)
 - 28 PROPOSED TRASH RECEPTACLES
 - 29 PROPOSED BIKE RACK
 - 30 PROPOSED PICNIC TABLE
 - 31 EXISTING SIDEWALK TO REMAIN
 - 32 EXISTING CURB AND GUTTER TO REMAIN
 - 33 PROPOSED SIDEWALK
 - 34 PROPOSED RAILING
 - 35 PROPOSED STREET LIGHT (REFERENCE PHOTOMETRIC PLANS)
 - 36 EXISTING FIRE HYDRANT TO REMAIN
 - 37 PROPOSED PATIO (AREA: 487.1 SQFT, 17.2% OF GFA)
 - 38 PROPOSED PERGOLA
 - 39 PROPOSED SCREEN WALL (REFERENCE LANDSCAPE PLANS FOR DETAIL)
 - 40 PROPOSED TURN RESTRICTION SIGN (R3-5)
 - 41 PROPOSED CROSSWALK
 - 42 PROPOSED TRAFFIC MOVEMENT RESTRICTION
 - 43 PROPOSED PRE-FORMED THERMOPLASTIC LEFT TURN MARKING
 - 44 PROPOSED CURB RAMP

- SITE LEGEND**
- PROPERTY BOUNDARY LINE
 - ADJACENT PROPERTY BOUNDARY LINE
 - PROPOSED SAWCUT LINE
 - EXISTING EASEMENT LINE
 - PROPOSED EASEMENT LINE
 - SETBACK LINE
 - ADA PATH
 - PROPOSED CURB AND GUTTER
 - EXISTING CURB AND GUTTER
 - PARKING COUNT
 - PROPOSED SIGN
 - PROPOSED SINGLE GUARD POST
 - PROPOSED ADA PARKING SYMBOL
 - EXISTING ELECTRIC LIGHT POLE
 - EXISTING ELECTRIC VAULT
 - EXISTING ELECTRICAL MANHOLE
 - EXISTING TELEPHONE MANHOLE
 - EXISTING IRRIGATION CONTROL BOX
 - EXISTING FIBER OPTIC CONTROL BOX
 - EXISTING SANITARY SEWER MANHOLE
 - EXISTING SANITARY STORM MANHOLE
 - EXISTING WATER VALVE
 - EXISTING FIRE HYDRANT
 - EXISTING WATER METER
 - PROPOSED WATER METER
 - PROPOSED FIRE HYDRANT
 - PROPOSED GREASE INTERCEPTOR
 - PROPOSED TRANSFORMER
 - PROPOSED STANDARD DUTY CONCRETE PAVEMENT
 - PROPOSED HEAVY DUTY CONCRETE PAVEMENT
 - PROPOSED CONCRETE SIDEWALK
 - PROPOSED WALL
 - PROPOSED ASPHALT
 - EXISTING CONCRETE SIDEWALK

BENCHMARK INFORMATION:
AURORA SURVEY CONTROL BENCHMARK, COA ID: 456619NE004
CITY OF AURORA BENCHMARK DESCRIPTION: ON THE NW CORNER OF FLORIDA AND CHAMBERS ROAD AT THE WEST P.C.R. A 3" BRASS CAP ATOP OF CURB OPEN INLET ON FLORIDA FACING SOUTH AND CENTERED ON THE BACK OF CURB OPEN INLET.
ELEVATION = 5530.96' (NAVD88 PER CITY OF AURORA VERTICAL CONTROL PAGE 11H)



MURPHY OIL USA, INC.
MURPHY OIL USA
200 PEACH STREET
EL DORADO, AR 71730

SITE DEVELOPMENT PLAN
MURPHY OIL USA
E MISSISSIPPI AVE & S CHAMBERS RD
AURORA, COLORADO

#	Date	Issue / Description	Init.
1	11/03/2023	1ST SUBMITTAL	LMK
2	03/21/2024	2ND SUBMITTAL	LMK
3	05/13/2024	3RD SUBMITTAL	LMK
4	08/13/2024	4TH SUBMITTAL	LMK
5	10/01/2024	5TH SUBMITTAL	LMK
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SITE PLAN

MURPHY EXPRESS SITE PLAN

LOT 1, BLOCK 1

CITADEL ON COLFAX SUBDIVISION FILING NO. 2

LOCATED IN THE NORTHEAST QUARTER OF SECTION 6,
TOWNSHIP 4 SOUTH, RANGE 66 WEST OF THE 6TH PRINCIPAL MERIDIAN,
CITY OF AURORA, COUNTY OF ARAPAHOE, STATE OF COLORADO
SITE PLAN

SITE LEGEND

- PROPERTY BOUNDARY
- PROPOSED EASEMENT
- PROPOSED ACCESSIBLE ROUTE
- PROPOSED CURB AND GUTTER
- EXISTING EASEMENT
- PROPOSED SAWCUT LINE
- PROPOSED PARKING COUNT
- PROPOSED SITE LIGHT
- PROPOSED STANDARD DUTY CONCRETE PAVEMENT
- PROPOSED HEAVY DUTY CONCRETE PAVEMENT
- PROPOSED CONCRETE SIDEWALK
- EXISTING CONCRETE SIDEWALK
- PROPOSED STAMPED CONCRETE
- PROPOSED ASPHALT PAVEMENT

NOTES

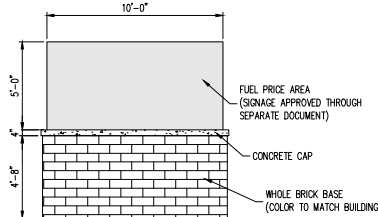
- ALL DIMENSIONS ARE MEASURED FROM THE FLOWLINE UNLESS NOTED OTHERWISE.
- THE PRIVATE DRIVE MUST BE COMPLETED FROM ALTURA BOULEVARD TO KMART PLAZA TO THE EAST PRIOR TO THE ISSUANCE OF A CERTIFICATE OF OCCUPANCY.
- ONSITE STORM SEWER SYSTEM IS PRIVATE AND WILL BE MAINTAINED BY MURPHY.
- A "RIGHT LANE MUST TURN RIGHT" SIGN MEETING MUTCD STANDARDS SHALL BE INSTALLED BY THE DEVELOPER ON THE SOUTHWEST SIDE OF THE COLFAX AVE AND ALTURA BLVD INTERSECTION PRIOR TO OPENING THE MURPHY EXPRESS ACCESS.
- A STOP SIGN PER MUTCD STANDARDS SHALL BE PLACED AT THE SOUTHEAST CORNER OF COLFAX AND ALTURA IF NO SIGNAL HAS BEEN INSTALLED AT THE INTERSECTION PRIOR TO THE OPENING OF THE MURPHY EXPRESS.
- CONTRACTOR SHALL SAWCUT ASPHALT ALONG EDGE OF PROPOSED CONCRETE GUTTER IN COLFAX AVE, AND AVOID USING FORMS ALONG THE EDGE OF ASPHALT.

BENCHMARK:

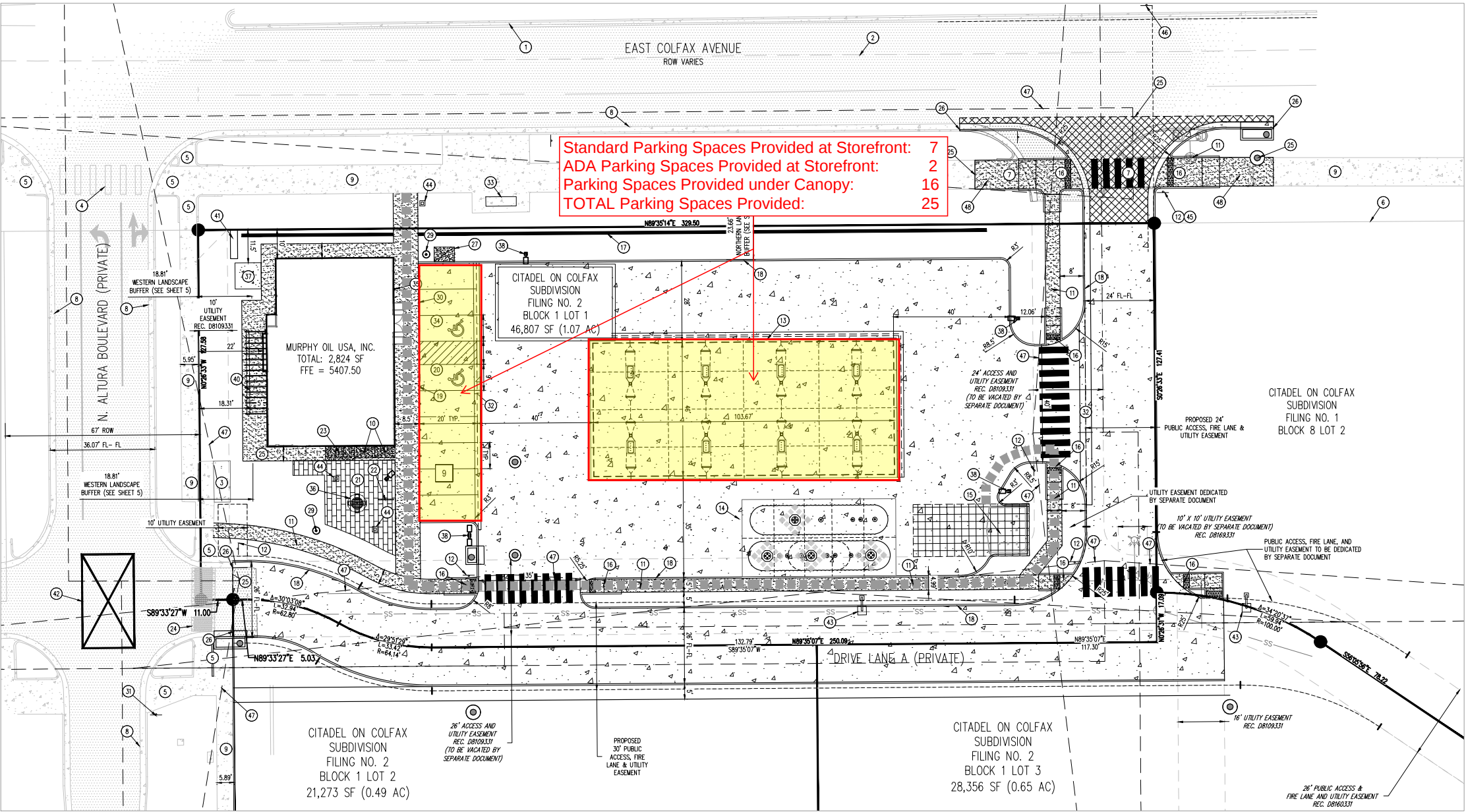
PUBLISHED BENCHMARK:
CITY OF AURORA BENCHMARK NO. 496606NE005 (OLD BENCHMARK ID G-038),
BEING A CHISELED SQUARE AT THE SOUTHWEST CORNER OF CONCRETE INLET
STRUCTURE SOUTH OF COLFAX AVENUE ON EAST SIDE OF THE WEST DRIVEWAY
ENTRY TO K-MART SHOPPING CENTER. NAVD 88 ELEVATION 5419.26

CAUTION - NOTICE TO CONTRACTOR

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- WHERE A PROPOSED UTILITY CROSSES AN EXISTING UTILITY, IT IS THE CONTRACTOR'S RESPONSIBILITY TO FIELD VERIFY THE HORIZONTAL AND VERTICAL LOCATION OF SUCH EXISTING UTILITY, EITHER THROUGH POT-HOLING OR ALTERNATIVE METHOD. REPORT INFORMATION TO THE ENGINEER PRIOR TO CONSTRUCTION.



MONUMENT SIGN
NOT TO SCALE

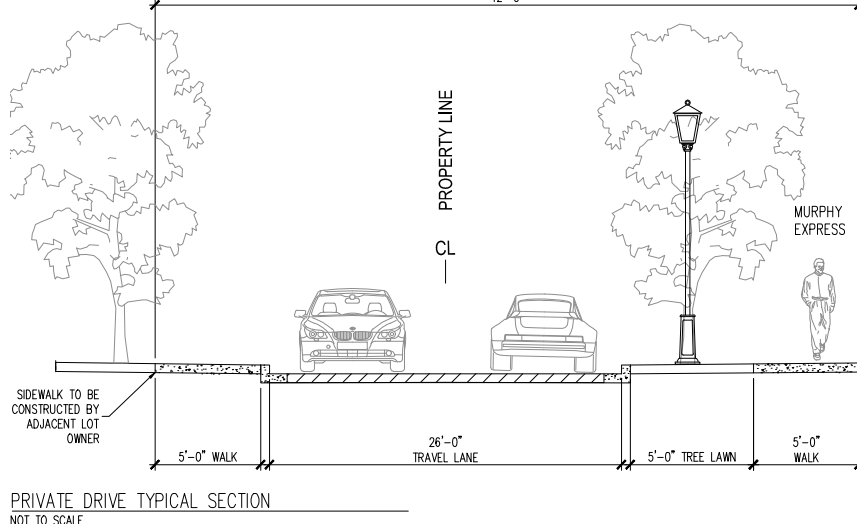


SITE PLAN SCHEDULE

- | | | |
|---|---|--|
| 1 EXISTING MEDIAN | 16 PROPOSED ADA RAMP PER CITY OF AURORA STANDARD S9.6 | 31 PROPOSED "DO NOT BLOCK INTERSECTION" SIGN TO BE MOUNTED BELOW PEDESTRIAN CROSSING SIGN |
| 2 EXISTING STRIPING | 17 PROPOSED LANDSCAPE SCREEN WALL | 32 PROPOSED CONCRETE CROSSSPAN |
| 3 EXISTING CONCRETE TO BE REMOVED | 18 PROPOSED CURB AND GUTTER | 33 PROPOSED BENCH TO BE ADDED TO EXISTING CITADEL BUS STOP (REFERENCE SITE DETAILS) |
| 4 EXISTING PEDESTRIAN CROSSING | 19 PROPOSED PARKING TO BE FLUSH WITH SIDEWALK | 34 PROPOSED STANDARD ACCESSIBLE PARKING STALL |
| 5 EXISTING ADA RAMP | 20 PROPOSED VAN ACCESSIBLE PARKING STALL | 35 PROPOSED KNOX BOX (REFERENCE ARCH. SHEETS) |
| 6 EXISTING ADJACENT PROPERTY LINE | 21 PROPOSED DECORATIVE CONCRETE PER CITADEL DESIGN STANDARDS | 36 PROPOSED PICNIC TABLE (REFERENCE SITE DETAILS) |
| 7 EXISTING ADA RAMP TO BE REMOVED | 22 PROPOSED BIKE RACK (REFERENCE SITE DETAILS) | 37 PROPOSED TRANSFORMER PAD |
| 8 EXISTING CURB AND GUTTER | 23 PROPOSED CONCRETE SLAB FOR PROPANE TANKS | 38 PROPOSED SITE LIGHT |
| 9 EXISTING CONCRETE WALK | 24 PROPOSED STAMPED CONCRETE CROSSWALK BY MASTER DEVELOPER (SEE LICENSE AGREEMENT D9078027) | 39 EXISTING STREET LIGHT |
| 10 PROPOSED CONCRETE SLAB FOR ICE UNITS | 25 PROPOSED SAWCUT LINE | 40 PROPOSED SCREEN WALL AND TRELLIS (REFERENCE SITE DETAILS) |
| 11 PROPOSED SIDEWALK | 26 MATCH EXISTING | 41 PROPOSED MONUMENT SIGN TO COMPLY WITH CITY OF AURORA AND CITADEL STANDARDS |
| 12 PROPOSED STOP SIGN | 27 PROPOSED VACUUM UNIT | 42 PROPOSED "DO NOT BLOCK INTERSECTION" STRIPING PER MUTCD FIGURE 3B-18, OPTION C, SINGLE X STRIPING |
| 13 PROPOSED OVERHEAD CANOPY | 28 PROPOSED CONCRETE TRANSFORMER PAD | 43 PROPOSED STREET LIGHT |
| 14 PROPOSED UNDERGROUND STORAGE TANKS (1-25,000 GAL OF REGULAR GASOLINE & 1-26,000 GAL OF PREMIUM GASOLINE) | 29 PROPOSED PEDESTRIAN LIGHT | 44 PROPOSED TRASH RECEPTACLE (REFERENCE SITE DETAILS) |
| 15 PROPOSED TRASH ENCLOSURE (REF. SHEET 2.2) | 30 PROPOSED BOLLARD (TYP.) | |

SIGNAGE TABLE

FACADE	PROPOSED SIGN AREA
WESTERN FACADE - BUILDING FRONTAGE	0 SQUARE FEET
NORTHERN FACADE - BUILDING FRONTAGE	49.5 SQUARE FEET (CANOPY)
EASTERN FACADE - BUILDING FRONTAGE	169.2 SQUARE FEET (53.3 SF - BUILDING, 115.9 SF - CANOPY)
SOUTHERN FACADE - BUILDING FRONTAGE	115.9 SQUARE FEET (CANOPY)



Galloway

6162 S. Willow Drive, Suite 320
Greenwood Village, CO 80111
303.770.8884
GallowayUS.com

MURPHY EXPRESS
CITADEL ON COLFAX SUBDIVISION FILING NO. 2
BLOCK 8, LOT 1

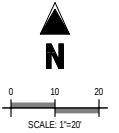
AURORA, CO

#	Date	Issue / Description	Init.
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Project No: MOC00055.20
Drawn By: ZLS
Checked By: JES
Date: 05/26/2022

SITE PLAN

SHOPS AT BEELER PARK / MURPHY EXPRESS
2ND AMENDMENT
TO THE SHOPS AT BEELER PARK SITE DEVELOPMENT PLAN
PART OF THE SOUTHWEST 1/4 OF SECTION 10, TOWNSHIP 3 SOUTH, RANGE 67 WEST OF THE 6TH P.M.
CITY AND COUNTY OF DENVER, STATE OF COLORADO
5640 N CENTRAL PARK BOULEVARD



CENTRAL PARK BOULEVARD
PRIMARY STREET - (ROW VARIES)

Standard Parking Spaces Provided at Storefront: 7
ADA Parking Spaces Provided at Storefront: 2
Parking Spaces Provided under Canopy: 16
TOTAL Parking Spaces Provided: 25

BUILDING F
MURPHY FUEL
TOTAL 2,824 SF
5640 N CENTRAL PARK BLVD
FFE = 5234.10'

BLDG. C -
PANDA
EXPRESS
TOTAL
2,381SF

SCHEDULE

- EXISTING STREET LIGHT TO REMAIN
- EXISTING FIRE HYDRANT TO REMAIN
- EXISTING CURB AND GUTTER TO REMAIN
- PROPOSED OVERHEAD CANOPY
- PROPOSED 4" WIDE PAINTED STRIPES, 2.0' O.C. @ 45°
- CONNECT NEW CURB INTO EXISTING CURB
- EXISTING CONCRETE SIDEWALK TO REMAIN
- EDGE OF CONCRETE SLAB UNDERNEATH THE CANOPY
- PROPOSED TYPE 1 CURB RAMP PER CCD STD DWG 7.1
- PROPOSED EDGE OF ASPHALT
- PROPOSED 6' TALL TRASH ENCLOSURE
- PROPOSED UNDERGROUND STORAGE TANKS
- NOT USED
- PROPOSED AIR VACUUM UNIT WITH 4'X7' CONCRETE SLAB
- PROPOSED MURPHY EXPRESS SIGN PER APPROVED ELEVATION
- NOT USED
- PROPOSED PARKING STALLS
- PROPOSED PRICE SIGN PER APPROVED ELEVATION
- PROPOSED 5-FT SIDEWALK PER CCD STD DWG 5.0
- PROPOSED CONCRETE INTEGRAL CONCRETE CURB
- PROPOSED 7'X10' CONCRETE SLAB FOR PROPANE TANKS
- PROPOSED CROSSWALK STRIPING
- PROPOSED ACCESSIBLE PARKING SIGN
- PROPOSED 5'X7' CONCRETE PAD (2 TOTAL) FOR ICE UNIT
- LIMITS OF SAWCUT AND PAVEMENT REMOVAL
- PROPOSED 2.756 SQUARE FOOT MURPHY EXPRESS KIOSK
- PROPOSED ACCESSIBLE PARKING SYMBOL
- EXISTING TRANSFORMER TO REMAIN
- PROPOSED DIRECTIONAL TRAFFIC FLOW ARROW
- PROPOSED SITE LIGHT
- PROPOSED "STOP" BAR, LANE STRIPING, AND "STOP" LETTERING
- PROPOSED "STOP" SIGN
- PROPOSED LANDSCAPE SCREEN WALL
- PROPOSED CONCRETE TRANSFORMER PAD
- PROPOSED 6" CONCRETE CURB AND GUTTER
- PROPOSED BILLBOARD TO BE REMOVED BY DEVELOPER
- PROPOSED TYPE 2 CURB RAMP PER CCD STD DWG 7.2A
- PROPOSED STRIPING BY BUILDING C

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BUILDING SPECIFIC BASE PLANE - MURPHY BLDG

BASE PLANE: AVERAGE ELEVATION AT ALL FOUR CORNERS OF THE BUILDING AT ORIGINAL GRADE OR FINISHED GRADE, WHICHEVER IS LOWER.
ORIGINAL GRADE: 5234.06 + 5233.44 + 5233.83 + 5234.35 = 20,935.68 / 4 = 5233.92 = 5234.00
FINISHED GRADE: 5233.92 = 5234.10
BUILDING SPECIFIC BASE PLANE = 5234.10

TRANSPORTATION NOTES

- SITE PLAN APPROVAL DOES NOT CONSTITUTE A NOTICE TO PROCEED FOR ROW WORK. THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING ALL PROJECT ROW PERMITS ASSOCIATED WITH CONSTRUCTION IN THE RIGHT-OF-WAY. IMPROVEMENTS MADE WITHIN THE PUBLIC RIGHT-OF-WAY TOTALING MORE THAN \$20,000 REQUIRE A PERFORMANCE BOND. CONTACT THE ROW INSPECTOR FOR THIS AREA OF THE CITY, JESSE CONTRERAZ AT (303) 446-3883 AT LEAST TWO WEEKS BEFORE ANY ROW PERMIT NEEDS.
- ALL WORK IN THE ROW SHALL CONFORM TO CURRENT CITY AND COUNTY OF DENVER SPECIFICATIONS, SHALL BE PERFORMED BY A LICENSED AND BONDED RIGHT-OF-WAY CONTRACTOR, AND REQUIRE INSPECTION BY THE CITY PRIOR TO A TEMPORARY CERTIFICATE OF OCCUPANCY (TCO) OR CERTIFICATE OF OCCUPANCY (CO) BEING ISSUED.
- CONTRACTOR IS RESPONSIBLE FOR PROVIDING AND MAINTAINING ADEQUATE TRAFFIC CONTROL THROUGHOUT THE PROJECT, INCLUDING PROPER TRAFFIC CONTROL DEVICES AND/OR PERSONNEL AS REQUIRED. A TRAFFIC CONTROL PLAN (TCP) IS SUBJECT TO CITY AND COUNTY OF DENVER AND/OR CDOT APPROVAL PRIOR TO COMMENCING WORK ON ROADWAY ROW. A COPY OF APPROVED TCPs AND MUST BE AVAILABLE ON SITE DURING WORK. TRAFFIC CONTROL TO BE IN ACCORDANCE WITH MUTCD, SECTION VI.
- PER SECTION 49-65.1.1 OF THE DENVER MUNICIPAL CODE, THE PROPERTY OWNER OR LESSEE OF ANY REAL PROPERTY IS RESPONSIBLE FOR THE CONTINUING CARE, MAINTENANCE, REPAIR, AND REPLACEMENT OF ALL IMPROVEMENTS INSTALLED IN THE PUBLIC ROW BETWEEN THE PROPERTY LINE AND THE CURB LINE ADJOINING THEIR PROPERTY.

LEGEND

	ZONE LOT BOUNDARY
	EASEMENT BOUNDARY LINE
	PROPOSED LOT LINE
	ADA PATH
	EXISTING WATER METER
	EXISTING WATER VALVE
	EXISTING FIRE HYDRANT
	EXISTING WATER MANHOLE
	EXISTING ELECTRIC MANHOLE
	EXISTING SANITARY SEWER MANHOLE
	EXISTING STORM SEWER MANHOLE
	EXISTING LIGHT POLE
	EXISTING TRANSFORMER
	EXISTING ELECTRICAL BOX
	EXISTING TRAFFIC SIGNAL BOX
	EXISTING TELEPHONE VAULT
	EXISTING SIGN
	EXISTING WATER PIPE POST
	EXISTING PETROLEUM PIPELINE POST
	EXISTING FIRE RISER PIPE
	EXISTING GAS VALVE
	EXISTING SURVEY BENCHMARK
	PROPOSED STANDARD DUTY CONCRETE PAVEMENT
	PROPOSED HEAVY DUTY CONCRETE PAVEMENT
	PROPOSED SITE LIGHTING
	PROPOSED TYPE 16 STORM SEWER INLET
	PARKING COUNT
	FFE
	FINISHED FLOOR ELEVATION
	FINISHED GRADE
	EXISTING GRADE

LEGAL DESCRIPTION:

A PORTION OF LOT 1, BLOCK 41, STAPLETON PLING NO. 49 AND BEING SITUATED IN THE SOUTHWEST QUARTER OF SECTION 10, TOWNSHIP 3 SOUTH, RANGE 67 WEST OF THE 6TH P.M., CITY AND COUNTY OF DENVER, STATE OF COLORADO.
PARCEL NUMBER: 01103-41-018-000

BENCHMARK

ELEVATIONS ARE BASED ON CITY & COUNTY OF DENVER BENCHMARK 707, DESCRIBED AS A CCD BRASS CAP, LOCATED IN THE SOUTHWEST CORNER OF A BRIDGE ABUTMENT.
(ELEVATION: 5270.34) NAVD 88

ON-SITE BENCHMARKS

SET BENCHMARK #1
DESCRIPTION: CHISELED SQUARE IN SW CORNER OF INLET BOX (ELEVATION=5231.04)

SET BENCHMARK #2
DESCRIPTION: CHISELED SQUARE IN NE CORNER OF INLET BOX (ELEVATION=5234.87)

BASIS OF BEARING

ALL BEARINGS ARE GRID BEARINGS OF THE COLORADO STATE PLANE COORDINATE SYSTEM, CENTRAL ZONE, NORTH AMERICAN DATUM 1983, NORTH LINE OF LOT 1, BLOCK 41 STAPLETON PLING NO. 49, RECORDED AT RECEPTION NUMBER 20187454 OF THE CITY AND COUNTY OF DENVER CLERK AND RECORDER OFFICE AND IS CONSIDERED TO BEAR N89°44'24"E, MONUMENTED ON THE WEST BY A 26.10 FOOT WITNESS CORNER BRASS PLUG STAMPED "ACCOM PLS 20683" AND ON THE EAST BY A 13.75 FOOT WITNESS CORNER BRASS PLUG STAMPED "ACCOM PLS 20683".

FLOODPLAIN STATEMENT

PER THE FEMA FLOOD INSURANCE RATE MAPS (FIRM), MAP NO. 080040092H, HAVING A MAP REVISED DATE OF NOV. 20, 2013, INDICATES THE SUBJECT PROPERTY TO BE DESIGNATED AS ZONE X (OUTSIDE 0.2% CHANCE OF FLOOD).

EASEMENT SCHEDULE

- | | |
|---|---|
| A | UTILITY EASEMENT DENVER WATER 42" CONDUIT NO. 93 (BOOK 1199, PAGE 622) |
| B | UTILITY EASEMENT DENVER WATER CONDUIT NO. 93 (RECEPTION NO. 2018087493, 2015105496) |
| C | RECEPTION NO. 2015113826, 2015072854, 2015072854, 2015072856, 2017072856 |
| D | UTILITY EASEMENT DENVER WATER CONDUIT NO. 93 (RECEPTION NO. 2016047302) |
| E | NOT USED |
| F | 30" PIPELINE EASEMENT, MAGELLAN PIPELINE COMPANY, LP. (RECEPTION NO. 2016176454) |
| G | UTILITY EASEMENT CENTURY LINK (RECEPTION NO. 2017028023) |

TRANSPARENCY NOTE

TRANSPARENCY IS REQUIRED FOR BUILDINGS LOCATED WITHIN 80' OF A PRIMARY STREET OR SIDE STREET.